

APPENDICES

CO 9 SOUTH

SUMMIT

ACCESS STUDY



DEC 2020



COLORADO
Department of
Transportation



Appendix A-Public Outreach

Appendix B-Existing Access Inventory

Appendix C-Crash History

Appendix D-Traffic Methodology, Data & Analysis

Appendix E-Access Control Plan Methodology & Evaluation Process

Appendix F-Intergovernmental Agreement

Appendix G-Access Control Plan Tables

Appendix H-Conceptual Trail Design and Opinion of Probable Cost

Appendix I-Conceptual Wildlife Crossing and Opinion of Probable Cost

Appendix A-Public Outreach



COLORADO
Department of
Transportation



CO 9 South Summit Access Control Plan Public Meeting Summary Open House 1 and 2

The Town of Blue River, the Town of Breckenridge, Summit County, and the Colorado Department of Transportation (CDOT) have identified a need to develop an Access Control Plan for SH 9 between the Park County/Summit County Line and Boreas Pass Road/Broken Lance Drive in Summit County, a total distance of approximately 8.76 miles. The access control plan will give CDOT and the local agencies a long-range planning document to identify access points along the state highway. In addition to the Access Control Plan, the project team has developed conceptual (15%) trail design within the study limits to expand on the Hoosier Pass Recreational Pathway Feasibility Study that was completed in 2013. The conceptual trail design will give local agencies a starting point to obtain future funding opportunities.

The project team, along with Stolfus and Associates, held two public meetings on May 15, 2019 (Open House 1) and September 25, 2019 (Open House 2) to seek public input on the proposed access plan and conceptual trail design.

Open House 1

Open House 1 was held at the Breckenridge Recreation Center, 880 Airport Road, Breckenridge, CO. A staff preview for CDOT and local agency staff was held between 3:30pm and 4:30pm. The public open house was held from 4:30pm to 7:30pm. A total of 55 people attended the open house. Display boards showing the access plan and the conceptual trail design were available for viewing. Comment forms were collected at the meeting in-person, through email, through CDOT's public comment tool, and through Survey Monkey. Comments were also collected in the form of sticky notes on the display boards. A total of eleven comment forms were filled out. Seven were submitted using the paper form, and four were collected via Survey Monkey. The comments received are summarized below.

Comment Summary

- 55% of respondents support the consolidation of access throughout the corridor
- 100% of respondents support the Conceptual Trail Plan
- Three most important priorities:
 - Pedestrian/Bicycle Trail Access (91% of respondents)
 - Safety (72% of respondents)
 - Driveway Access (55% of respondents)
- Concerns:
 - Sharing access with neighbor (45% of respondents)
 - Relocation of access on property (18% of respondents)
 - Potential for traffic signals (9% of respondents)
 - All others marked no concerns (45% of respondents)



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Open House 2

Open House 2 was held at the Town of Blue River Town Hall, 0110 Whispering Pines Circle, Blue River, CO. A staff preview for CDOT and local agency staff was held between 3:00pm and 4:00pm. The public open house was held from 4:00pm to 7:30pm. A total of 38 people attended the open house. As with Open House 1, display boards showing the updated access plan and conceptual trail design were available for viewing. Changes that were made since the last open house were highlighted on the plan with pink arrows on the display boards. Comment forms were collected in-person and through email. Survey Monkey was available, but no comments were received through Survey Monkey. A total of two comment forms were filled out using the paper form. The comments received are summarized below.

Comment Summary

- 50% of respondents support the consolidation of access throughout the corridor
- 50% of respondents support the Conceptual Trail Plan
- Concerns:
 - Private property encroachment
 - Converting portions of private roads to public roads for access improvements

Attachments

Sign-In Sheets

Completed Comment Forms

Sticky notes from open house boards (Open House 1)

CO 9 SOUTH SUMMIT ACCESS PLAN
 Blue River, Colorado
 SIGN-IN SHEET
 May 15, 2019

Name	Representing	E-mail or Mailing Address
Marilyn Cross	CDOT	marilyn_cross@state.co.us
Michelle Eddy	Blue Killy	michelle@townofblueriver.org
TYLER BROOK	BLUE RIVER	BROOK.BWEAVER@GMAIL.COM
Dan Cleary	TOWN OF BLUE RIVER	dan.blueriver@gmail.com
Jim Cernutte	Summit County	Jim.Cernutte@SummitCountyCO.gov
Don Reimer	Summit County	Don.Reimer@summitcountyCO.gov
BEATLEY HENDERSON	Summit County	BEATLEY.HENDERSON@summitcountyCO.gov
Scott Jackman	Town of Breckenridge	sjackman@townofbreckenridge.com
Shannon Smith	Town of Breckenridge	Shannons@townofbreckenridge.com
Michael Wurzel	Summit County Open Space Trails	michael.wurzel@summitcountyco.gov
Brian Lorch	Summit County Open Space	Brian.Lorch@summitcountyco.gov
Rendolph Johnson	Blue River Resident	rijohn81114@aol.com
MAT BENEDICT	BLUE RIVER RES.	mbenedict2003@yahoo.com
JAMES SCHEPPER S	BLUE RIVER	schepperslj@yahoo.com
SLAUBERTER, T.	Blue River	asl10000ft@hotmail.com
MARK PROVINO	Provino Architecture / L.B.T.B.	MARK@PROVINOARCHITECTURE.COM
Toby Babich	TOBR	Toby@rmlodge.com
Lyn da Tenhundfeld	TOBR - resident	E.tenhundfelds@gmail.com
KATHY SLAUGHTER	BRECK RESIDENT	VORRESITES@OUTLOOK.COM
TED MUELLER	SUMMIT CO RES.	THEOWILLIAMMUELLER@GMAIL.COM
Tom Gosiorowski	Summit County	tom.gosiorowski@summitcountyco.gov
Nathy Nattress + Dennis Nattress	Summit Blue River	dennis.nattress@gmail.com
Graciela Harris	Summit Blue River	stanleywayneharris@gmail.com
BEVANN HARDY	BLUE RIVER	BEVANN.H@SHOTMAKERSPHOTOGRAPHY.COM
JANE HARDY	BLUE RIVER	Jane.H@shotmakersphotography.com
CURTIS GARNER	Summit County	CURTIS.GARNER@SUMMITCOUNTYCO.GOV
Greg J. Giske	Blue River	greg@hotmail.com
PHILIP HOUGHTON	BRECKENRIDGE	theashers@comcast.net
Carrie Asher	Blue River	wiley.asher@comcast.net
Viley Asher	Blue River	
CHRISTIE LEIDAL	LODGE BY THE BLUE	CAO.MATHENSLEIDAL.COM
SEBASTIAN NIEMKIEWICZ	BLUE RIVER	SEBASTIANNIEMKIEWICZ@GMAIL.COM

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CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE

May 15, 2019

Name: Graciela Harris Representing: My Family
Address: 5475 Hwy 9 City: Blue River State: CO Zip: _____
Phone: ³⁶⁰ 460-2441 Fax: NONE Email: stanleywayneharris@gmail

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- | | |
|--|---|
| <input checked="" type="checkbox"/> Property Owner on CO 9 | <input checked="" type="checkbox"/> ^{work} Commuter through corridor |
| <input type="checkbox"/> Renter/Lessee on CO 9 | <input type="checkbox"/> Recreational Commuter through corridor |
| <input type="checkbox"/> Business Owner on CO 9 | <input type="checkbox"/> Other _____ |

2. Of the following community issues, please mark up to three that are most important to you.

- | | |
|---|---|
| <input checked="" type="checkbox"/> Safety | <input type="checkbox"/> Bicycle Trail Access |
| <input checked="" type="checkbox"/> Mobility through the corridor | <input type="checkbox"/> Property Impacts |
| <input type="checkbox"/> Driveway Access | <input type="checkbox"/> Wildlife |
| <input checked="" type="checkbox"/> Pedestrian Trail Access | <input type="checkbox"/> Transit |

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor - #1
- Accessing my property/business from a side/county road and not from CO 9
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property #2

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: _____
- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____
- I do **not** support consolidation of access throughout the corridor

Please continue on the back

7. Do you have any other comments, questions, or concerns?

I would like to see safe walking paths between Blue River and Preek, and pull-off sections for school buses so traffic is not delayed on the highway.

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Thanks, Appreciate your efforts to make access safer and more efficient.

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating

**CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE
May 15, 2019**

Name: Melanie Benedict Representing: _____

Address: 10011 Highway 9 City: Breck State: CO Zip: 80424

Phone: 970-333-9830 Fax: _____ Email: MNBenedict@yahoo.com

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- | | |
|--|---|
| <input checked="" type="checkbox"/> Property Owner on CO 9 | <input type="checkbox"/> Commuter through corridor |
| <input type="checkbox"/> Renter/Lessee on CO 9 | <input type="checkbox"/> Recreational Commuter through corridor |
| <input type="checkbox"/> Business Owner on CO 9 | <input type="checkbox"/> Other _____ |

2. Of the following community issues, please mark up to three that are most important to you.

- | | |
|--|---|
| <input checked="" type="checkbox"/> Safety | <input type="checkbox"/> Bicycle Trail Access |
| <input type="checkbox"/> Mobility through the corridor | <input checked="" type="checkbox"/> Property Impacts → <i>how does it affect car septic</i> |
| <input checked="" type="checkbox"/> Driveway Access | <input type="checkbox"/> Wildlife |
| <input type="checkbox"/> Pedestrian Trail Access | <input type="checkbox"/> Transit |

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor
- Accessing my property/business from a side/county road and not from CO 9 *n/a*
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: _____
- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____
- I do **not** support consolidation of access throughout the corridor

Please continue on the back

7. Do you have any other comments, questions, or concerns?

*Speed limits, how close the path is to accident areas

* our septic field is in front of our house would this affect it?

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating

**CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE
May 15, 2019**

Name: Matt Benedict Representing: BLUE RIVER
Address: 6011 HWY 9 City: BLUE RIVER State: CO Zip: 80424
Phone: 970-390-9630 Fax: _____ Email: mbenedict2003@yahoo.com

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- | | |
|--|---|
| <input checked="" type="checkbox"/> Property Owner on CO 9 | <input type="checkbox"/> Commuter through corridor |
| <input type="checkbox"/> Renter/Lessee on CO 9 | <input type="checkbox"/> Recreational Commuter through corridor |
| <input type="checkbox"/> Business Owner on CO 9 | <input type="checkbox"/> Other _____ |

2. Of the following community issues, please mark up to three that are most important to you.

- | | |
|--|--|
| <input checked="" type="checkbox"/> Safety | <input checked="" type="checkbox"/> Bicycle Trail Access |
| <input type="checkbox"/> Mobility through the corridor | <input type="checkbox"/> Property Impacts |
| <input checked="" type="checkbox"/> Driveway Access | <input type="checkbox"/> Wildlife |
| <input type="checkbox"/> Pedestrian Trail Access | <input type="checkbox"/> Transit |

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor
- Accessing my property/business from a side/county road and not from CO 9
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: _____
- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____
- I do **not** support consolidation of access throughout the corridor

7. Do you have any other comments, questions, or concerns?

I HAVE CONCERNS ABOUT THE SPEED LIMIT AND
WOULD LIKE IT REDUCED. I FEEL THAT THE
BIKE PATH IS GREAT.

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating

CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE

May 15, 2019

Name: CHASE PATTERSON Representing: _____

Address: 43 LEAP YEAR TRAIL City: _____ State: _____ Zip: _____

Phone: 601 512 1720 Fax: _____ Email: _____

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- | | |
|--|---|
| <input checked="" type="checkbox"/> Property Owner on CO 9 | <input type="checkbox"/> Commuter through corridor |
| <input type="checkbox"/> Renter/Lessee on CO 9 | <input type="checkbox"/> Recreational Commuter through corridor |
| <input type="checkbox"/> Business Owner on CO 9 | <input type="checkbox"/> Other _____ |

2. Of the following community issues, please mark up to three that are most important to you.

- | | |
|---|--|
| <input checked="" type="checkbox"/> Safety | <input checked="" type="checkbox"/> Bicycle Trail Access |
| <input type="checkbox"/> Mobility through the corridor | <input checked="" type="checkbox"/> Property Impacts |
| <input checked="" type="checkbox"/> Driveway Access | <input type="checkbox"/> Wildlife |
| <input checked="" type="checkbox"/> Pedestrian Trail Access | <input checked="" type="checkbox"/> Transit |

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor
- Accessing my property/business from a side/county road and not from CO 9
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: IMPACT TO OUR SIDE OF THE HIGHWAY
IMPACT ON LEAP YEAR TRAIL RESIDENCES
- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____
- I do **not** support consolidation of access throughout the corridor

Please continue on the back

7. Do you have any other comments, questions, or concerns?

I WOULD LIKE THE PATHWAY MORE IF IT WAS ON THE EAST SIDE OF HWY 9

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating

CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE

May 15, 2019

Name: Daniel Dunn Representing: _____

Address: _____ City: _____ State: _____ Zip: _____

Phone: _____ Fax: _____ Email: _____

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- Property Owner on CO 9
- Renter/Lessee on CO 9
- Business Owner on CO 9
- Commuter through corridor
- Recreational Commuter through corridor
- Other _____

2. Of the following community issues, please mark up to three that are most important to you.

- Safety
- Mobility through the corridor
- Driveway Access
- Pedestrian Trail Access
- Bicycle Trail Access
- Property Impacts
- Wildlife
- Transit

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor
- Accessing my property/business from a side/county road and not from CO 9
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: _____
- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____
- I do **not** support consolidation of access throughout the corridor

* OVER *

7. Do you have any other comments, questions, or concerns?

I am highly in favor of this. Let's get it moving.

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating

CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE

May 15, 2019

Name: Randolph Johnson Representing: Blm River ^{6/55}
Address: 1336 S. Milwaukee St City: Denver State: CO Zip: 80210
Phone: 303.641.1765 Fax: _____ Email: rjohn81114@aol.com

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- Property Owner on CO 9
- Renter/Lessee on CO 9
- Business Owner on CO 9
- Commuter through corridor
- Recreational Commuter through corridor
- Other _____

2. Of the following community issues, please mark up to three that are most important to you.

- Safety
- Mobility through the corridor
- Driveway Access
- Pedestrian Trail Access
- Bicycle Trail Access
- Property Impacts
- Wildlife
- Transit

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor *NOT GOOD - see pictures*
- Accessing my property/business from a side/county road and not from CO 9
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: _____
- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____
- I do **not** support consolidation of access throughout the corridor

7. Do you have any other comments, questions, or concerns?

Pictures enclosed to depict:

1. Telephone pole between the two driveways

2. My drive way is paved and I ^{have} owned this house for 15 years. Property "80" is a rental property and has different renters every year. Each year can be as many as 6-8 or more cars. That drive is muddy with puddles.

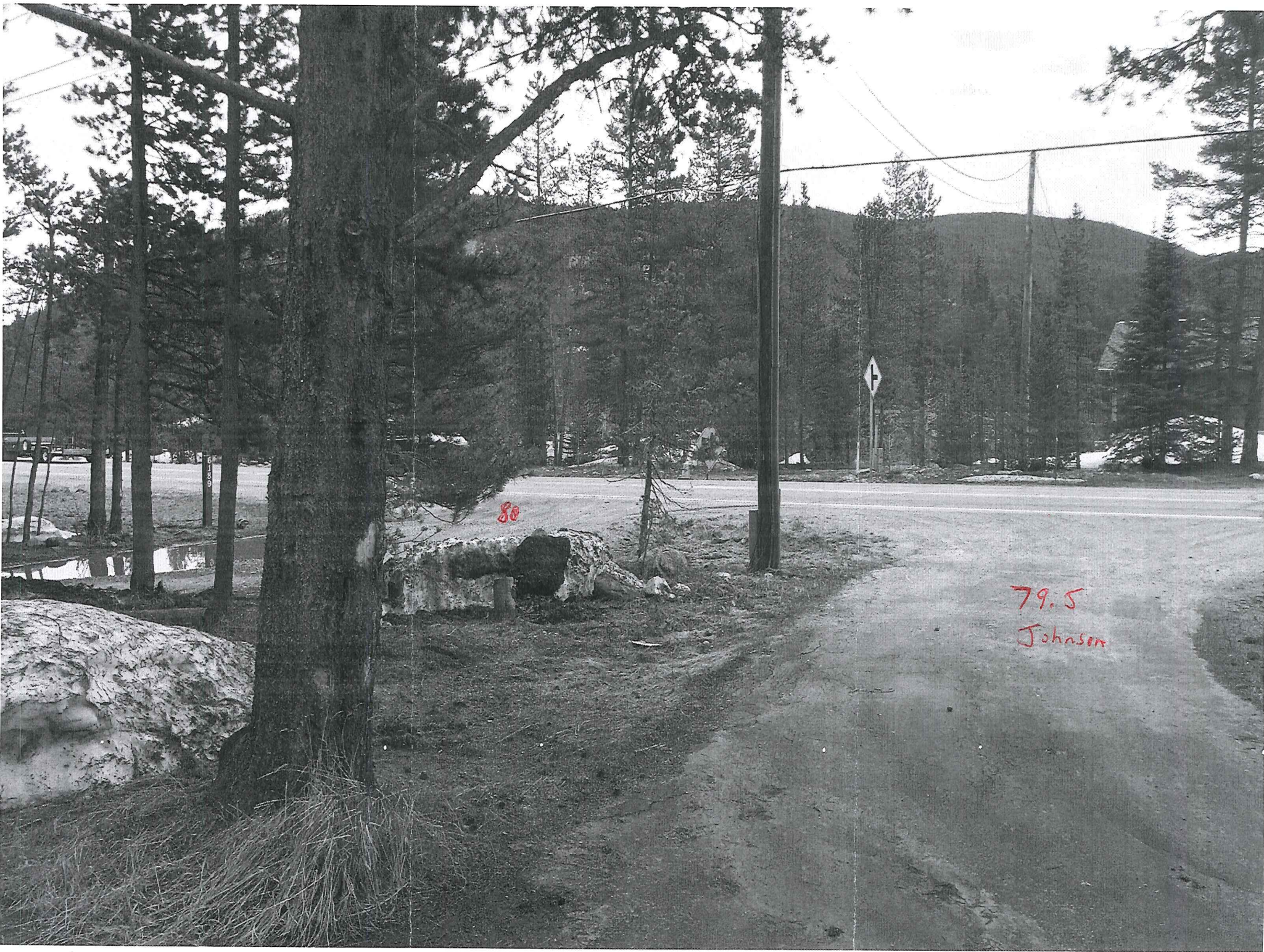
I will not share "access points", absolutely zero benefit to me.

3. Will CDOT be placing the new access points?

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating



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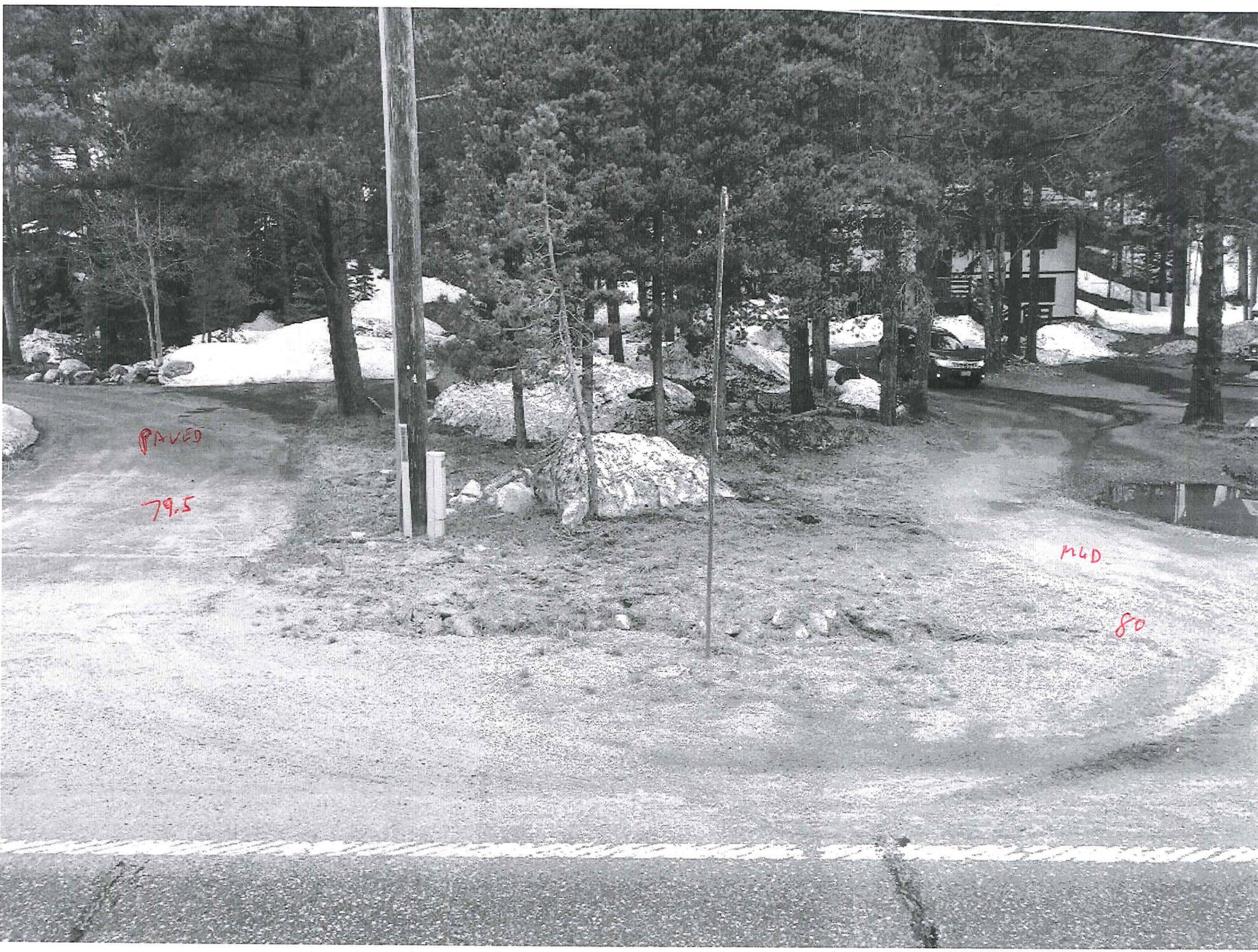
79.5
Johnson

PAVED

79.5

MUD

80





CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE

May 15, 2019

Name: SEBASTIAN NIEMKIEWICZ Representing: BLUE REVER

Address: 14 ROCK SPRING RD City: BLUERIVER State: CO Zip: 80424

Phone: 9703904688 Fax: _____ Email: SEBASTIAN NIEMKIEWICZ

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplan>

1. Are you a (check all that apply):

- | | |
|--|---|
| <input checked="" type="checkbox"/> Property Owner on CO 9 | <input checked="" type="checkbox"/> Commuter through corridor |
| <input type="checkbox"/> Renter/Lessee on CO 9 | <input type="checkbox"/> Recreational Commuter through corridor |
| <input type="checkbox"/> Business Owner on CO 9 | <input type="checkbox"/> Other _____ |

2. Of the following community issues, please mark up to three that are most important to you.

- | | |
|---|--|
| <input checked="" type="checkbox"/> Safety | <input type="checkbox"/> Bicycle Trail Access |
| <input type="checkbox"/> Mobility through the corridor | <input checked="" type="checkbox"/> Property Impacts |
| <input type="checkbox"/> Driveway Access | <input type="checkbox"/> Wildlife |
| <input checked="" type="checkbox"/> Pedestrian Trail Access | <input type="checkbox"/> Transit |

3. What are some of your concerns regarding the consolidation of access points? (check all that apply)

- Sharing access with my neighbor
- Accessing my property/business from a side/county road and not from CO 9
- Modification of circulation on my property
- Reducing the number of access points to my property
- Relocation of access point on my property

4. Overall, how do you feel about the Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____

- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

5. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: ROCK SPRINGS RD. AND BLUEROCK RD

- Do **not** support conceptual trail plan

6. What statement best reflects how you feel about the consolidation of access shown in the plan?

- I support consolidation of access throughout the corridor
- I support consolidation of access, but have concerns about access at particular locations. Please note those areas: _____

- I do not support consolidation of access throughout the corridor

7. Do you have any other comments, questions, or concerns?

PRIVET PROPERTY IMPACT REDUC IN VALUE!
I WILL REDUCING SPEED TRUE TOWN OF BLUERIVER!

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **May 31st** to:
Jenna Siegel, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) jenna@stolfusandassociates.com

Thank you for participating

#1

COMPLETE

Collector: Web Link 1 (Web Link)
Started: Thursday, May 16, 2019 3:58:44 PM
Last Modified: Thursday, May 16, 2019 4:23:27 PM
Time Spent: 00:24:43
IP Address: 67.177.216.246

Page 1

Q1 Are you a (check all that apply): **Property Owner on CO 9**

Q2 Of the following community issues, please mark up to three that are most important to you. **Driveway Access, Bicycle Trail Access, Property Impacts**

Q3 What are some of your concerns regarding the consolidation of access points? If you have more than one concern, please list them in the "other" box below: **Sharing access with my neighbor**

Q4 Overall, how do you feel about the Access Control Plan? **Do not support consolidation of access throughout the corridor, Support major intersection locations**

Q5 Overall, how do you feel about the Conceptual Trail Plan? **Support conceptual trail plan**

Q6 What statement best reflects how you feel about the consolidation of access shown in the plan? **Do not support consolidation of access throughout the corridor**

Q7 Do you have any other comments, questions, or concerns?

I'm assuming CDOT will be responsible for snow removal on consolidated driveways. Otherwise there will be no access onto the highway at all.
 I feel that the same number of cars will be accessing the highway regardless of number of access points. Lower the speed limit and get Blue River Police watching for speeders. Especially those drivers doing 60 mph plus on their way to Alma at the end of the day.
 I support turn lanes at major intersections.

#2

COMPLETE

Collector: Web Link 1 (Web Link)
Started: Wednesday, May 29, 2019 11:55:19 AM
Last Modified: Wednesday, May 29, 2019 11:58:13 AM
Time Spent: 00:02:54
IP Address: 67.177.216.246

Page 1

Q1 Are you a (check all that apply): **Property Owner on CO 9**

Q2 Of the following community issues, please mark up to three that are most important to you. **Safety, Driveway Access, Bicycle Trail Access**

Q3 What are some of your concerns regarding the consolidation of access points? If you have more than one concern, please list them in the "other" box below: **Sharing access with my neighbor**

Q4 Overall, how do you feel about the Access Control Plan? **Do not support consolidation of access throughout the corridor**

Q5 Overall, how do you feel about the Conceptual Trail Plan? **Support conceptual trail plan**

Q6 What statement best reflects how you feel about the consolidation of access shown in the plan? **Do not support consolidation of access throughout the corridor**

Q7 Do you have any other comments, questions, or concerns?

Will CDOT plow the access points?

#3

COMPLETE

Collector: Web Link 1 (Web Link)
Started: Thursday, May 30, 2019 9:57:39 AM
Last Modified: Thursday, May 30, 2019 10:00:41 AM
Time Spent: 00:03:02
IP Address: 73.170.0.152

Page 1

Q1 Are you a (check all that apply): **Property Owner on CO 9**

Q2 Of the following community issues, please mark up to three that are most important to you.

- Mobility through the corridor** ,
- Pedestrian Trail Access** ,
- Bicycle Trail Access**

Q3 What are some of your concerns regarding the consolidation of access points? If you have more than one concern, please list them in the "other" box below:

Other (please specify):
 Traffic lights will negatively alter the character of our community. I prefer the small, rural character we have always enjoyed in Blue River.

Q4 Overall, how do you feel about the Access Control Plan? **Do not support major intersection locations**

Q5 Overall, how do you feel about the Conceptual Trail Plan? **Support conceptual trail plan**

Q6 What statement best reflects how you feel about the consolidation of access shown in the plan? **Do not support consolidation of access throughout the corridor**

Q7 Do you have any other comments, questions, or concerns? **Respondent skipped this question**

#4

COMPLETE

Collector: Web Link 1 (Web Link)
Started: Thursday, May 30, 2019 10:00:25 AM
Last Modified: Thursday, May 30, 2019 10:02:08 AM
Time Spent: 00:01:43
IP Address: 70.89.162.141

Page 1

Q1 Are you a (check all that apply):

Other (please specify):
 Public transit operator along Hwy 9 corridor

Q2 Of the following community issues, please mark up to three that are most important to you.

Safety,
Pedestrian Trail ,
Access
Transit

Q3 What are some of your concerns regarding the consolidation of access points? If you have more than one concern, please list them in the "other" box below:

Other (please specify):
 I have no concerns about consolidating access points.

Q4 Overall, how do you feel about the Access Control Plan?

Support consolidation of access throughout the corridor

Q5 Overall, how do you feel about the Conceptual Trail Plan?

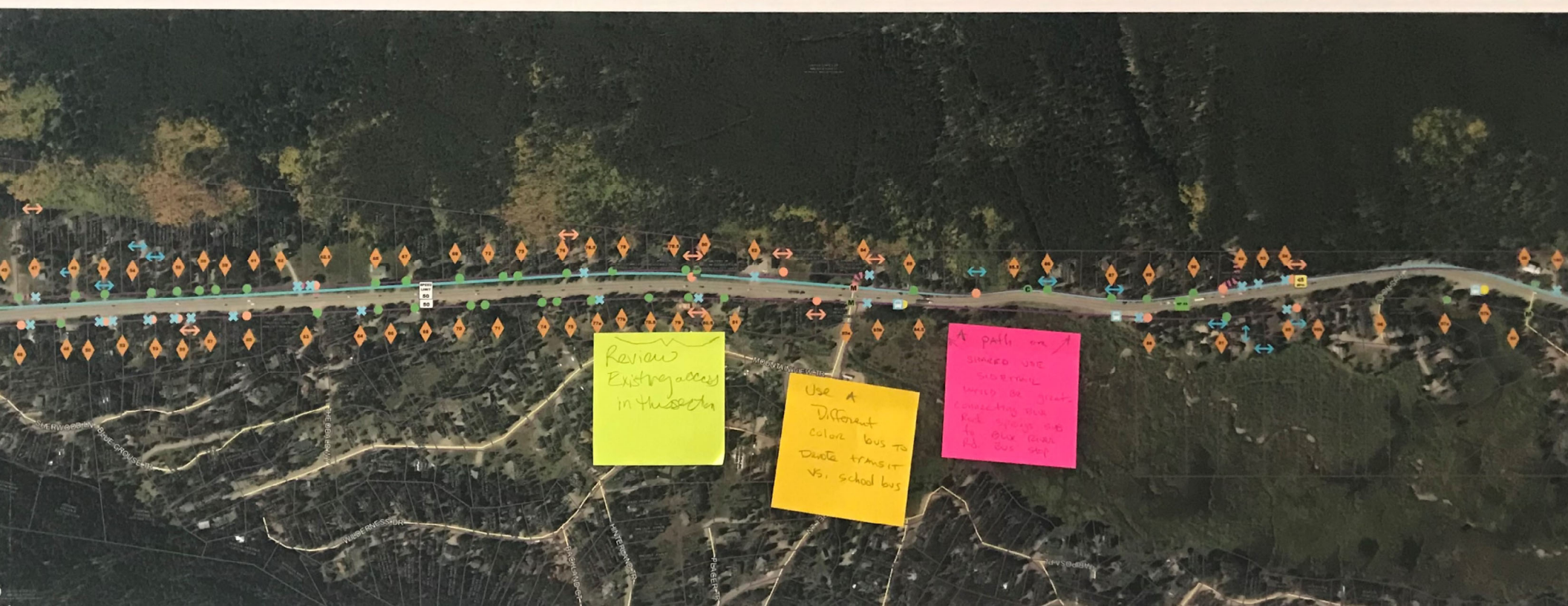
Support conceptual trail plan

Q6 What statement best reflects how you feel about the consolidation of access shown in the plan?

Support consolidation of access throughout the corridor

Q7 Do you have any other comments, questions, or concerns?

Please make this corridor more safe and friendly for pedestrians and cyclists!





Proposed Wildlife Overpass

SPEED LIMIT 50

SPEED LIMIT 50

QUANDARY RD

TIMBER CT

NOT A LEGITIMATE ACCESS

26
27

28

30

32

29a

29b

31

SUMMIT CITY BRD
OR COMMISSIONERS
2371-3120-17-001

QUANDARY TOWN
2371-3610-03-001

QUANDARY BRIDGE
CONDOR 2371-3120-01-017

CABIN PROPERTIES LLC
2373-1610-06-004

CMJ PROPERTIES LLC
2371-3120-03-001

PETER ROBERT
2371-3120-16-005

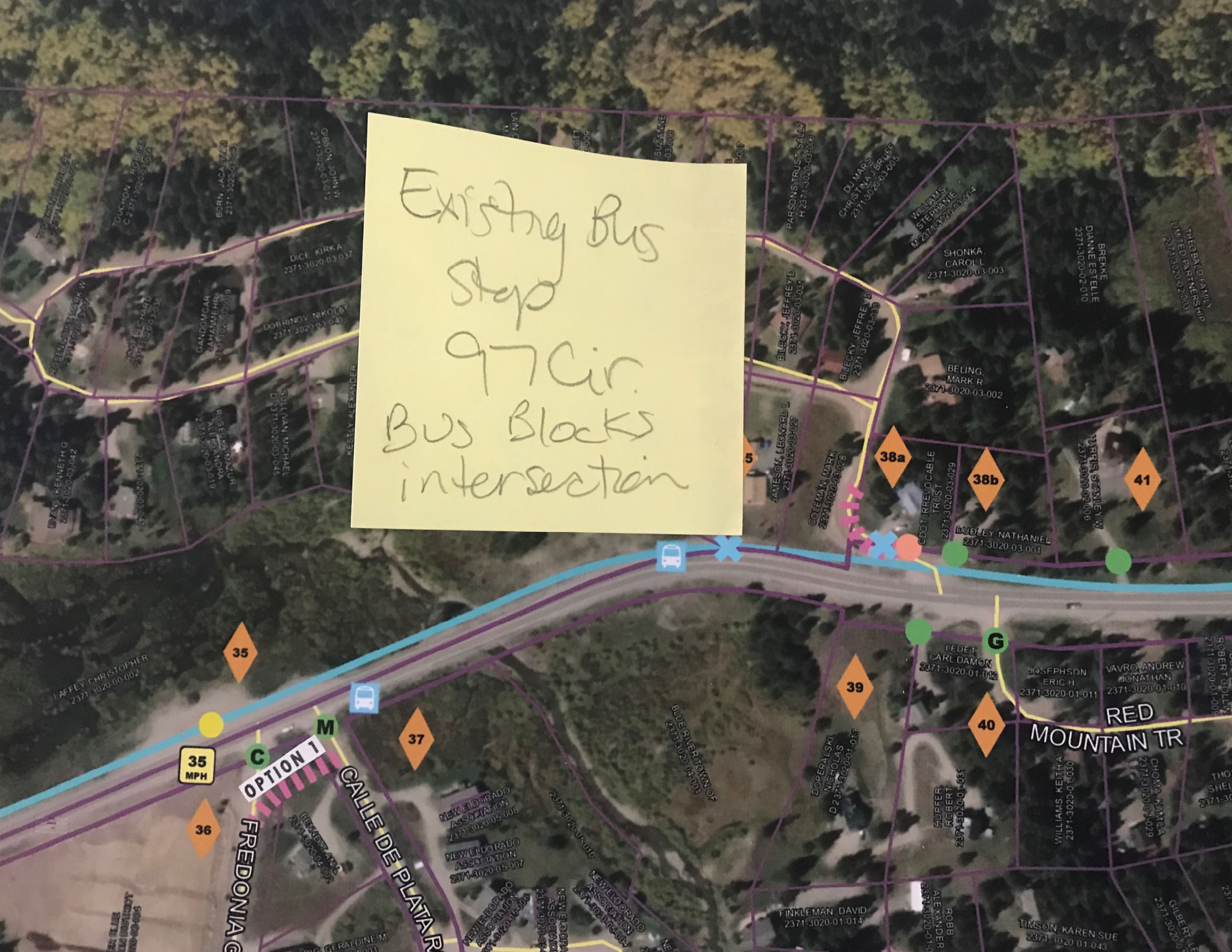
HENNES VIRGINIA H
2371-3120-09-008

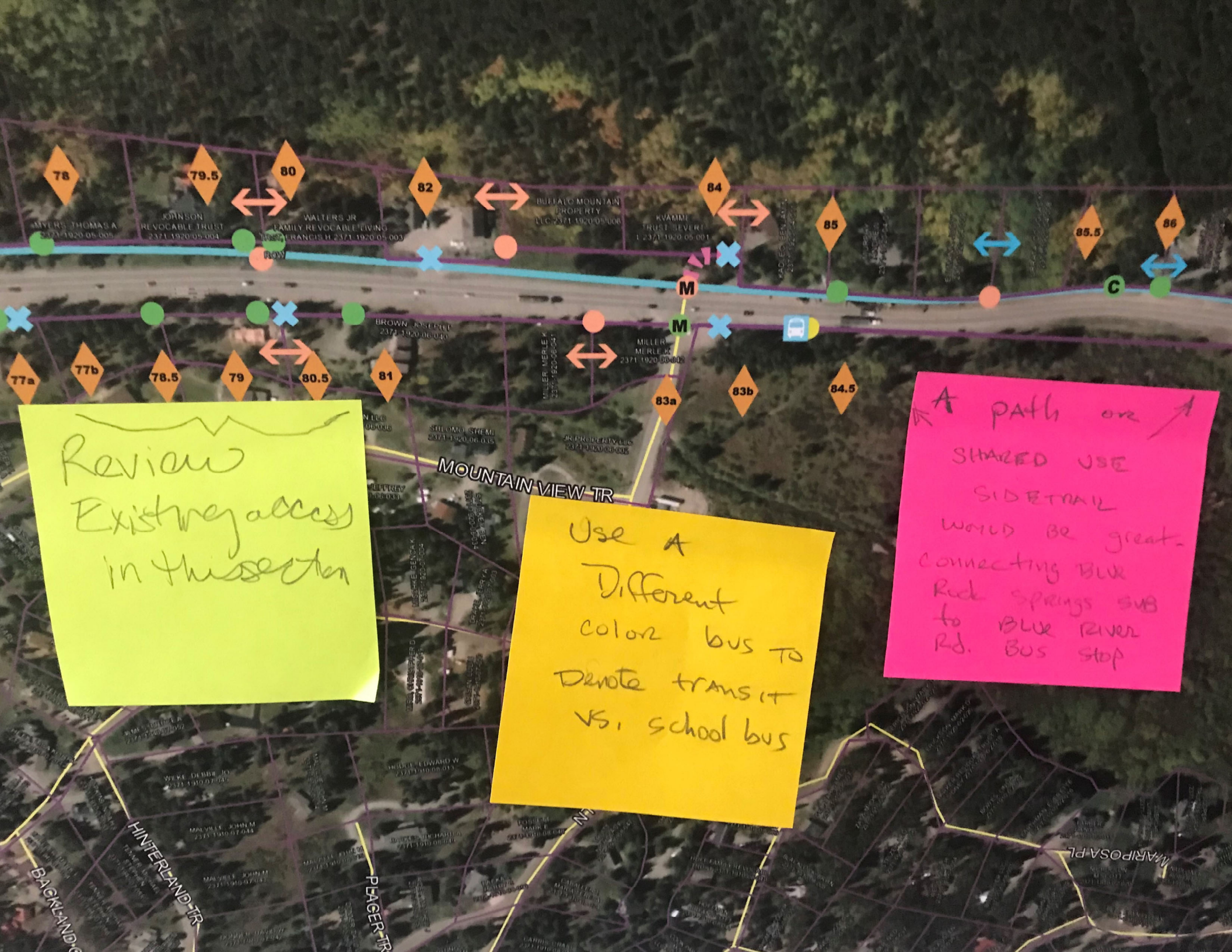
DONALD ANDREW
2371-3120-01-001

750 1,000 Feet

N

Existing Bus
Stop
977 Cir.
Bus Blocks
intersection





Review
Existing access
in this section

Use A
Different
color bus to
denote transit
vs. school bus

A path or
SHARED USE
SIDE TRAIL
would be great
connecting BLUE
Ridge Springs SUB
to BLUE RIVER
RD. BUS STOP

CO 9 SOUTH SUMMIT ACCESS PLAN
 Blue River, Colorado
 SIGN-IN SHEET
 September 25, 2019

Name	Representing	E-mail or Mailing Address
Rob Oderman		alife4all@gmail.com
Mike Ross		Zmike.ross@gmail.com
TOM MYERS		6115 Hwy 9
Fandy Johnson		6155 Hwy 9
Scott Van Rensholst		Scott@adcoproperties.com
RANBY NATIONS		RANBYN@AOL.COM
Ray Douglas		rdouglas@jacks.com
MAT BENEDICT		
Brian Lorch		on file
Joel Dixon		on file
EMMA DIXON		EMMA DIXON
Travis Bennett		travisbennett@comcast.net
Ani Bennett		anibennett@gmail.com
John Peterson		John@skitip.net
Richard LYSTLUND		5723 Hwy 9
Joyce Mueller		joycemueller@gmail.com
TED MUELLER		TEOWILLIAMMUELLER@GMAIL.COM
Jeremy Slaughter		slthouse@gmail.com
JAMES SCHEPPERS		schepperslj@yahoo.com
Don Sobania		SUBAMTHO@gmail.com
Darcy E. Lystlund		darcy_L@msn.com
Nathan Dudley		897 Cir. Blue River Co 80424
Miyuki Takada		letit snow.mtkeda@gmail.com
Barb Tabb		barbtabb@comcast.net
Steve Cross		stcross1@yahoo.com
Jen + Sean Gallagher		tenthousandft@comcast.net
Dave Cleary	TRUSTEE TOWN of Blue River	on file
Pauline Stein	RIO AZUL	thesteins5@gmail.com
David and Leisa Jungers	Rio Azul	ljungers@yahoo.com djungers9596@gmail.com
Neil & Bob Rehner		bobrehner@gmail.com
Jack & Marlene Probasco		mprobasco@gmail.com
Dennis & Kathy Nattress		dennis.nattress@gmail.com

CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE
September 25, 2019

Beachfield

Name: Nathan Dudley Representing: _____

Address: 899 Circle City: Blue River State: CO Zip: 80424

Phone: 303 263 7756 Fax: _____ Email: youchero.ndudley@gmail.com

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplanOH2>

1. Are you a (check all that apply):

- Property Owner on CO 9
- Renter/Lessee on CO 9
- Business Owner on CO 9
- Commuter through corridor
- Recreational Commuter through corridor
- Other _____

2. Overall, how do you feel about the updated Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: Access points 38A, 38B
Proposed access point at 38A is on their current beachfield
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

3. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: _____
- Do **not** support conceptual trail plan

4. Do you have any comments, questions, or concerns?

If proposed mixed use trail is built, it should be made unlawful for bikes to use the road.

The Mixed use path also encroaches on our privacy.

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **October 11th** to:
Michelle Hansen, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) michelle@stolfusandassociates.com

**CO 9 SOUTH SUMMIT ACCESS PLAN
COMMENT SHEET - PUBLIC OPEN HOUSE
September 25, 2019**

Name: Angela Abeler - President Representing: McCullough Gulch Reserve LOA/HOA

Address: 6834 S University Blvd. Suite 430 City: Centennial State: CO Zip: 80122

Phone: 303.339.0118 Fax: _____ Email: angela.abeler@live.com

* This survey can also be completed online at: <https://www.surveymonkey.com/r/southsummitplanOH2>

1. Are you a (check all that apply):

- | | |
|--|---|
| <input checked="" type="checkbox"/> Property Owner on CO 9 | <input type="checkbox"/> Commuter through corridor |
| <input type="checkbox"/> Renter/Lessee on CO 9 | <input type="checkbox"/> Recreational Commuter through corridor |
| <input type="checkbox"/> Business Owner on CO 9 | <input type="checkbox"/> Other _____ |

2. Overall, how do you feel about the updated Access Control Plan?

- Support consolidation of access throughout the corridor
- Support consolidation of access throughout the corridor, but have concerns about access at particular locations. Please note particular areas of concern: _____
- Do **not** support consolidation of access throughout the corridor
- Support major intersection locations
- Do **not** support major intersection locations

3. Overall, how do you feel about the Conceptual Trail Plan?

- Support conceptual trail plan
- Support conceptual trail plan, but have concerns about the trail at particular locations. Please note particular areas of concern: I represent 72 acres of privately owned riverfront land that sits along Hwy 9. If a trailhead is installed alongside Hwy 9 against our acreage, a split rail log fence matching existing along our road must be built to prevent trespassing on private riverfront land.
- Do **not** support conceptual trail plan

4. Do you have any comments, questions, or concerns?

McCullough Gulch Reserve owners are strongly against consolidating our privately built and privately maintained asphalt road (Rio Azul Road) with the public dirt parking lot next to us. We have worked hard to eliminate hiking car traffic down our road. Cars may currently park in the dirt parking lot and walk to the hiking entrance which sits off Rio Azul Road. There is no vehicular parking right conferred on Rio Azul Road.

Should connection be pursued, owners would like to be a part of all conversations regarding scope, design and implementation from the beginning. Fair mediation would include 1) Installation of a gate and log arch feature (like existing log arch) to keep the remainder of the road private 2) Compensation for public usage and ongoing maintenance of portion of Rio Azul Road in front of gate 3) Construct connection to parking lot in asphalt matching road standards and including traffic signs. 4) Perform any ongoing repairs to the balance of the road 4) Construction of a log split rail fence between Rio Azul and Lot 1. Construct a log split rail fence between pedestrian trail and private acreage.

If you think of something else after you leave here, please leave a comment on our online comment tool at: www.co.summit.co.us/hwy9south

Please leave this with us, mail, or email by **October 11th** to:
Michelle Hansen, P.E. Stolfus & Associates, Inc.
5690 DTC Boulevard, Suite 560E, Greenwood Village, CO 80111
303.221.2330 (Phone) 303.221.2331 (Fax) michelle@stolfusandassociates.com

Please continue on the back if needed

Appendix B-Existing Access Inventory

Blue River - Existing Access Point Inventory
Carroll Ln (MP 77.50) to Broken Lance Dr (MP 86.26)

* Mile Posts defined per CDOT Windshield reference point marker beginning at MP 77.49

Access ID No.	Mile Post *	Description	Type	Side	Existing Configuration	Surface Material	Gate		Cross Culvert		Existing Condition Comments	Field Notes	Existing Access Permit					
							(Y/N)	Width	(Y/N)	Size			(Y/N)	Number	Date	Comments		
23	80.28	Forest Service	R	LT	Unsig. Full Movement	Gravel	N		N									
24a	80.30	CR 626 (Quandary Rd)	PRU	RT	Unsig. Full Movement	Gravel	N		N									
24b	80.31	Vehicle pull-off	FA	RT	Unsig. Full Movement	Asphalt	N		N		Vehicle pull-off							
25	80.40	Residential	R	RT	Unsig. Full Movement	Gravel	N		Y?									
26	80.43	Residential	R	RT	Unsig. Full Movement	Gravel	N		Y?									
27	80.47	Residential	R	LT	Unsig. Full Movement	Asphalt	N		Y?				Y	195029	5/17/1995	Left turn movements in and out of the driveway(s) may be prohibited at some future date.		
28	80.67	Parking for trailhead	FA	LT	Unsig. Full Movement	Gravel	N		N									
29a	80.690	Lodge by the Blue	BA	RT	Unsig. Full Movement	Asphalt	N		N									
29b	80.695	Lodge by the Blue	FA	RT	Unsig. Full Movement	Gravel	N		N		Seems like a pointless gate							
30	80.695	Rio Azul	PVRU	LT	Unsig. Full Movement	Asphalt	N		N		Also McCullough Gulch Reserve		Y	101081	2/5/2002	Left turn movements in and out of this access may be prohibited at some future date.	Installation of auxillary lane(s) (acceleration, deceleration and left turn lanes) and/or a traffic signal may be required in the future.	
													Y	196007	2/13/1996	Left turn movements in and out of the driveway(s) may be prohibited at some future date.		
31	80.84	Residential	R	RT	Unsig. Full Movement	Gravel	Y		N									
32	80.89	Vehicle pull-off	FA	LT	Unsig. Full Movement	Gravel	N		N		Vehicle pull-off							
	80.95	Milepost 81																
33	81.18	Whispering Pines Circle	PRU	RT	Unsig. Full Movement	Asphalt	N		N									
34	81.33	CR 590 (Silverheels Rd)	PRU	RT	Unsig. Full Movement	Asphalt	N		N				Y	191014	6/11/1991			
34.5	81.33	Field access	FA	LT	Unsig. Full Movement	Grass	N		N									
35	81.53	Vehicle pull-off	FA	LT	Unsig. Full Movement	Gravel	N		N		Vehicle pull-off							
36	81.56	CR 600 (Fredonia Gulch Rd)	PRU	RT	Unsig. Full Movement	Gravel	N		N									
37	81.57	CR 589 (Calle De Plata)	PRU	RT	Unsig. Full Movement	Asphalt	N		N									
38.5	81.69	Vehicle pull-off	FA	LT	Unsig. Full Movement	Gravel	N		N									
38a	81.750	CR 804 (97 Cir)	PRU	LT	Unsig. Full Movement	Asphalt	N		N									
38b	81.76	Residential	R	LT	Unsig. Full Movement	Gravel	N		N									
39	81.755	Residential	R	RT	Unsig. Full Movement	Gravel	N		N									
40	81.77	Red Mountain Trail	PVRU	RT	Unsig. Full Movement	Gravel	N		N									
41	81.81	Residential	R	LT	Unsig. Full Movement	Gravel	N		N									
42	81.84	Residential	R	RT	Unsig. Full Movement	Asphalt	N		N				Y	195058	8/29/1995			
43	81.86	Residential	R	LT	Shared Unsig. Full Movement	Gravel	N		Y				Y	198088	3/16/1999			

Blue River - Existing Access Point Inventory
Carroll Ln (MP 77.50) to Broken Lance Dr (MP 86.26)

* Mile Posts defined per CDOT Windshield reference point marker beginning at MP 77.49

Access ID No.	Mile Post *	Description	Type	Side	Existing Configuration	Surface Material	Gate		Cross Culvert		Existing Condition Comments	Field Notes	Existing Access Permit					
							(Y/N)	Width	(Y/N)	Size			(Y/N)	Number	Date	Comments		
92	83.050	Residential	R	LT	Unsig. Full Movement	Gravel	N		N									
	83.055	Milepost 83																
93	83.06	Residential	R	LT	Unsig. Full Movement	Gravel	N		N		Connected to Access ID 94							
94	83.070	Residential	R	LT	Unsig. Full Movement	Gravel	N		N		Connected to Access ID 93							
95a	83.075	Residential	R	RT	Unsig. Full Movement	Asphalt	N		N				Y	102011	3/11/2002	Address: 6700 State Highway 9, Breckenridge, CO 80424		
96	83.11	CR 603 (Rustic Terrace Rd)	PRU	RT	Unsig. Full Movement	Asphalt	N		N				Y	195030	7/7/1995	Left turn movements in and out of the driveway(s) may be prohibited at some future date.		
97a	83.16	Residential	R	RT	Unsig. Full Movement	Gravel	N		N									
97b	83.17	Vehicle pull-off	FA	RT	Unsig. Full Movement	Gravel	N		N		Kind of an extended pulloff right in front of their driveway							
98	83.20	Residential	R	LT	Unsig. Full Movement	Gravel	N		N				Y	195021	5/5/1995	Address: 6871 State Highway 9, Breckenridge, CO 80424 Left turn movements in and out of the driveway(s) may be prohibited at some future date.		
99	83.21	CR 579 (Blue Rock Dr)	PRU	RT	Unsig. Full Movement	Dirt	N		N									
100	83.22	Residential	R	LT	Unsig. Full Movement	Gravel	N		N									
101	83.26	CR 578 (Leap Year Trail)	PRU	LT	Unsig. Full Movement	Gravel	N		N									
102	83.28	Residential	R	RT	Unsig. Full Movement	Gravel	N		N				Y	107021	6/1/2007	Address: 6994 State Highway 9, Breckenridge, CO 80424		
103	83.33	Residential	R	RT	Unsig. Full Movement	Gravel	N		N									
104	83.38	CR 577 (Rock Springs Rd)	PRU	RT	Unsig. Full Movement	Gravel	N		N									
105	83.43	Residential	R	LT	Unsig. Full Movement	Gravel	N		N									
106	83.44	Residential	R	RT	Unsig. Full Movement	Gravel	N		N				Y	193016	5/14/1993	Left turn movements in and out of the driveway(s) may be prohibited at some future date.		
107	83.54	Conifer Dr	PVRU	LT	Unsig. Full Movement	Gravel	N		N		Appears to be called Conifer Rd							
108	83.62	Vehicle pull-off	FA	RT	Unsig. Full Movement	Gravel	N		N									
108.5	83.80	Vehicle pull-off	FA	LT	Unsig. Full Movement	Gravel	N		N									
	83.94	Milepost 84																
109	83.98	CR 800 (Spruce Creek Rd)	PRU	LT	Unsig. Full Movement	Asphalt	N		N				Y	315130	8/25/2015			
110	84.22	Parking for Goose Pasture Tarn	FA	RT	Unsig. Full Movement	Gravel	N		N									
111	84.30	Field access	FA	LT	Unsig. Full Movement	Gravel/Dirt	Y		N		Appears unused and chained off							
112	84.44	Vehicle pull-off	FA	RT	Unsig. Full Movement	Gravel	N		N		Vehicle pull-off							
112.5	84.55	Vehicle pull-off	FA	LT	Unsig. Full Movement	Gravel	N		N									
113	84.58	CR 571 (Lakeshore Loop)	PRU	RT	Unsig. Full Movement	Asphalt/Gravel	N		N				Y	316030	5/2/2016			
114	84.58	CR 574 (Crown Dr)	PRU	LT	Unsig. Full Movement	Asphalt/Gravel	N		N				Y	187084	11/13/1987			
115	84.63	Residential	R	LT	Unsig. Full Movement	Gravel	N		N				Y	192062	10/22/1992			

**Blue River - Existing Access Point Inventory
Carroll Ln (MP 77.50) to Broken Lance Dr (MP 86.26)**

* Mile Posts defined per CDOT Windshield reference point marker beginning at MP 77.49

Access ID No.	Mile Post *	Description	Type	Side	Existing Configuration	Surface Material	Gate		Cross Culvert		Existing Condition Comments	Field Notes	Existing Access Permit				
							(Y/N)	Width	(Y/N)	Size			(Y/N)	Number	Date	Comments	
116	84.64	Rivershore Dr	PVRU	RT	Unsig. Full Movement	Asphalt	N		N								
117	84.87	CR 790 (Green Mountain Dr)	PRU	LT	Unsig. Full Movement	Asphalt	N		N								
118	84.87	CR 550 (Horizon Ln)	PRU	RT	Unsig. Full Movement	Asphalt	N		Y								
	85.00	Milepost 85															
119	85.08	CR 11 (Wagon Rd)	PRU	RT	Unsig. Full Movement	Asphalt	N		N								
120	85.11	Utility access/Vehicle Pull-off	FA	RT	Unsig. Full Movement	Gravel	N		N								
121	85.29	Vehicle pull-off	FA	LT	Unsig. Full Movement	Gravel	N		N		Vehicle pull-off						
122	85.52	CR 789 (Alpenview Rd)	PRU	RT	Unsig. Full Movement	Asphalt/Gravel	N		N				Y	108050	9/3/2008		
	85.98	Milepost 86															
123	86.00	Southside Dr	PVRU	RT	Unsig. Full Movement	Asphalt	N		Y								
124	86.00	CR 765 (River Park Dr)	PRU	LT	Unsig. Full Movement	Asphalt	N		Y				Y	313131	7/24/2014		
125	86.17	Broken Lance Dr	PRS	LT	Unsig. Full Movement	Asphalt	N		N		END OF PROJECT						
126	86.17	CR 10 (Boreas Pass Rd)	PRS	RT	Unsig. Full Movement	Asphalt	N		N								

Legend		
Access Type	Abbreviation	Google Earth kmz color
Business/Commercial Access	BA	Red
Field Access	FA	Yellow
Residential Access		Green
Public Road Signalized	PRS	Light Blue
Public Road Unsignalized	PRU	Dark Blue
Private Road Unsignalized	PVRU	White
Old Access Point		Black
Mile Post	MP	Pink
Speed Limit	SL	Orange

Appendix C-Crash History

CO-9C Crash Listing

#	Highway	MP	Date	Time	Severity	Location	Road Description	# of Vehicles	Road Contour	Road Condition	Lighting	Weather	Ramp	Accident Type	Direction	Vehicle Type	Drugs / Alcohol	Human Factor	Speed	Vehicle Movement	Violation Code
1	9C	77.5	01/13/2013	045	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN W/TRAILER	NO IMPAIRMENT	NONE APPARENT	7	OTHER	LEAVING SCENE OF AN ACCIDENT
2	9C	77.5	12/22/2013	223	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	SNOW/SLEET/THAL	DARK UNLIGHTED	NONE	N	ROAD MAINTENANCE EQUIPMENT	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	45	OTHER	CARELESS DRIVING
3	9C	77.5	11/16/2014	1545	PDO	ON	NON-INTERSECTION	3	CURVE ON GRADE	ICY	DAYLIGHT	SNOW/SLEET/THAL	N	REAR END	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	30	SLOWING	CARELESS DRIVING
4	9C	77.8	12/24/2014	1330	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON GRADE	WET	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	ALCOHOL/DRUGS	UNKNOWN	40	WEAVING	CARELESS DRIVING CAUSE INJURY
5	9C	77.8	12/24/2014	1353	INI	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	SNOW	DAYLIGHT	NONE	N	LEGITIMATE CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	40	OTHER	CARELESS DRIVING CAUSE INJURY
6	9C	77.8	12/24/2014	2114	INI	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	SNOW	DAYLIGHT	NONE	N	GUARD RAIL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	40	GOMG STRAIGHT	IMPROPER DRIVING ON MTH HWAY
7	9C	77.8	11/21/2015	1910	INI	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	SNOW	DARK UNLIGHTED	SNOW/SLEET/THAL	N	TREE SHURBERRY	N	SUV	NO IMPAIRMENT	NONE APPARENT	20	GOMG STRAIGHT	OTHER
8	9C	77.8	12/01/2013	1903	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNDER INFLUENCE ALCOHOL	30	PASSING	DRIVE UNDER INFLUENCE ALCOHOL
9	9C	78	10/13/2016	1928	PDO	ON	AT INTERSECTION	2	STRAIGHT ON GRADE	DRY	DARK UNLIGHTED	NONE	N	SIGN	S	SUV W/TRAILER	ALCOHOL	UNKNOWN	20	GOMG STRAIGHT	DEFECTIVE OR UNSAFE VEHICLE
10	9C	78.02	4/15/2015	1549	INO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	SIGN	N	TRUCK GVW > 10K/BUSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	25	GOMG STRAIGHT	OTHER
11	9C	78.2	4/12/2013	1829	PDO	OFF RIGHT	NON-INTERSECTION	2	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	PEDESTRIAN	N	HIT & RUN	NO IMPAIRMENT	NONE APPARENT	20	GOMG STRAIGHT	OTHER
12	9C	78.2	3/10/2012	1700	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	GUARD RAIL	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	15	GOMG STRAIGHT	OTHER
13	9C	78.4	9/27/2012	2000	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DARK UNLIGHTED	NONE	N	GUARD RAIL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNDER INFLUENCE ALCOHOL	60	GOMG STRAIGHT	CARELESS DRIVING
14	9C	78.4	1/25/2014	1445	INI	OFF RIGHT	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	ROAD MAINTENANCE EQUIPMENT	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	20	OTHER	IMPROPER DRIVING ON MTH HWAY
15	9C	78.4	2/24/2013	655	INO	ON	NON-INTERSECTION	2	CURVE ON GRADE	SNOW	DAWN OR DUSK	SNOW/SLEET/THAL	N	EMBANKMENT CUT/FILL SLOPE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	25	OTHER	IMPROPER DRIVING ON MTH HWAY
16	9C	78.5	11/18/2015	1629	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	HEAD-ON	N	SUV	NO IMPAIRMENT	NONE APPARENT	25	GOMG STRAIGHT	CARELESS DRIVING
17	9C	78.5	2/2/2013	1445	INI	ON	NON-INTERSECTION	3	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	REAR END	S	SUV	NO IMPAIRMENT	NONE APPARENT	30	OTHER	TOO FAST FOR CONDITIONS
18	9C	78.5	1/18/2015	1745	INO	ON	NON-INTERSECTION	3	CURVE ON GRADE	ICY	DARK UNLIGHTED	SNOW/SLEET/THAL	N	HEAD-ON	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	30	GOMG STRAIGHT	CARELESS DRIVING
19	9C	78.6	2/13/2015	1615	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	SIDEWIRE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	10	SLOWING	CARELESS DRIVING
20	9C	78.6	8/28/2014	2218	INO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	WET	DARK UNLIGHTED	NONE	N	GUARD RAIL	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	45	OTHER	CARELESS DRIVING
21	9C	78.7	3/11/2015	1400	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DARK UNLIGHTED	NONE	N	SIDEWIRE SAME DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	15	GOMG STRAIGHT	IMPROPER DRIVING ON MTH HWAY
22	9C	78.7	2/4/2014	1425	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	SNOW W/VS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/THAL	N	SIDEWIRE OPPOSITE DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	10	WEAVING	FAILED TO DRIVE IN SINGLE LANE
23	9C	78.7	11/12/2013	1657	PDO	OFF RIGHT	NON-INTERSECTION	2	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	20	GOMG STRAIGHT	TOO FAST FOR CONDITIONS
24	9C	78.7	7/18/2015	2230	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DARK UNLIGHTED	NONE	N	TREE SHURBERRY	N	SUV	NO IMPAIRMENT	UNKNOWN	40	OTHER	CARELESS DRIVING
25	9C	78.8	12/28/2014	1320	PDO	ON	INTERSECTION RELATED	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	HEAD-ON	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	10	OTHER	OTHER
26	9C	78.8	1/10/2013	1345	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	REAR END	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	35	GOMG STRAIGHT	FOLLOWING TOO CLOSE
27	9C	78.8	2/19/2013	1250	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	REAR END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	10	SLOWING	CARELESS DRIVING
28	9C	78.8	2/10/2013	1636	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	SIDEWIRE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	10	GOMG STRAIGHT	OTHER
29	9C	78.8	12/12/2013	1615	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	SIDEWIRE OPPOSITE DIRECTION	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	25	GOMG STRAIGHT	IMPROPER DRIVING ON MTH HWAY
30	9C	78.8	1/21/2013	1250	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	PARKED MOTOR VEHICLE	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	40	OTHER	TOO FAST FOR CONDITIONS
31	9C	78.8	8/22/2016	1115	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNDER INFLUENCE ALCOHOL	25	GOMG STRAIGHT	OTHER
32	9C	78.8	2/22/2013	1900	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON GRADE	ICY	DAYLIGHT	NONE	N	ROAD MAINTENANCE EQUIPMENT	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	40	OTHER	IMPROPER DRIVING ON MTH HWAY
33	9C	78.9	1/26/2013	1405	INO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	SNOW W/VS ICY ROAD TREATMENT	DAYLIGHT	SNOW/SLEET/THAL	N	OTHER NON-COLLISION	N	TRUCK GVW > 10K/BUSES > 15 PEOPLE	NO IMPAIRMENT	DRIVER UNDER INFLUENCE ALCOHOL	20	OTHER	IMPROPER DRIVING ON MTH HWAY
34	9C	79	3/1/2016	1959	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DARK UNLIGHTED	NONE	N	SIGN	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNDER INFLUENCE ALCOHOL	30	GOMG STRAIGHT	IMPROPER DRIVING ON MTH HWAY
35	9C	79.3	1/17/2015	660	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON GRADE	ICY	DARK UNLIGHTED	SNOW/SLEET/THAL	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	20	OTHER	CARELESS DRIVING
36	9C	79.42	12/12/2013	1445	INO	ON	NON-INTERSECTION	1	STRAIGHT ON GRADE	SNOW	DAYLIGHT	NONE	N	SIDEWIRE OPPOSITE DIRECTION	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	45	OTHER	CARELESS DRIVING CAUSE INJURY
37	9C	79.5	2/23/2015	635	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON GRADE	DRY	DARK UNLIGHTED	NONE	N	OVERTURNING	N	SUV	NO IMPAIRMENT	NONE APPARENT	15	OTHER	TOO FAST FOR CONDITIONS
38	9C	79.5	9/12/2012	1500	INO	ON	NON-INTERSECTION	2	CURVE ON GRADE	WET	DAYLIGHT	RAIN	N	REAR END	N	TRUCK GVW > 10K/BUSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	20	GOMG STRAIGHT	IMPROPER DRIVING ON MTH HWAY
39	9C	79.5	12/15/2013	1400	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON GRADE	DRY	DAYLIGHT	NONE	N	SIDEWIRE SAME DIRECTION	N	SUV	NO IMPAIRMENT	DRIVER PREOCCUPIED	20	OTHER	PASSING ON MTH HWAY
40	9C	79.5	12/15/2013	1520	INO	ON	NON-INTERSECTION	2	CURVE ON GRADE	SNOW	DAWN OR DUSK	SNOW/SLEET/THAL	N	SIDEWIRE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	15	GOMG STRAIGHT	CARELESS DRIVING CAUSE INJURY
41	9C	79.5	2/4/2014	1225	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	WET	DAYLIGHT	NONE	N	SIDEWIRE OPPOSITE DIRECTION	S	TRUCK GVW > 10K/BUSES > 15 PEOPLE	NO IMPAIRMENT	NONE APPARENT	10	WEAVING	FAILED TO DRIVE IN SINGLE LANE
42	9C	79.6	11/22/2013	1400	PDO	ON	NON-INTERSECTION	1	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	TRUCK GVW > 10K/BUSES > 15 PEOPLE	NO IMPAIRMENT	UNKNOWN	40	OTHER	OTHER
43	9C	79.6	8/25/2013	145	INO	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DARK UNLIGHTED	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	30	MAKING RIGHT TURN	DRIVE UNDER INFLUENCE ALCOHOL
44	9C	79.7	3/31/2016	2130	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON LEVEL	ICY	DARK UNLIGHTED	SNOW/SLEET/THAL	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	55	OTHER	IMP PASS/OVERTAKING ON LEFT
45	9C	79.7	3/31/2016	2230	INO	ON	NON-INTERSECTION	2	CURVE ON GRADE	DRY	DARK UNLIGHTED	NONE	N	HEAD-ON	N	SUV	NO IMPAIRMENT	DRIVER UNDER INFLUENCE ALCOHOL	30	OTHER	WIDING W/AY
46	9C	79.7	10/23/2013	740	PDO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAWN OR DUSK	NONE	N	SIDEWIRE OPPOSITE DIRECTION	S	SUV	NO IMPAIRMENT	NONE APPARENT	10	OTHER	AVOIDING OBJECT/VEHICLE IN ROAD
47	9C	79.7	3/7/2013	1332	INI	OFF LEFT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	ALCOHOL	UNKNOWN	10	GOMG STRAIGHT	CARELESS DRIVING
48	9C	79.7	10/12/2013	737	PDO	ON	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	REAR END	N	SUV	NO IMPAIRMENT	NONE APPARENT	10	OTHER	CARELESS DRIVING
49	9C	79.8	3/21/2015	745	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	DRY	DAYLIGHT	NONE	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	30	OTHER	TOO FAST FOR CONDITIONS
50	9C	79.8	4/22/2013	745	INO	ON	NON-INTERSECTION	2	CURVE ON GRADE	ICY	DAYLIGHT	SNOW/SLEET/THAL	N	SIDEWIRE OPPOSITE DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	25	OTHER	FALL TO DR IN DESIGNATED LANE
51	9C	79.9	10/19/2015	100	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON LEVEL	DRY	DARK UNLIGHTED	NONE	N	EMBANKMENT CUT/FILL SLOPE	N	SUV	NO IMPAIRMENT	NONE APPARENT	45	OTHER	AVOIDING OBJECT/VEHICLE IN ROAD
52	9C	80	10/21/2013	737	PDO	OFF LEFT	NON-INTERSECTION	3	CURVE ON GRADE	ICY	DAWN OR DUSK	NONE	N	GUARD RAIL	N	SUV W/TRAILER	NO IMPAIRMENT	NONE APPARENT	5	OTHER	CARELESS DRIVING
53	9C	80.0	1/21/2013	1000	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON LEVEL	DRY	DAYLIGHT	NONE	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	40	OTHER	CARELESS DRIVING
54	9C	80.05	4/27/2013	800	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	ICY	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	60	OTHER	TOO FAST FOR CONDITIONS
55	9C	80.1	8/25/2016	1800	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON LEVEL	DRY	DAYLIGHT	NONE	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	50	GOMG STRAIGHT	OTHER
56	9C	80.15	12/15/2013	1400	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON LEVEL	DRY	DAYLIGHT	NONE	N	OVERTURNING	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	50	OTHER	CARELESS DRIVING
57	9C	80.2	10/17/2012	700	INI	OFF RIGHT	NON-INTERSECTION	1	CURVE ON GRADE	ICY	DAWN OR DUSK	SNOW/SLEET/THAL	N	CLUEVET/HEDMALL	N	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	15	MAKING LEFT TURN	TOO FAST FOR CONDITIONS
58	9C	80.2	4/22/2015	1800	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON GRADE	DRY	DARK UNLIGHTED	NONE	N	EMBANKMENT CUT/FILL SLOPE	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	50	OTHER	CARELESS DRIVING
59	9C																				

CO-9C Crash Listing

#	Highway	MP	Date	Time	Severity	Location	Road Description	# of Vehicles	Road Contour	Road Condition	Lighting	Weather	Ramp	Accident Type	Direction	Vehicle Type	Drugs / Alcohol	Human Factor	Speed	Vehicle Movement	Violation Code
116	9C	83.2	12/20/2013	2322	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	EMBANKMENT CUT/FILL SLOPE	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	40	OTHER	DRIVE UNDER INFLUENCE ALCOHOL
117	9C	83.21	2/25/2014	1800	PDO	OFF RIGHT	INTERSECTION RELATED	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	OTHER NON-COLLISION	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	20	SLOWING	OTHER
118	9C	83.23	5/2/2013	810	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLEASHY	DAYLIGHT	SNOW/SLEET/HAAL	N	OVERTURNING	N	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	40	OTHER	OTHER
119	9C	83.27	1/25/2016	1250	INI	ON	NON-INTERSECTION	2	HILLCREST	DRY W/WS IGY ROAD TREATMENT	DAYLIGHT	NONE	N	OVERTURNING TURN	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	MAKING U-TURN	MADE U-TURN WHERE PROHIBITED
120	9C	83.29	1/21/2016	1840	INI	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	45	GOING STRAIGHT	FOLLOWING TOO CLOSE
121	9C	83.47	1/7/2013	1733	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-UNLIGHTED	NONE	N	BROADSIDE	E	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	UK	SLOWING	TOO FAST FOR CONDITIONS
122	9C	83.5	11/2/2014	1850	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	OVERTURNING	S	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	50	OTHER	CARELESS DRIVING
123	9C	83.5	4/26/2013	130	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	UK	WEAVING	LEAVING SCENE OF AN ACCIDENT
124	9C	83.7	10/27/2014	1700	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SLEASHY W/WS IGY ROAD TREATMENT	DARK-UNLIGHTED	NONE	N	OVERTURNING	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	40	OTHER	OTHER
125	9C	83.7	8/24/2014	2000	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DAWN OR DUSK	NONE	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	45	AVOIDING OBJECT/VEHICLE IN ROAD	OTHER
126	9C	83.7	11/21/2016	2200	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	65	OTHER	CARELESS DRIVING
127	9C	83.8	10/6/2014	1925	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	OVERTURNING	S	SUV	ALCOHOL	UNKNOWN	50	OTHER	DRIVE UNDER INFLUENCE ALCOHOL
128	9C	83.9	1/23/2016	2240	INI	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	SNOWY	DARK-UNLIGHTED	NONE	N	HEAD-ON	S	SUV	ALCOHOL	UNKNOWN	50	GOING STRAIGHT	OTHER
129	9C	83.9	5/25/2016	2209	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	LARGE BOULDERS OR ROCKS	S	SUV	ALCOHOL	UNKNOWN	50	WEAVING	CARELESS DRIVING
130	9C	84	9/2/2016	1650	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	45	OTHER	CARELESS DRIVING
131	9C	84.02	5/30/2013	934	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SLEASHY	DAYLIGHT	SNOW/SLEET/HAAL	N	OTHER NON-COLLISION	N	SUV	NO IMPAIRMENT	NONE APPARENT	UK	GOING STRAIGHT	OTHER
132	9C	84.04	4/23/2013	432	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	40	GOING STRAIGHT	TOO FAST FOR CONDITIONS
133	9C	84.06	2/21/2014	1145	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	45	GOING STRAIGHT	FOLLOWING TOO CLOSE
134	9C	84.1	9/2/2016	1620	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE	N	SIDESWIPE SAME DIRECTION	N	SUV	NO IMPAIRMENT	NONE APPARENT	45	CHANGING LANES	IMPROPER DRIVING ON MTN HWAY
135	9C	84.17	1/16/2012	1588	INI	FF IN MEDIA	NON-INTERSECTION	2	CURVE ON-LEVEL	SNOWY	DAYLIGHT	NONE	N	HEAD-ON	N	PASS CAR/VAN	NO IMPAIRMENT	DRIVER INEXPERIENCE	50	GOING STRAIGHT	CARELESS DRIVING
136	9C	84.2	6/18/2015	2330	INI	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	OVERTURNING	S	SUV	ALCOHOL	UNKNOWN	50	OTHER	DRIVE UNDER INFLUENCE ALCOHOL
137	9C	84.4	8/22/2016	2102	PDO	ON	NON-INTERSECTION	2	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	WILD ANIMAL	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	45	GOING STRAIGHT	OTHER
138	9C	84.4	3/13/2016	1305	INI	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE	N	SIGN	N	SUV	NO IMPAIRMENT	ILLNESS	40	OTHER	OTHER
139	9C	84.5	12/21/2014	357	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-LEVEL	SNOWY W/WS IGY ROAD TREATMENT	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	OVERTURNING	S	SUV	NO IMPAIRMENT	NONE APPARENT	UK	OTHER	LEAVING SCENE OF AN ACCIDENT
140	9C	84.5	4/4/2012	1730	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAYLIGHT	NONE	N	WILD ANIMAL	S	SUV	NO IMPAIRMENT	NONE APPARENT	50	GOING STRAIGHT	OTHER
141	9C	84.5	4/25/2012	220	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	SIGN	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	55	OTHER	OTHER
142	9C	84.5	2/13/2012	119	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	TREE/SHRUBBERY	S	PICKUP TRUCK/UTILITY VAN	ALCOHOL	UNKNOWN	50	GOING STRAIGHT	DRIVE UNDER INFLUENCE ALCOHOL
143	9C	84.5	11/21/2014	1145	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	SNOWY	DAYLIGHT	NONE	N	TREE/SHRUBBERY	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	50	GOING STRAIGHT	CARELESS DRIVING
144	9C	84.58	12/29/2012	1050	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAAL	N	EMBANKMENT CUT/FILL SLOPE	W	SUV	NO IMPAIRMENT	DRIVER INEXPERIENCE	5	GOING STRAIGHT	TOO FAST FOR CONDITIONS
145	9C	84.7	6/14/2014	1910	PDO	ON	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DAWN OR DUSK	NONE	N	WILD ANIMAL	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	35	GOING STRAIGHT	OTHER
146	9C	84.87	3/18/2013	1410	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE	N	BROADSIDE	S	PASS CAR/VAN	NO IMPAIRMENT	UNKNOWN	35	MAKING RIGHT TURN	OTHER
147	9C	84.9	9/4/2012	840	PDO	ON	NON-INTERSECTION	1	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE	N	WILD ANIMAL	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	45	GOING STRAIGHT	OTHER
148	9C	84.9	1/16/2016	2300	PDO	OFF LEFT	NON-INTERSECTION	1	STRAIGHT ON-GRADE	SNOWY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	EMBANKMENT CUT/FILL SLOPE	S	PASS CAR/VAN	ALCOHOL	UNKNOWN	60	OTHER	DRIVE UNDER INFLUENCE ALCOHOL
149	9C	84.9	3/7/2013	1730	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DAYLIGHT	NONE	N	TREE/SHRUBBERY	S	SUV	NO IMPAIRMENT	NONE APPARENT	50	GOING STRAIGHT	IMPROPER DRIVING ON MTN HWAY
150	9C	84.9	10/25/2013	1505	INI	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DAYLIGHT	NONE	N	TREE/SHRUBBERY	N	PASS CAR/VAN	NO IMPAIRMENT	ASSLET AT WHEEL	60	GOING STRAIGHT	CARELESS DRIVING
151	9C	85	4/17/2015	40	PDO	OFF LEFT	NON-INTERSECTION	1	CURVE ON-GRADE	ICY	DARK-LIGHTED	SNOW/SLEET/HAAL	N	OVERTURNING	S	SUV	ALCOHOL	UNKNOWN	50	OTHER	DRIVE UNDER INFLUENCE ALCOHOL
152	9C	85.08	7/28/2013	1315	INI	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	2	GOING STRAIGHT	FOLLOWING TOO CLOSE
153	9C	85.49	3/5/2014	1100	PDO	ON	NON-INTERSECTION	2	CURVE ON-GRADE	DRY	DAYLIGHT	SNOW/SLEET/HAAL	N	SIDESWIPE OPPOSITE DIRECTION	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	40	OTHER	OTHER
154	9C	85.7	1/16/2013	245	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY W/WS IGY ROAD TREATMENT	DARK-UNLIGHTED	NONE	N	SIGN	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	UK	AVOIDING OBJECT/VEHICLE IN ROAD	CARELESS DRIVING
155	9C	85.7	2/10/2014	2300	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	DRY	DARK-UNLIGHTED	NONE	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	NO IMPAIRMENT	UNKNOWN	50	OTHER	TOO FAST FOR CONDITIONS
156	9C	86	2/10/2016	850	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	UNKNOWN	UNKNOWN	NONE	N	REAR-END	N	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	35	GOING STRAIGHT	OTHER
157	9C	86	5/8/2016	2330	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-GRADE	DRY	DARK-UNLIGHTED	SNOW/SLEET/HAAL	N	REAR-END	S	PASS CAR/VAN	ALCOHOL/DRUGS	UNKNOWN	50	GOING STRAIGHT	DUI/BAC .08 MORE
158	9C	86	11/20/2014	180	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAAL	N	EMBANKMENT CUT/FILL SLOPE	S	SUV	NO IMPAIRMENT	NONE APPARENT	50	OTHER	LEAVING SCENE OF AN ACCIDENT
159	9C	86.16	1/2/2013	1000	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE	N	REAR-END	N	SUV	NO IMPAIRMENT	NONE APPARENT	7	GOING STRAIGHT	OTHER
160	9C	86.17	2/27/2015	1545	PDO	ON	AT DRIVEWAY ACCESS	2	STRAIGHT ON-GRADE	SNOWY	DAYLIGHT	NONE	N	BROADSIDE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	5	GOING STRAIGHT	OTHER
161	9C	86.17	3/25/2015	963	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAAL	N	BROADSIDE	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	UNKNOWN	10	MAKING RIGHT TURN	TOO FAST FOR CONDITIONS
162	9C	86.17	2/23/2012	1730	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DARK-LIGHTED	SNOW/SLEET/HAAL	N	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	35	OTHER	DRIVE WHEN DR LICENSE EXPIRED
163	9C	86.17	12/6/2013	1430	PDO	ON	NON-INTERSECTION	2	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	SNOW/SLEET/HAAL	N	REAR-END	W	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	35	GOING STRAIGHT	OTHER
164	9C	86.17	10/1/2014	850	PDO	ON	AT INTERSECTION	2	HILLCREST	DRY	DAYLIGHT	WIND	N	REAR-END	E	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	2	MAKING LEFT TURN	OTHER
165	9C	86.17	9/26/2015	1544	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-GRADE	DRY	DAYLIGHT	NONE	N	REAR-END	S	PASS CAR/VAN	NO IMPAIRMENT	NONE APPARENT	30	GOING STRAIGHT	OTHER
166	9C	86.17	10/8/2015	1600	PDO	ON	ROUNDABOUT	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE	N	REAR-END	N	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	5	STOPPED IN TRAFFIC	OTHER
167	9C	86.17	12/3/2016	1540	PDO	ON	INTERSECTION RELATED	2	CURVE ON-GRADE	WET	DAYLIGHT	NONE	N	SIDESWIPE SAME DIRECTION	W	SUV	NO IMPAIRMENT	DRIVER UNFAMILIAR W/AREA	5	MAKING LEFT TURN	OTHER
168	9C	86.17	3/25/2015	850	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-GRADE	ICY	DAYLIGHT	SNOW/SLEET/HAAL	N	SIDESWIPE OPPOSITE DIRECTION	E	SUV	NO IMPAIRMENT	NONE APPARENT	30	STOPPED IN TRAFFIC	OTHER
169	9C	86.17	12/6/2013	856	PDO	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE	N	APPROACH TURN	S	SUV	NO IMPAIRMENT	NONE APPARENT	10	MAKING LEFT TURN	TOO FAST FOR CONDITIONS
170	9C	86.17	9/26/2015	1315	PDO	ON	INTERSECTION RELATED	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE	N	BICYCLE	W	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	NONE APPARENT	5	GOING STRAIGHT	OTHER
171	9C	86.17	10/5/2014	1814	PDO	OFF RIGHT	NON-INTERSECTION	1	CURVE ON-GRADE	DRY	DARK-UNLIGHTED	NONE	N	EMBANKMENT CUT/FILL SLOPE	E	PASS CAR/VAN	ALCOHOL	UNKNOWN	60	OTHER	DRIVE UNDER INFLUENCE ALCOHOL
172	9C	86.19	12/25/2012	900	PDO	OFF RIGHT	NON-INTERSECTION	1	STRAIGHT ON-LEVEL	ICY	DAYLIGHT	NONE	N	LIGHT/UTILITY POLE	N	SUV	NO IMPAIRMENT	NONE APPARENT	25	AVOIDING OBJECT/VEHICLE IN ROAD	TOO FAST FOR CONDITIONS
173	9C	86.25	3/23/2012	1155	INI	ON	AT INTERSECTION	2	STRAIGHT ON-LEVEL	DRY	DAYLIGHT	NONE	N	BROADSIDE	S	PICKUP TRUCK/UTILITY VAN	NO IMPAIRMENT	DRIVER PREOCCUPIED	25	GOING STRAIGHT	FAIL TO OBSERVE TRAFFIC DEVICE

Appendix D-Traffic Methodology, Data & Analysis



Memorandum

TO: Blue River Access Control Plan (ACP) Project Team
FROM: Maxwell Rusch, EIT
DATE: November 28, 2018
PROJECT: **Blue River ACP: CO 9– Carroll Lane to Broken Lance Drive/Boreas Pass Road (MP 77.49 – MP 86.26)**
RE: Traffic Methodology

This memorandum describes the general traffic engineering and transportation planning approach proposed by Stolfus & Associates, Inc. for the CO 9 Blue River Access Control Plan (ACP). The purpose of this memorandum is to outline the primary assumptions and procedures that will be used in the traffic analyses for the project. All traffic analyses conducted for the ACP will be in accordance with this methodology and used to support access-related decisions made during the course of the project. While access point consolidation is the primary goal of the project, recommendations resulting from the ACP traffic analyses, such as the addition of auxiliary lanes, may be incorporated as well.

STUDY AREA

The study area consists of an 8.77-mile segment of CO 9 in Summit County. The highway is an undivided, 2-lane, rural highway through mountainous terrain. The segment begins at Carroll Lane (MP 77.49) and ends just north of Boreas Pass Rd/Broken Lance Dr (MP 86.26). This stretch of CO 9 passes through multiple jurisdictions, shown in the table below.

Mile Post	Jurisdiction
MP 77.49 to MP 80.77	Summit County
MP 80.77 to MP 84.72	Town of Blue River
MP 84.72 to MP 86.00	Summit County
MP 86.00 to MP 86.26	Town of Breckenridge

EXISTING TRAFFIC

Hourly vehicle classification counts were collected at four locations along CO 9 from Tuesday, July 31, 2018 to Thursday, August 2, 2018. Peak period Turning Movement Counts (TMCs) were collected at 31 public street and driveway locations along the highway. Using data from the count stations maintained by the Colorado Department of Transportation (CDOT), it was determined that July and August are the peak months for traffic volumes through this corridor. Since the counts were taken during the months with the highest volumes, no seasonal adjustment to the traffic count data is proposed. Figures showing the existing traffic counts are attached.

FUTURE TRAFFIC

Traffic growth along CO 9, within the study area, will be forecasted using publicly available CDOT estimates. The count station data is shown in the table below.

CDOT Count Station	MP	Annual Growth Rate	22-Year Growth Factor
100495	82.63	2.2%	1.63
100499	86.17	1.2%	1.30

Since there is not expected to be side street growth, the number of additional thru vehicles on the mainline in 2040 should be consistent through the length of segment. From averaging the projected volume increase at the two count stations, it is assumed that the northbound volumes will increase by 190 vehicles per hour (VPH) in the AM and 138 VPH in the PM. The southbound volumes are expected to increase by 117 VPH in the AM and 218 VPH in the PM. These additional volumes are added to the existing thru movements to get the 2040 volume projections for the thru movements on CO 9.

The side streets along this portion of CO 9 are not thru roads, and almost exclusively serve residential areas. Due to this, traffic growth on a side street will be estimated by the potential for future development on that particular street, rather than background growth on CO 9. Along the study area, no developments are planned within the towns of Breckenridge and Blue River, so no side street growth was assumed. There is potential for an expansion of Lodge by The Blue located at MP 80.69, as a proposal to add 46 lodge units and 10-15 workforce housing units was recently submitted. While it is unclear at this time whether this expansion will take place, the potential trips generated by this development are included in the 2040 volume projections.

TRAFFIC ANALYSIS SCENARIOS

Traffic operations will be evaluated for the following three scenarios:

- Existing
- '2040 No ACP'
- '2040 ACP'

The existing scenario will be evaluated using the July/August 2018 count data and the existing roadway geometry. The 2040 No ACP scenario will evaluate traffic conditions with the assumption that roadway geometry has remained the same. The traffic counts will be inflated in accordance with the future growth projections. The 2040 ACP scenario will analyze the study area assuming full implementation of the proposed ACP plan. Where warranted by the State Highway Access Code (SHAC) criteria, auxiliary lanes will also be assumed.

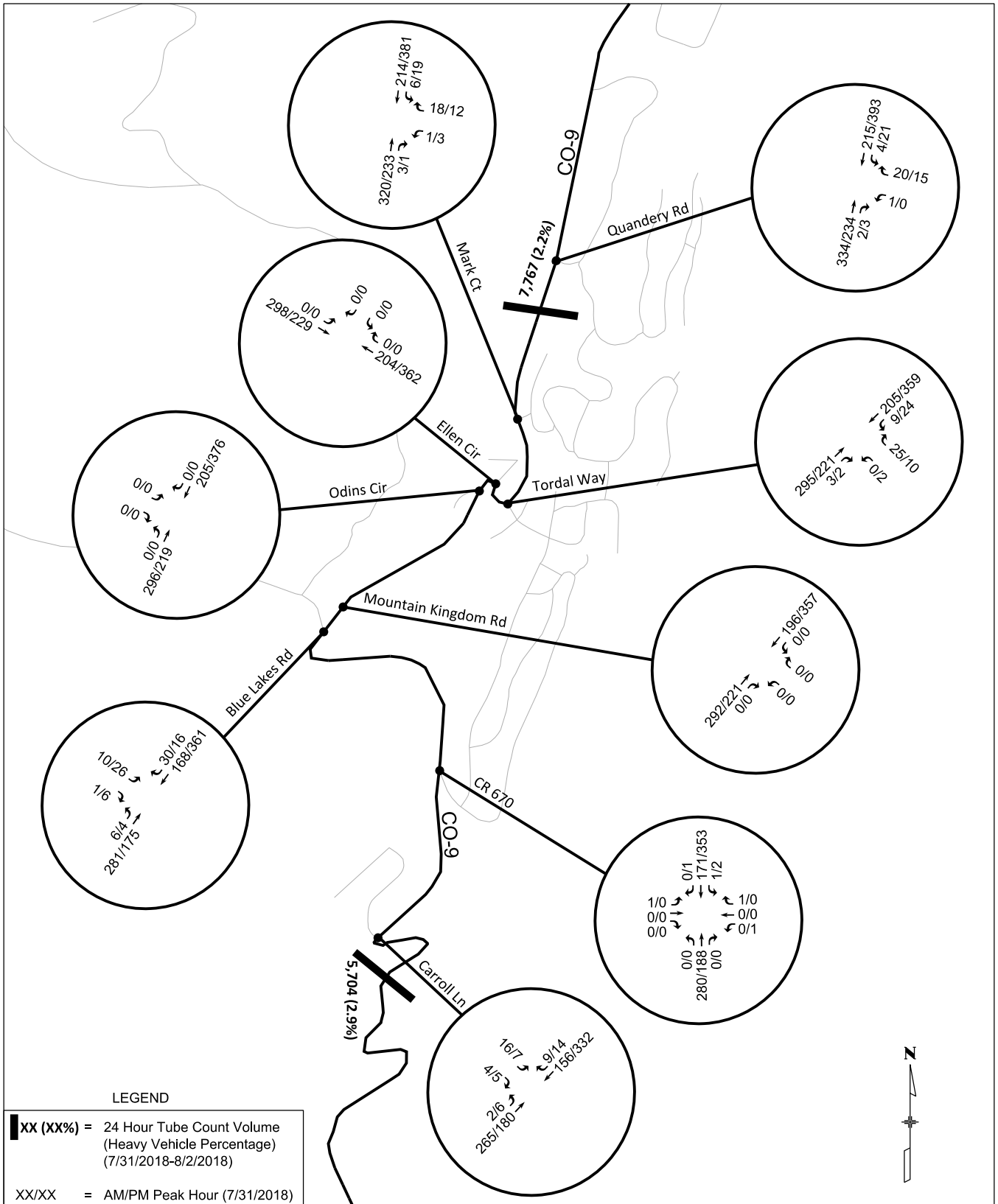
TRAFFIC ANALYSIS APPROACH

Traffic operations will be evaluated using *Highway Capacity Manual, 6th Edition* methods as applied in the HCS 7 software. Both the individual intersections and the highway as an overall segment will be analyzed. The intersection analysis will be conducted using the Two-Way Stop-Control and Streets methods. The Level of Service (LOS), derived from the average vehicle delay, will be reported for each of the intersections. The Two-Lane Highway method will be used to conduct the segment analysis. CO 9 will be analyzed as three separate segments with boundaries determined by changes in roadway characteristics and traffic volumes. The segment boundaries are shown in the table below.

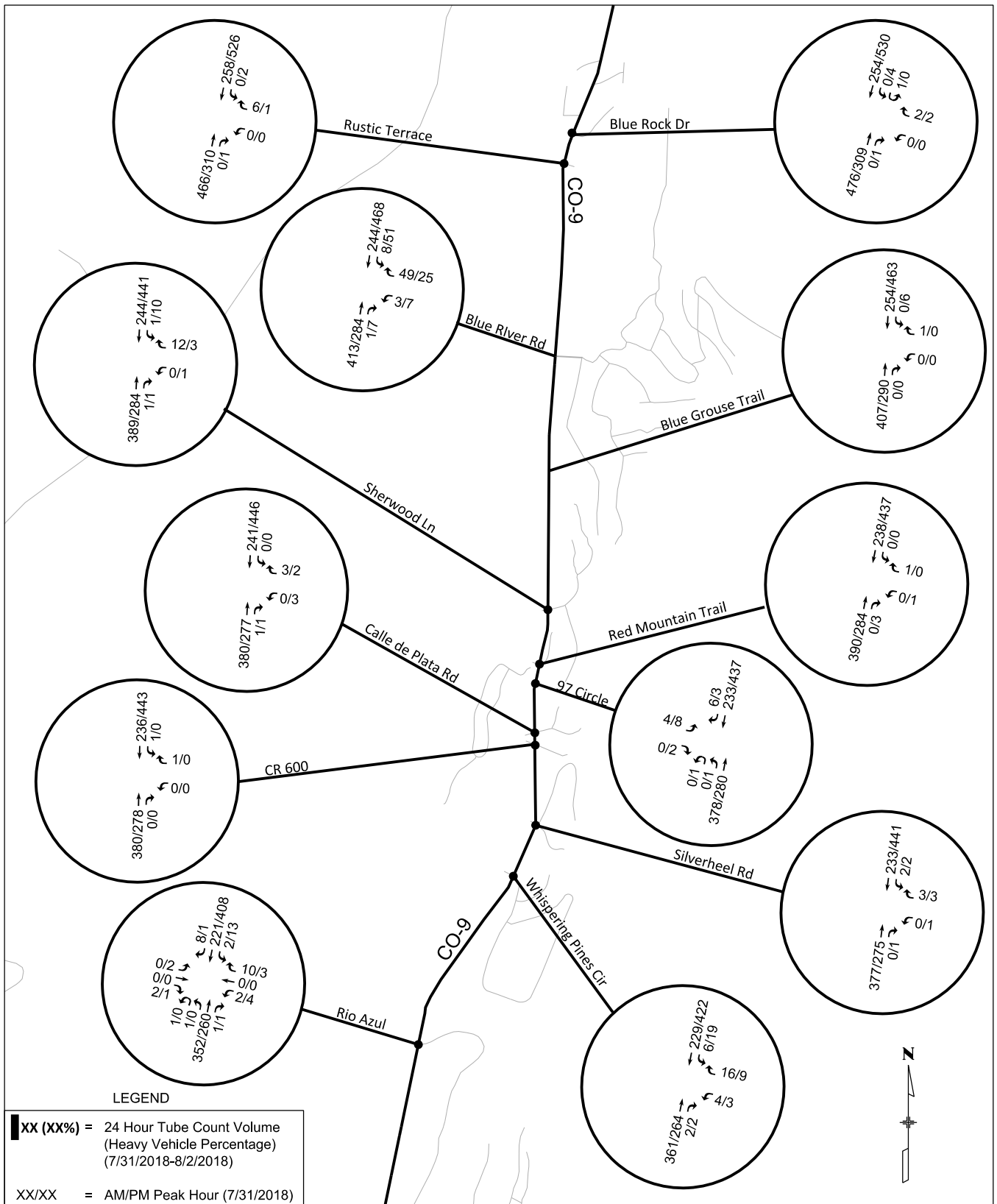
Segment	Mile Post
Segment 1	MP 77.49 to MP 79.97
Segment 2	MP 79.97 to MP 84.00
Segment 3	MP 84.00 to MP 85.88

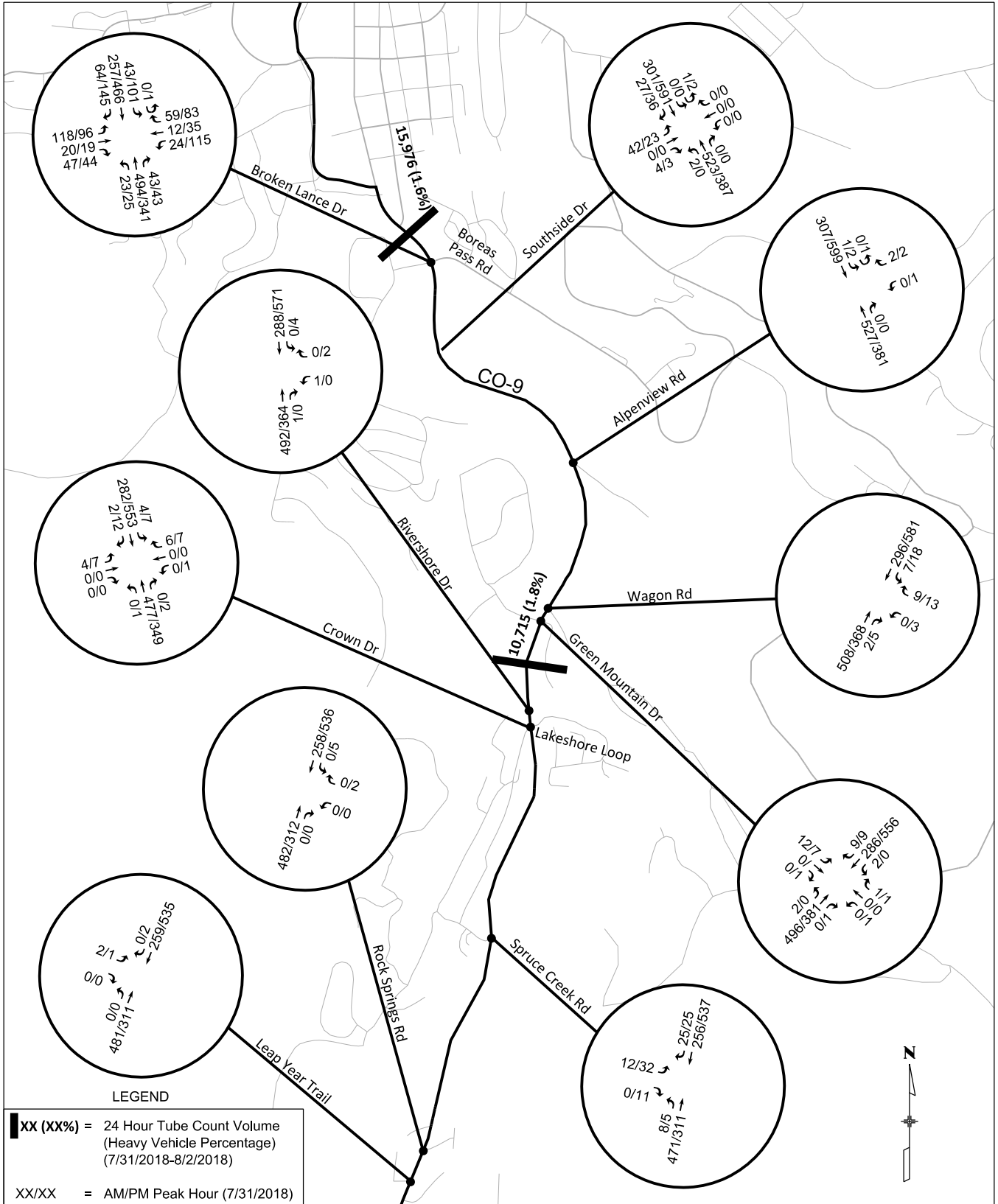
Since this portion of CO 9 is a Class II highway, the Level of Service (LOS) for the segment analysis will be derived from the percent time spent following. If either an intersection or highway segment is expected to operate at an unacceptable LOS in 2040, possible geometric changes to the roadway will be considered. Improvements such as passing lanes and additional passing zones are not in the scope of this ACP, however, the operational benefits that these could provide will be analyzed in HCS. Based on these results, recommendations will be made on whether the construction of passing lanes or passing zones is something that local jurisdictions may wish to pursue in the future.

Attachments: Existing Traffic Figures



Existing Volumes
Sheet 1 of 3





Appendix E-Access Control Plan Methodology & Evaluation Process



Memorandum

TO: Blue River Access Control Plan (ACP) Project Team
FROM: Michelle Hansen, PE and Jenna Siegel, PE
DATE: November 28, 2018
PROJECT: **Blue River ACP: CO 9– Carroll Lane to Broken Lance Drive/Boreas Pass Road (MP 77.49 – MP 86.26)**
RE: Access Plan Methodology

This memorandum describes the general approach proposed by Stolfus & Associates, Inc. to develop the CO 9 Blue River Access Plan. The purpose of this memorandum is to outline, for the benefit of the Summit County, the Town of Blue River, the Town of Breckenridge, and CDOT, the primary assumptions that will be used in developing the recommended access plan and to document agency concurrence with the proposed methodology. A separate methodology for the related traffic engineering and transportation planning elements of the project has been prepared documenting the primary assumptions and procedures that will be used to develop future traffic projections.

STUDY AREA

The study area consists of approximately 9 miles of CO 9 in Summit County, which includes a segment through the Town of Blue River and a small segment at the south end of Breckenridge. The project limits stretch from Carroll Lane (MP 77.49) to Broken Lance Drive/Boreas Pass Road (MP 86.26).

HIGHWAY ASSUMPTIONS

We will assume that the highway category as defined by the State Highway Access Code (SHAC) for CO 9 will remain the same as it is today. Currently, the study corridor falls under Category Regional Highway or Category R-A. Category R-A applies to medium to high speed sections with medium to high traffic volumes within rural areas. Direct access is subordinate to through traffic movements. Full movement intersections are spaced 1/2 mile apart. Exceptions to 1/2 mile spacing may be permitted if no reasonable alternative exists, the need for the intersection is justified, and minimum 35% signal progression efficiency is achieved. One access is granted per parcel if reasonable access cannot be obtained from the local street system. Locations of full movement intersections should serve as many properties and interests as possible and preference shall be given to public roadways that meet or may reasonably be expected to meet signal warrants in the future. The auxiliary lane warrants for R-A highway are as follows:

- Left turn deceleration lane – 10 vehicles per hour (vph)
- Right turn deceleration lane – 25 vph
- Right turn acceleration lane – 50 vph

Guidance from the SHAC for R-A highways will be considered in developing the Access Plan.

A single access control plan table will be developed for this segment of CO 9. An access ID number and a reference point (milepost) will identify the location of each access point in the table. A control point will be established for CO 9 at the center of the intersection of Broken Lance Drive/Boreas Pass Road (MP 86.165) based on the CDOT Highway Segment Description Table to establish reference points (mileposts). All other access point locations will be measured from the control point established.

ACCESS ASSUMPTIONS

The following assumptions regarding access points will be used during the development of the access plan:

- Maximum 1-mile out of direction travel standard (1/2 mile each way) on CO 9.
- Key full movement intersections with the potential for auxiliary lanes and the long term potential for signalization will be identified. Minimum key full movement intersection spacing will be based on the following measures:
 - Functional Intersection Area – AASHTO and the TRB Access Management Manual indicate separation of access points by a distance not less than the functional area of the intersection. The functional intersection area extends upstream and downstream from the physical intersection. The upstream distance is a combination of the storage length, deceleration and taper length, and the perception-reaction distance required for the speed of the segment. The downstream distance is measured as either acceleration length or decision sight distance. Providing acceleration length allows vehicles to accelerate to normal speed without conflict. Providing decision sight distance allows drivers to pass through an intersection before considering potential conflicts at the next intersection. Based on the traffic volumes and the topography along the CO 9 corridor, the need for acceleration lanes and the ability to provide acceleration lanes is low. Therefore, we will use decision sight distance as the controlling downstream functional intersection distance.

The functional intersection area will depend on the speed of the segment and the number of projected turning vehicles. The majority of the corridor is posted at 50 mph. As an example, in a 50-mph rural section with a maximum of 25 turning vehicles during the peak hour, the upstream and downstream functional intersection areas (FIA) are as follows:

- Upstream FIA = 25' (SHAC storage) + 415'(AASHTO decel + taper) + 185'(Access Management Manual 2.5 s (rural) perception-reaction) = 625'
- Downstream FIA = 465'(AASHTO Decision Sight Distance (DSD))

For additional explanation of the functional intersection area, refer to attached excerpts from AASHTO A Policy on Geometric Design of Highways and Streets, 2018 and TRB Access Management Manual, 2014. The most current guidance available from AASHTO will be used for deceleration and taper lengths.

Ideally, the full functional intersection area will be provided between key full movement intersections. At a minimum, the physical length needed to accommodate storage length, deceleration, and taper length will be provided between intersections for the posted speed to ensure that proposed improvements will meet current design standards on opening day upon construction.

- Other site-specific considerations as appropriate, including: locations of existing intersections, physical and/or right-of-way limitations, community and stakeholder input, type of access/traffic using access, etc.
- The appropriateness of additional access points between key full movement intersections will be considered on a case-by-case basis. We will confirm that access to the highway or public road

system can be provided to each property as defined by ownership. Multiple parcels under one ownership will be considered a single property or ownership. If access to the highway is appropriate, access will be located outside the functional intersection area of the nearest key full movement intersections, if possible.

- It is anticipated that CO 9 will remain a two-lane highway within the 20-year planning period considered in the traffic analysis. Based on a two-lane highway configuration, all intermediate access points identified between key full movement intersections will be identified as unsignalized full movement intersections without auxiliary lanes. Limited movement access points will not be considered as part of this access plan unless extenuating circumstances suggest that limiting movements is needed for safety or operational considerations.
- Consolidate private access to one access per ownership unless extenuating circumstances are identified related to property size, circulation and/or business operations.
- Due to low volume use, field access points will be maintained until land use changes occur. Exceptions to this approach may include identification of a safety issue with the existing configuration.
- Opportunities to share private accesses may be considered. Due to the rural/residential nature of the land uses and the challenge of implementation, shared access is not preferred and must have strong justification for inclusion in the plan.
- Locate public accesses to serve multiple properties, wherever possible.
- Align closely spaced access points on the opposite side of the road to eliminate overlapping vehicle conflicts.
- Eliminate direct private access to CO 9 if reasonable access to the local street network is available. Reasonable access is defined per the State Highway Access Code as: "Reasonable local access will be determined in consultation with the appropriate local authority. A determination of reasonable access from a local street or road should include consideration of the local street or road function, purpose, capacity, operational and safety conditions and opportunities to improve the local street or road. Direct access to the highway should not be denied if the alternative local access would create a significant operational or safety problem at the alternative location and the direct access to the state highway would not be a significant problem to the highway."
- The Summit County Wide Comprehensive Plan, The Breckenridge Comprehensive Plan, The Blue River Comprehensive Plan, and the Joint Upper Blue Master Plan will be considered in identifying key full movement intersections. Additional or modified connections that provide circulation will be identified, where applicable.
- Potential techniques for access management may be identified within the study document, but particular techniques will not be identified for specific access points.
- Highway pull-off areas will be evaluated on location and sight distance. Recommendations to eliminate or expand pull-off areas will be made.

ACCESS PLAN EVALUATION

The project team will develop a single overall recommended long-term access scenario. While options for specific areas may be identified and evaluated during the overall development of the plan, multiple corridor scenarios will not be developed and compared beyond those defined in the Traffic Methodology Memo. In order to provide a logical means for determining whether the Access Plan meets the purpose and need of the project without comparing multiple corridor alternatives, a compatibility index was developed. The index identifies a set of evaluation criteria that correspond with each access related project goal defined by the project team at the beginning of the project. A simple rating system that identifies if the plan is favorable, neutral or unfavorable with respect to each criterion is defined. Each of the three ratings under each criterion is given a definition specific to the criterion to

assist in the evaluation. In cases where the access plan evaluation requires a comparison, the criteria will be measured against the Year 2040 without ACP scenario. Please refer to the attached compatibility index for evaluation criteria and definitions.

Blue River CO 9 Access Plan Compatibility Index



The Access Plan will be evaluated using the following criteria to determine if the Plan meets purpose and need of the project based on the established project goals. In cases where the evaluation requires a comparison, the criteria will be measured against the Year 2040 No-ACP scenario.

Project Goal	Evaluation Criteria	Status with Respect to Criteria		
		Favorable (+)	Neutral (0)	Unfavorable (-)
Provide effective and efficient through travel for traffic on CO 9.	Highway Segment LOS	Highway LOS improves for most segments as compared to the No- ACP scenario	Little or no change to Highway LOS for most segments as compared to the No- ACP scenario	Highway LOS reduced for most segments as compared to the No- ACP scenario
	Functional Intersection Area	Full functional intersection area provided between key intersections.	At a minimum, accommodates turn lane storage, decel and taper lengths between key intersections without overlap.	Turn lane storage, decel and taper lengths overlap between key intersections.
	Number of Access Points	Fewer accesses per mile	Number of accesses is maintained	More accesses per mile
Provide safe, effective, and efficient access to and from CO 9 for businesses, residents and guests.	Intersection Sight Distance	More intersections have adequate sight distance	Same number of intersections have adequate sight distance	Fewer intersections have adequate sight distance
	Intersection Level of Service (LOS)	LOS improves for most intersections as compared to the No-ACP scenario	Little or no change to LOS for most intersections as compared to the No-ACP scenario	LOS reduced for most intersections as compared to the No-ACP scenario
	Conformance with State Highway Access Code Auxiliary Lane Requirements	More locations meet auxiliary lane standards	Some locations meet auxiliary lane standards	Fewer locations meet auxiliary lane standards
	Out of Direction Travel Distance	Less out-of-direction travel distance is required	No change	More out-of-direction travel distance is required
	Intersection Crash Risk	Reduce the number of potential conflict points by implementing needed physical improvements and access control measures	Reduce the number of potential conflict points by implementing needed physical improvements only	Maintain or increase the number of potential conflict points through failure to implement needed physical improvements or access control measures
Maintain compatibility with existing and proposed off-system connections that provide local circulation to support the transportation system.	Local Route Connectivity	Improve connectivity of local routes	Maintain connectivity of local routes	Reduce connectivity of local routes
	Function of Local Transportation System	Plan recommendations support travel to local amenities	Plan recommendations do not affect travel to local amenities	Plan recommendations impede travel to local amenities
Provide a plan that is adoptable by all entities and can be implemented in phases.	Public Support	Has positive public support	Has balanced public support	Does not have public support
	Phasing Opportunities	Plan recommendations can be segmented into logical, compatible pieces funded by private development	Plan recommendations can be segmented into logical, compatible pieces requiring public & private funding	Plan recommendations not easily segmented and require significant public investment to implement
	Physical Constraints	No physical constraints	Manageable physical constraints	Physical constraints are not manageable
Support the economic viability of the project area.	Business Market Area	Expands market area for the majority of businesses in the corridor	Market area maintained for a majority of businesses in the corridor	Reduced market area for a majority of businesses in the corridor
Maintain compatibility with previous local planning efforts, including wildlife planning.	Compatibility with Local Planning	Expands/improves upon previous local planning recommendations	Consistent with previous local planning recommendations	Not consistent with previous local planning efforts
	Compatibility with Summit County Safe Passages Plan	Consistent with all wildlife planning recommendations	Consistent with some wildlife planning recommendations	Not consistent with wildlife planning recommendations
Support the development of alternative modes, including transit, pedestrian, and bicycle routes.	Access for Multi-modal Users (Cyclists, Pedestrians, Transit)	Opportunities to expand access and/or improve safety	Current level of access maintained	Access reduced through the corridor

Appendix F-Intergovernmental Agreement

Appendix G-Access Control Plan Tables

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
1	77.49	LT	CR 860 (Carroll Ln)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
1b	77.61	RT	Vehicle pull-off	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
2	77.78	LT	CR 856	Unsignalized Full Movement	Unsignalized Full Movement	
3	77.78	RT	Back access to CR 671	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	When safety or operational issues occur, or when a public project is funded.
4	77.91	RT	Residential	Unsignalized Full Movement	Close access - share access at Access No. 5.5	When property redevelops. Cross access agreements required between property no. 2483-1210-14-007, property no. 2483-1210-14-006, property no. 2483-1210-14-005, property no. 2483-1210-14-003, and any other properties of interest upon redevelopment.
5	77.92	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
5.5	77.93	RT	New Access	None	Shared Unsignalized Full Movement at property line	When either property redevelops. Cross access agreements required between property no. 2483-1210-14-007, property no. 2483-1210-14-006, property no. 2483-1210-14-005, property no. 2483-1210-14-003, and any other properties of interest upon redevelopment.
6	77.94	RT	Field access	Unsignalized Full Movement	Close access - share access at Access No. 5.5	When property redevelops. Cross access agreements required between property no. 2483-1210-14-007, property no. 2483-1210-14-006, property no. 2483-1210-14-005, property no. 2483-1210-14-003, and any other properties of interest upon redevelopment.
7	77.95	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
	77.97		Milepost 78			
8	78.00	RT	CR 670	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
8.5	78.00	LT	Realigned CR 805 (Hamilton Ln)	None	Full Movement intersection - realign CR 805 across from CR 670 (Access No. 8). Potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Realign CR 805 across from CR 670 when safety or operational issues occur, or when a public project is funded. Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
9	78.01	LT	CR 805 (Hamilton Ln)	Unsignalized Full Movement	Close access - realign CR 805 across from CR 670 (Access No. 8).	When safety or operational issues occur, or when a public project is funded.
9.5	78.06	RT	New Access	None	Shared Unsignalized Full Movement at property line	When either property redevelops. Cross access agreements required between property no. 2483-0140-18-004, property no. 2483-0140-18-003, and any other properties of interest upon redevelopment.
10	78.32	LT	Field access	Gated Unsignalized Full Movement	Gated Unsignalized Full Movement	
11	78.61	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off.	Access to property no. 2843-0140-07-002 to be permitted as necessary.
12	78.75	LT	CR 855	Unsignalized Full Movement	Unsignalized Full Movement	
13	78.80	LT	CR 850 (Blue Lakes Rd)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
14b	78.84	RT	Vehicle pull-off	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
14	78.86	RT	CR 871 (Mountain Kingdom Rd)	Unsignalized Full Movement	Unsignalized Full Movement	If legal agreements do not exist, cross access agreements required between property no. 2483-0140-24-003, property no.2483-0140-24-004, property no. 2483-0140-25-002, and any other properties of interest upon redevelopment.
14.5	78.91	RT	Vehicle pull-off	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
	78.94		Milepost 79			
15	79.25	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
16	79.40	LT	CR 849 (Odins Cir)	Unsignalized Full Movement	Unsignalized Full Movement	
17	79.45	LT	CR 848 (Ellen Cir)	Unsignalized Full Movement	Unsignalized Full Movement	
18a	79.53	RT	Residential	Unsignalized Full Movement	Close access - access available via CR 656	When property redevelops, safety or operational issues occur, or when a public project is funded.
18b	79.53	RT	CR 656 (Tordal Way)	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. Movements may be restricted if operational or safety issues develop.

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
19	79.62	RT	Vehicle pull-off	Unsignalized Full Movement	Unsignalized Full Movement	When property redevelops.
20	79.81	LT	CR 847 (Susan Ct)	Unsignalized Full Movement	Close access - realign Susan Ct across from CR 650 (Mark Ct) (Access No. 21).	When safety or operational issues occur, or when a public project is funded.
20.5	79.83	LT	Realigned Susan Ct	None	Full Movement intersection - realign Susan Ct across from CR 650 (Mark Ct) (Access No. 21). Potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Realign Susan Ct accross from CR 650 (Mark Ct) when safety or operational issues occur, or when a public project is funded. Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
21	79.83	RT	CR 650 (Mark Ct)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
22	79.89	LT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
	79.93		Milepost 80			
23	80.28	LT	Forest Service	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
24a	80.30	RT	CR 626 (Quandary Rd)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
24b	80.31	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted. May be used for bus stop.	
25	80.40	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
26	80.43	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
27	80.47	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
28	80.67	LT	Parking for trailhead	Unsignalized Full Movement	Close access - access available via CR 806 (Rio Azul)(Access No. 30)	When alternate access to Access No. 30 is available. Cross access agreements required between property no. 2371-3120-17-001, property no. 2373-3610-04-002, and any other properties of interest upon redevelopment. Or public roadway between trailhead access and CO 9 along Rio Azul required.

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
29a	80.690	RT	Lodge by the Blue	Unsignalized Full Movement	Close access - access available via Access No. 29b or 29c	When property redevelops and alternate access to Access No. 29b or 29c is available. Cross access agreements required between property no. 2371-3120-03-003, property no. 2371-3120-00-008, and any other properties of interest upon redevelopment.
29b	80.695	RT	Lodge by the Blue	Unsignalized Full Movement	Conditional Full Movement intersection - realign Quandary Rd to connect to CO 9. Potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Either Access No. 29b or 29c shall remain open. Access shall close when property redevelops and if alternative access to Access No. 29c is available. If Access No. 29b remains open, auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop. Cross access agreements required between property no. 2371-3120-03-003, property no. 2371-3120-00-008, and any other properties of interest upon redevelopment.
29c	80.78	RT	Lodge by the Blue	None	Conditional Full Movement intersection - realign Quandary Rd to connect to CO 9. Potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Either Access No. 29b or 29c shall remain open. Access may not be provided at Access No. 29c until Access No. 29b closes. If Access No. 29c opens when property redevelops, auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop. Cross access agreements required between property no. 2371-3120-03-003, property no. 2371-3120-00-008, and any other properties of interest upon redevelopment.
30	80.695	LT	Rio Azul	Unsignalized Full Movement	Conditional Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. If Access No. 29b remains open, a signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop. If Access No. 29b closes and Access No. 29c opens, a traffic signal will not be implemented regardless of warrants. Cross access agreements required between property no. 2371-3120-17-001, property no. 2373-3610-04-002, and any other properties of interest upon redevelopment. Or public roadway between trailhead access and CO 9 along Rio Azul required.
31	80.84	RT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 29b or 29c	When property redevelops and alternate access to Access No. 29b or 29c is available. Cross access agreements required between property no. 2371-3120-03-003, property no. 2371-3120-00-008, and any other properties of interest upon redevelopment.
32	80.89	LT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
	80.95		Milepost 81			
33	81.18	RT	Whispering Pines Ln	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.

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Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
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* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
34	81.33	RT	CR 590 (Silverheels Rd)	Unsignalized Full Movement	Unsignalized Full Movement	
34.5	81.33	LT	New Access	None	Unsignalized Full Movement across from Access No. 34.	When property redevelops. Access to be aligned with Access No. 34. If legal agreements do not exist, cross access agreements required between property no. 2371-3030-07-008, property no. 2371-3020-07-009, and any other properties of interest upon redevelopment.
35	81.53	LT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
36	81.56	RT	CR 600 (Fredonia Gulch Rd)	Unsignalized Full Movement	Conditional Unsignalized Full Movement	When property redevelops and alternate access to Calle De Plata Rd is available, access will close.
37	81.57	RT	CR 589 (Calle De Plata Rd)	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. Movements may be restricted if operational or safety issues develop.
38.5	81.69	LT	Vehicle pull-off	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
38a	81.750	LT	CR 804 (97 Cir)	Unsignalized Full Movement	Close access - realign 97 Cir across from Access No. 39	Realign CR 804 (97 Cir) across from Access no. 39 when safety or operational issues occur, or when a public project is funded.
38c	81.755	LT	Realigned 97 Cir	None	Unsignalized Full Movement across from Access No. 39	Realign CR 804 (97 Cir) across from Access no. 39 when safety or operational issues occur, or when a public project is funded.
38b	81.76	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	Access to 97 Cir may be provided inside the CO 9 ROW and outside the CO 9 traveled way.
39	81.755	RT	Residential	Unsignalized Full Movement	Shared Unsignalized Full Movement at property line	If legal agreements do not exist, cross access agreements required between property no. 2371-3020-01-013, property no. 2371-3020-01-012, and any other properties of interest upon redevelopment.
40	81.77	RT	Red Mountain Trail	Unsignalized Full Movement	Gated Unsignalized Full Movement	When property redevelops, safety or operational issues occur, or when a public project is funded.
41	81.81	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
42	81.84	RT	Residential	Unsignalized Full Movement	Close access - access available via Red Mountain Trail	When property redevelops and alternate access to Red Mountain Trail is available.
43	81.86	LT	Residential	Shared Unsignalized Full Movement	Shared Unsignalized Full Movement at property line	If legal agreements do not exist, cross access agreements required between property no. 2371-3020-02-007, property no. 2371-3020-02-008, and any other properties of interest upon redevelopment.

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Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
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* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
44	81.92	RT	CR 613 (Sherwood Lane)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
	81.98		Milepost 82			
45	82.02	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	Cross access agreements required between property no. 2371-3020-02-009, property no. 2371-3020-02-001, property no. 2371-1930-05-026, and any other properties of interest upon redevelopment. A potential connection between properties along the ROW line and outside of the CO 9 traveled way may be considered.
45.5	82.02	RT	New access	None	Unsignalized Full Movement access across from Access No. 45	When property redevelops.
46	82.030	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 45	When property redevelops.
47	82.035	LT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 45	When property redevelops. Cross access agreements required between property no. 2371-3020-02-009, property no. 2371-3020-02-001, property no. 2371-1930-05-026, and any other properties of interest upon redevelopment. A potential connection between properties along the ROW line and outside of the CO 9 traveled way may be considered.
48	82.05	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
49	82.06	LT	Residential	Unsignalized Full Movement	Shared Unsignalized Full Movement at property line	If legal agreements do not exist, cross access agreements required between property no. 2371-1930-05-025, property no. 2371-1930-05-024, and any other properties of interest upon redevelopment.
50	82.080	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 51	When property redevelops.
50.5	82.080	RT	New access	None	Unsignalized Full Movement across from Access No. 51	When property redevelops.
51	82.085	LT	Residential	Unsignalized Full Movement	Shared Unsignalized Full Movement at property line	If legal agreements do not exist, cross access agreements required between property no. 2371-1930-05-023, property no. 2371-1930-05-022, and any other properties of interest upon redevelopment.
52	82.09	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 51	When property redevelops.
53	82.110	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 54	When property redevelops.
53.5	82.115	RT	New access	None	Unsignalized Full Movement access across from Access No. 54	When property redevelops.
54	82.115	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	If legal agreements do not exist, cross access agreements required between property no. 2371-1930-05-021, property no. 2371-1930-05-020, property no. 2371-1930-05-019, and any other properties of interest upon redevelopment.

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* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
55	82.140	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 56	When property redevelops. Cross access agreements required between property no. 2371-1930-11-007, property no. 2371-1930-11-006, and any other properties of interest upon redevelopment.
55.5	82.145	RT	New access	None	Unsignalized Full Movement across from Access No. 56	When either property redevelops. Cross access agreements required between property no. 2371-1930-11-007, property no. 2371-1930-11-006, and any other properties of interest upon redevelopment.
56	82.145	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
57	82.150	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 56	When property redevelops. Cross access agreements required between property no. 2371-1930-11-007, property no. 2371-1930-11-006, and any other properties of interest upon redevelopment.
58	82.155	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
59	82.17	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
60	82.18	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 61	When property redevelops.
60.5	82.19	RT	New access	None	Unsignalized Full Movement across from Access No. 61	When property redevelops.
61	82.19	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
62	82.22	LT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 63.5	When property redevelops. Cross access agreements required between property no. 2371-1930-05-029, property no. 2371-1930-05-028, and any other properties of interest upon redevelopment. A potential connection between properties along the ROW line and outside of the CO 9 traveled way may be considered.
62.5	82.230	LT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 63	When property redevelops. Cross access agreements required between property no. 2371-1930-05-029, property no. 2371-1930-05-028, and any other properties of interest upon redevelopment.
63.5	82.235	RT	New access	None	Unsignalized Full Movement across from Access No. 63	When property redevelops. Cross access agreements required between property no. 2371-1930-05-029, property no. 2371-1930-05-028, and any other properties of interest upon redevelopment. A potential connection between properties along the ROW line and outside of the CO 9 traveled way may be considered.
63	82.235	RT	Blue Grouse Trail	Unsignalized Full Movement	Unsignalized Full Movement	
64	82.27	RT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 66	When property redevelops, safety or operational issues occur, or when a public project is funded.
65	82.28	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
66	82.28	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
67	82.29	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
68	82.31	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
69	82.33	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	

Access in ROW?

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Access ID No.	Mile Post	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
	*					
70	82.35	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
71	82.36	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
72	82.37	LT	Residential	Unsignalized Full Movement	Conditional Unsignalized Full Movement	Either Access no. 72 or Access no. 73 shall remain open. If legal agreements do not exist, cross access agreements required between property no. 2371-1930-05-009, property no. 2371-1930-0-008, and any other properties of interest upon redevelopment. Access shall close if alternative access to Access no. 73 is available.
73	82.38	LT	Residential	Unsignalized Full Movement	Conditional Unsignalized Full Movement	Either Access no. 72 or Access no. 73 shall remain open. If legal agreements do not exist, cross access agreements required between property no. 2371-1930-05-009, property no. 2371-1930-0-008, and any other properties of interest upon redevelopment. Access shall close if alternative access to Access no. 72 is available.
74	82.40	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
75	82.41	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
76	82.42	LT	Residential	Unsignalized Full Movement	Shared Unsignalized Full Movement at property line	Cross access agreements required between property no. 2371-1930-05-007, property no. 2371-1930-05-006, and any other properties of interest upon redevelopment.
76.5	82.43	LT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 76	When property redevelops. Cross access agreements required between property no. 2371-1930-05-007, property no. 2371-1930-05-006, and any other properties of interest upon redevelopment.
77a	82.440	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
77b	82.445	RT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 77b	When property redevelops, safety or operational issues occur, or when a public project is funded.
78	82.45	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
78.5	82.48	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
79	82.500	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
80a	82.505	LT	Residential	Unsignalized Full Movement	Close access - share access at Access No. 80b	When property redevelops. Cross access agreements required between property no. 2371-1920-05-004, property no. 2371-1920-05-003, and any other properties of interest upon redevelopment.
80b	82.510	LT	New access	None	Shared Unsignalized Full Movement at property line	When either property redevelops. Cross access agreements required between property no. 2371-1920-05-004, property no. 2371-1920-05-003, and any other properties of interest upon redevelopment.
80c	82.515	LT	Residential	Unsignalized Full Movement	Close access - share access at Access No. 80b	When property redevelops. Cross access agreements required between property no. 2371-1920-05-004, property no. 2371-1920-05-003, and any other properties of interest upon redevelopment.
80.5	82.52	RT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 79	When property redevelops, safety or operational issues occur, or when a public project is funded.

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Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
81	82.53	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
82	82.56	LT	Residential	Unsignalized Full Movement	Close access - share access at Access No. 82.5	When property redevelops. Cross access agreements required between property no. 2371-1920-05-007, property no. 2371-1920-05-006, and any other properties of interest upon redevelopment.
82.5	82.58	LT	New access	None	Shared Unsignalized Full Movement at property line	When either property redevelops. Cross access agreements required between property no. 2371-1920-05-007, property no. 2371-1920-05-006, and any other properties of interest upon redevelopment.
82.75	82.60		New access	None	Shared Unsignalized Full Movement at property line	When either property redevelops. Cross access agreements required between property no. 2371-1920-06-041, property no. 2371-1920-06-042, and any other properties of interest upon redevelopment.
83a	82.63	RT	CR 680 (Blue River Rd)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
83b	82.64	RT	Vehicle pull-off	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
83.5	82.63	LT	New access	None	Full Movement intersection - realign Access No. 84 across from Blue River Rd (Access No. 83a). Potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	When either property redevelops. Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop. Cross access agreements required between property no. 2371-1920-05-001, property no. 2371-1920-04-011, and any other properties of interest upon redevelopment.
84	82.65	LT	Residential	Unsignalized Full Movement	Close access - realign access across from Blue River Rd (Access No. 83a)	When property redevelops. Cross access agreements required between property no. 2371-1920-05-001, property no. 2371-1920-04-011, and any other properties of interest upon redevelopment.
84.5	82.67	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted. May be used for bus stop.	
85	82.70	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
85.5	82.78	LT	New access	None	Shared Unsignalized Full Movement at property line	When either property redevelops. If legal agreements do not exist, cross access agreements required between property no. 2371-1920-04-009, property no. 2371-1920-04-008, and any other properties of interest upon redevelopment.

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Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
86	82.87	LT	Field access	Unsignalized Full Movement	Conditional Unsignalized Full Movement	If utility access is still required upon redevelopment of property, unsignalized full movement gated access will remain. If utility access is no longer required, access will close and will be provided via Access No. 86.5. If legal agreements do not exist, cross access agreements required between property no. 2371-1920-04-007, property no. 2371-1920-04-006, and any other properties of interest upon redevelopment.
86.5	82.89	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	If legal agreements do not exist, cross access agreements required between property no. 2371-1920-04-007, property no. 2371-1920-04-006, and any other properties of interest upon redevelopment.
87	82.98	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
88	83.00	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 89	When property redevelops.
88.5	83.02	RT	New access	None	Unsignalized Full Movement across from Access No. 89	When property redevelops.
89	83.02	LT	Residential	Shared Unsignalized Full Movement	Shared Unsignalized Full Movement at property line	If legal agreements do not exist, cross access agreements required between property no. 2371-1920-04-005, property no. 2371-1830-09-016, and any other properties of interest upon redevelopment.
90	83.03	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
91	83.04	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	If legal agreements do not exist, cross access agreements required between property no. 2371-1830-10-008, property no. 2371-1920-03-004, property no. 2371-1830-10-016, property no. 2371-1830-10-010, and any other properties of interest upon redevelopment.
91.5	83.04	LT	New access	None	Unsignalized Full Movement across from Access No. 91	When property redevelops.
92	83.050	LT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 91	When property redevelops.
	83.055		Milepost 83			
93	83.06	LT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 94.5	When property redevelops. Cross access agreements required between property no. 2371-1830-09-012, property no. 2371-1830-00-017, and any other properties of interest upon redevelopment.
94	83.070	LT	Residential	Unsignalized Full Movement	Close access - relocate access to property line across from Access No. 95b.	When property redevelops. If legal agreements do not exist, cross access agreements required between property no. 2371-1830-09-012, property no. 2371-1830-00-017, and any other properties of interest upon redevelopment.
94.5	83.08	LT	New access	None	Shared Unsignalized Full Movement at property line across from Access No. 95b.	When either property redevelops. If legal agreements do not exist, cross access agreements required between property no. 2371-1830-09-012, property no. 2371-1830-00-017, and any other properties of interest upon redevelopment.

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
95a	83.075	RT	Residential	Unsignalized Full Movement	Close access - relocate access to property line across from Access No. 94.5.	When property redevelops. If legal agreements do not exist, cross access agreements required between property no. 2371-1830-10-007, property no. 2371-1830-10-011, and any other properties of interest upon redevelopment.
95b	83.08	RT	New access	None	Shared Unsignalized Full Movement at property line across from Access No. 94.5	When either property redevelops. If legal agreements do not exist, cross access agreements required between property no. 2371-1830-10-007, property no. 2371-1830-10-011, and any other properties of interest upon redevelopment.
96	83.11	RT	CR 603 (Rustic Terrace)	Unsignalized Full Movement	Unsignalized Full Movement	
97a	83.16	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
97b	83.17	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
98	83.20	LT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
99	83.21	RT	CR 579 (Blue Rock Dr)	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. Movements may be restricted if operational or safety issues develop.
100	83.22	LT	Residential	Unsignalized Full Movement	Close access - access available via Access No. 98	
101	83.26	LT	CR 578 (Leap Year Trail)	Unsignalized Full Movement	Unsignalized Full Movement	
101.5	83.26	RT	New access	None	Unsignalized Full Movement across from Access No. 101	When property redevelops.
102	83.28	RT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 101	When property redevelops.
103	83.33	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
104	83.38	RT	CR 577 (Rock Springs Rd)	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. Movements may be restricted if operational or safety issues develop.
105	83.43	LT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 106	When property redevelops.
105.5	83.44	LT	New access	None	Unsignalized Full Movement across from Access No. 106	When property redevelops.
106	83.44	RT	Residential	Unsignalized Full Movement	Unsignalized Full Movement	
107	83.54	LT	Conifer Dr	Unsignalized Full Movement	Unsignalized Full Movement	If legal agreements do not exist, cross access agreements required between property no. 2371-1830-00-016, property no. 2371-1820-00-009, and any other properties of interest upon redevelopment.
107.5	83.54	RT	New access	None	Unsignalized Full Movement across from Access No. 107	When property redevelops.

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
108	83.62	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
108.5	83.80	LT	Vehicle pull-off	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
	83.94		Milepost 84			
109	83.99	LT	CR 800 (Spruce Creek Rd)	Unsignalized Full Movement	Close access - realign access CR 800 (Spruce Creek Rd) to reduce intersection skew at CO 9.	When either property redevelops, safety or operational issues occur, or when a public project is funded.
109.5	84.00	LT	Realigned CR 800 (Spruce Creek Rd)	None	Full Movement intersection - realign access CR 800 (Spruce Creek Rd) to reduce intersection skew at CO 9. Potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	When either property redevelops, safety or operational issues occur, or when a public project is funded. Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
110	84.22	RT	Parking for Goose Pasture Tarn	Unsignalized Full Movement	Unsignalized Full Movement	If legal agreements do not exist, cross access agreements required between property no. 2371-0740-00-029, property no. 2371-0740-00-023, and any other properties of interest upon redevelopment.
111	84.30	LT	Field access	Unsignalized Full Movement	Close access	When safety or operational issues occur, or when a public project is funded.
111.5	84.43	RT	Temporary construction access	None	Conditional Unsignalized Full Movement	Access to be provided as a one-way exit for support of the Goose Pasture Tarn Dam Rehabilitation project. Access to be closed upon completion of construction.
112	84.44	RT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
112.5	84.55	LT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
113	84.58	RT	CR 571 (Lakeshore Loop)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
114	84.58	LT	CR 574 (Crown Dr)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
115	84.63	LT	Residential	Unsignalized Full Movement	Close access - relocate access across from Access No. 116	When property redevelops.

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
115.5	84.64	LT	New access	None	Unsignalized Full Movement across from Access No. 116	When property redevelops.
116	84.64	RT	Rivershore Dr	Unsignalized Full Movement	Unsignalized Full Movement	
117	84.87	LT	CR 790 (Green Mountain Dr)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
118	84.87	RT	CR 550 (Horizon Ln)	Unsignalized Full Movement	Full Movement intersection - potential for auxiliary lanes and signalization (or other traffic control recognized by MUTCD) in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A signal may be implemented if and when warranted per current MUTCD standards. Once a signal is warranted and until such time as it is constructed, movements may be restricted if operational or safety issues develop.
	85.00		Milepost 85			
119	85.08	RT	CR 11 (Wagon Rd)	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. Movements may be restricted if operational or safety issues develop.
120	85.11	RT	Utility access	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
121	85.29	LT	Vehicle pull-off	Unsignalized Full Movement	Vehicle pull-off. No access to private properties permitted.	
122	85.52	RT	CR 789 (Alpenview Rd)	Unsignalized Full Movement	Conditional Unsignalized Full Movement	Access will close when alternative access to Boreas Pass Road is available.
	85.98		Milepost 86			
123	86.00	RT	Southside Dr	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. A roundabout may be considered or movements may be restricted if operational or safety issues develop.
124	86.00	LT	CR 765 (River Park Dr)	Unsignalized Full Movement	Unsignalized Full Movement intersection - potential for auxiliary lanes in the future.	Auxiliary lanes may be implemented when traffic volumes meet auxiliary lane requirements in the State Highway Access Code. A traffic signal will not be implemented regardless of warrants. A roundabout may be considered or movements may be restricted if operational or safety issues develop.
125	86.17	LT	Broken Lance Dr	Signalized Full Movement	Signalized Full Movement	
126	86.17	RT	CR 10 (Boreas Pass Rd)	Signalized Full Movement	Signalized Full Movement	

¹ Oriented from direction of reference point (S-N)

² If Towns, County or CDOT improves CO 9 or if safety or operational issues develop, access modifications may be implemented.

³ Implement with development, redevelopment or use change

Blue River - ACCESS CONTROL PLAN
Carroll Ln (MP 77.49) to Broken Lance Dr (MP 86.26)
December 31, 2020

* Mile Posts defined per by the approximate CDOT reference point (milepost) (in hundredths of a mile) based on CDOT Highway Data Explorer.

Access ID No.	Mile Post *	Side ¹	Description/Current Business	Existing Configuration	Proposed Configuration ^{4,7}	Conditions for Implementation ^{2,3,4,5,6}
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⁴ MUTCD - Manual on Uniform Traffic Control Devices

⁵ Unless otherwise specified, conditions listed refer to proposed configuration.

⁶ Access closures are conditional upon alternative access to the highway or local street system. Refer to alternative access listed in proposed configuration.

⁷ Conditional proposed configurations may be further restricted under certain circumstances. Refer to conditions for implementation.

Appendix H-Conceptual Trail Design and Opinion of Probable Cost

PoDI / NHS

FHWA PROJECTS OF DIVISION INTEREST (PoDI)? NO YES

NATIONAL HIGHWAY SYSTEM? NO YES

DEPARTMENT OF TRANSPORTATION STATE OF COLORADO

Related Projects:
P. E. UNDER PROJECT: XXXXXXXXX
Project Number: XXXXX
Project Code: XXXXX

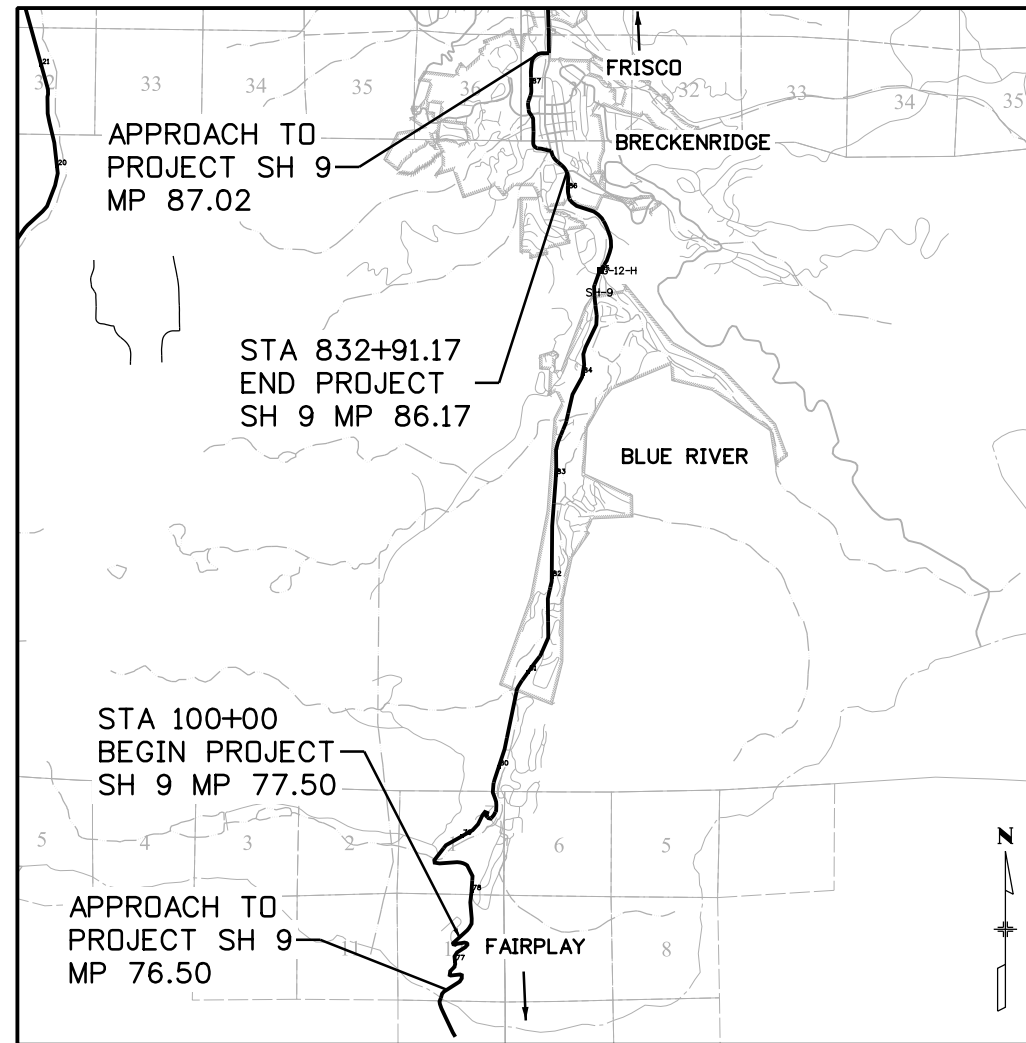
R.O.W. Projects:
R.O.W. Project Description: XXXXXXXXXXXXXXXXX

PROJECT DESCRIPTION

THE TOWN OF BLUE RIVER, THE TOWN OF BRECKENRIDGE, SUMMIT COUNTY, AND THE COLORADO DEPARTMENT OF TRANSPORTATION HAVE IDENTIFIED A NEED TO DEVELOP AN ACCESS CONTROL PLAN FOR SH 9 BETWEEN MP 77.50 (CARROLL LANE) AND MP 86.26 (BROKEN LANCE DRIVE) IN SUMMIT COUNTY, A TOTAL DISTANCE OF APPROXIMATELY 8.76 MILES. THE ACCESS CONTROL PLAN WILL GIVE CDOT AND THE LOCAL AGENCIES A LONG-RANGE PLANNING DOCUMENT TO IDENTIFY ACCESS POINTS ALONG THE STATE HIGHWAY. IN ADDITION TO THE ACCESS CONTROL PLAN, THE PROJECT TEAM IS DEVELOPING A CONCEPTUAL (15%) TRAIL DESIGN WITHIN THE STUDY LIMITS TO EXPAND ON THE HOOSIER PASS RECREATIONAL PATHWAY FEASIBILITY STUDY THAT WAS COMPLETED IN 2013. THE CONCEPTUAL TRAIL DESIGN WILL GIVE LOCAL AGENCIES A STARTING POINT TO PURSUE FUTURE FUNDING OPPORTUNITIES.

HIGHWAY CONSTRUCTION BID PLANS OF PROPOSED STATE HIGHWAY NO. 9 SUMMIT COUNTY CONSTRUCTION PROJECT CODE NO. 22768

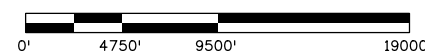
DESIGN DATA	DESIGN CRITERIA	
	BLUE RIVER TRAIL	SH 9
MAXIMUM GRADE	8%	--
MINIMUM DESIGN SPEED	20 MPH for grades less than 4% 30 MPH FOR GRADES STEEPER THAN 4%	--
MINIMUM LANE WIDTH	12 ft.	--
MINIMUM SHOULDER WIDTH	2 ft.	--
MAXIMUM SUPERELEVATION	2%	--
CLEAR ZONE DISTANCE (TANGENT)	2 ft. min.	--
2018 EXISTING TRAFFIC - ADT	--	5,704 - 15,976
2018 EXISTING TRAFFIC - TRUCK PERCENTAGE	--	1.6% - 2.9%
2040 DESIGN TRAFFIC - ADT	--	9,934 - 20,998
2038 DESIGN TRAFFIC - TRUCK PERCENTAGE	--	1.6% - 2.9%



SHEET NO.	INDEX OF SHEETS
1	TITLE SHEET
2 to 3	TYPICAL SECTIONS
4 to 76	ROADWAY PLANS
77 to 124	PROFILES

TABULATION OF LENGTH & DESIGN DATA		
STATION	SH 9 FEET	
APPROACH TO PROJECT MP 76.50	5280	
BEGIN PROJECT STA 100+00, MP 77.50	50,744	
END PROJECT STA 832+91.17, MP 86.17	5280	
APPROACH TO PROJECT MP 87.02	61304	
TOTAL (FEET)	61304	
SUMMARY OF PROJECT LENGTH		
ROADWAY (NET LENGTH)	17361	3.29
PROJECT GROSS LENGTH	50669	9.60

PROJECT LOCATION MAP



NOTE:
STATIONING FOR PROJECT IS NOT CONTIGUOUS.

CONCEPTUAL PLANS

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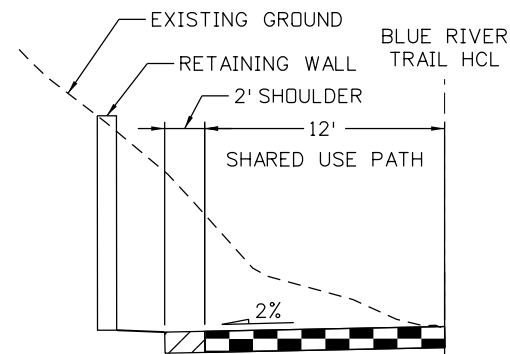
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Contract Information
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Resident Engineer:
Project Engineer:
PROJECT STARTED: / / ACCEPTED: / /
Comments:

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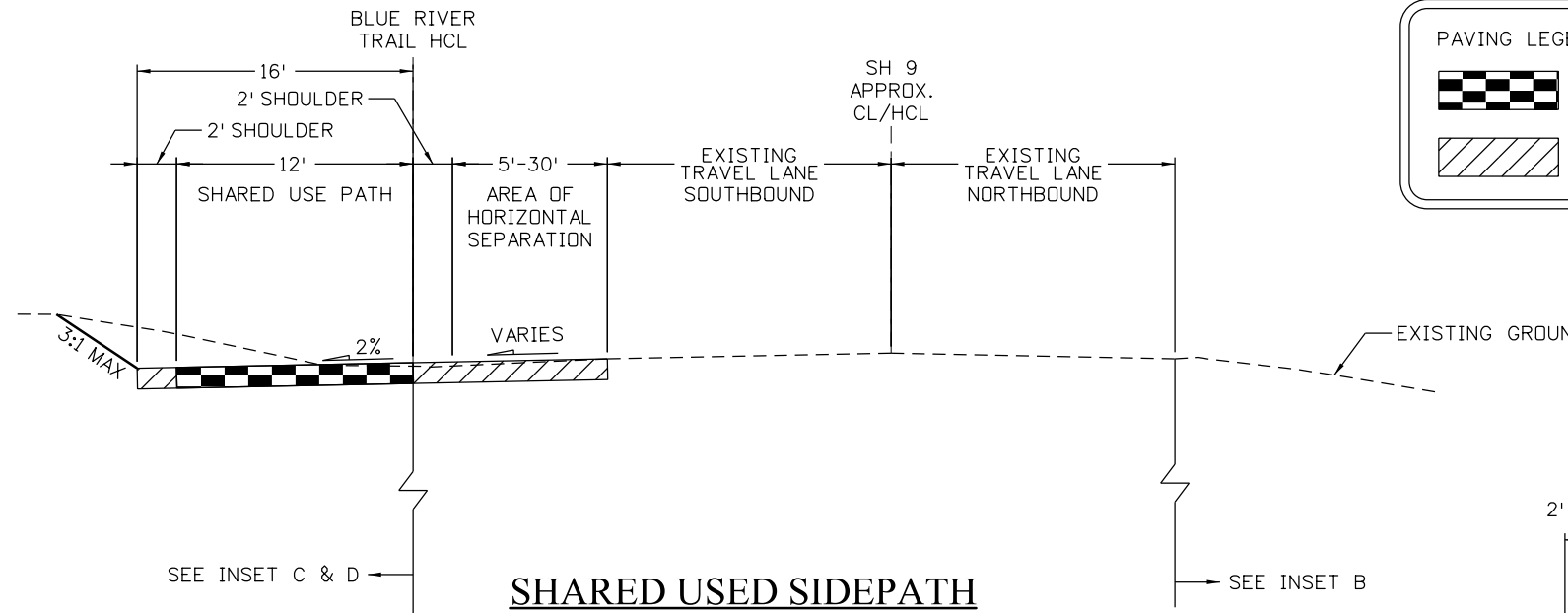


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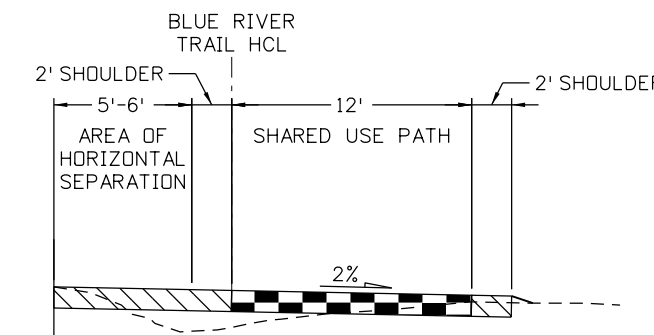
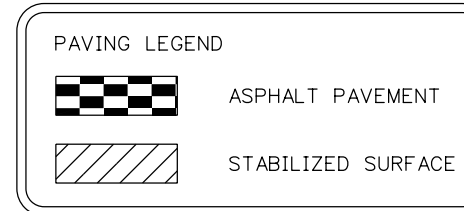
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STA. 709+21.29 TO STA. 720+04.97
STA. 728+98.70 TO STA. 734+98.84



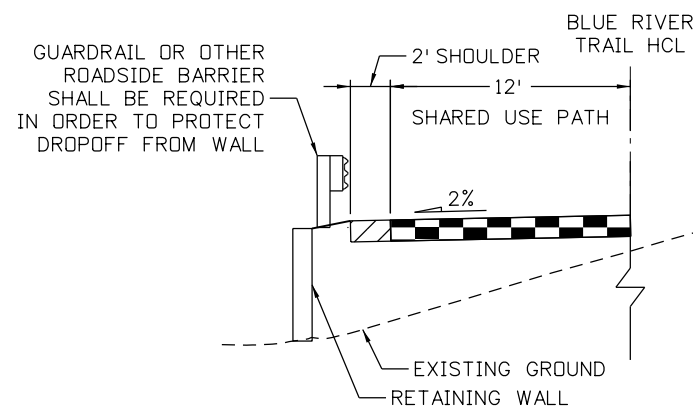
SHARED USED SIDEPATH

BLUE RIVER TRAIL
STA. 524+89.57 TO STA. 775+60.00



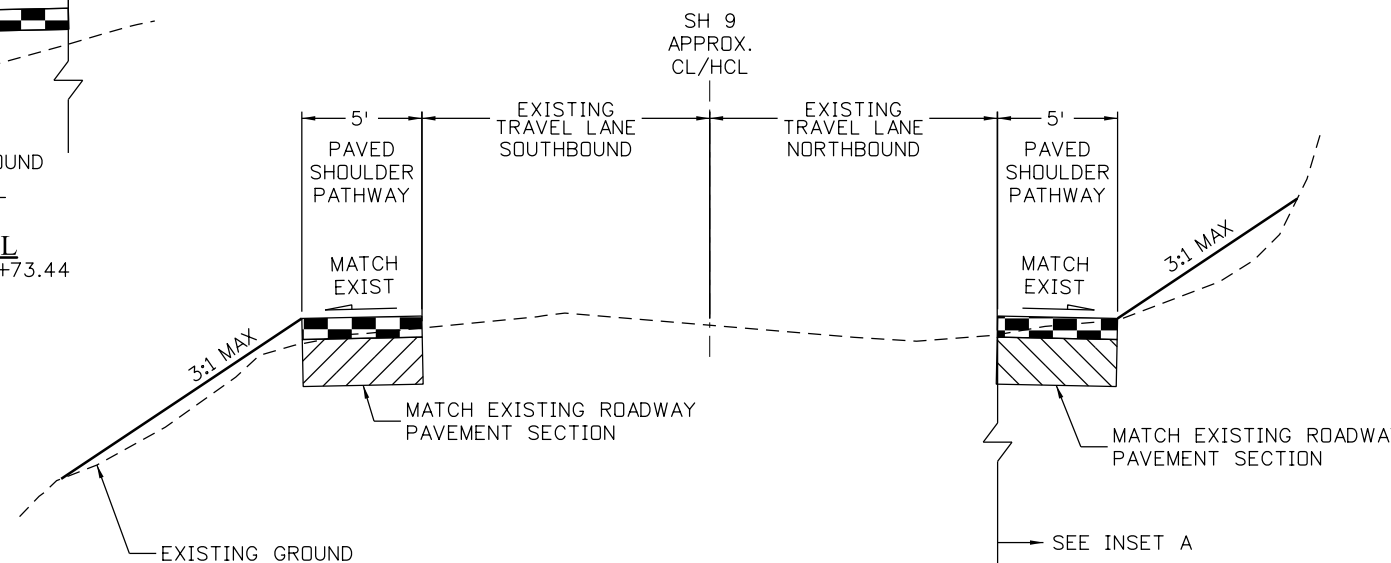
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STA. 651+80 TO STA. 675+40



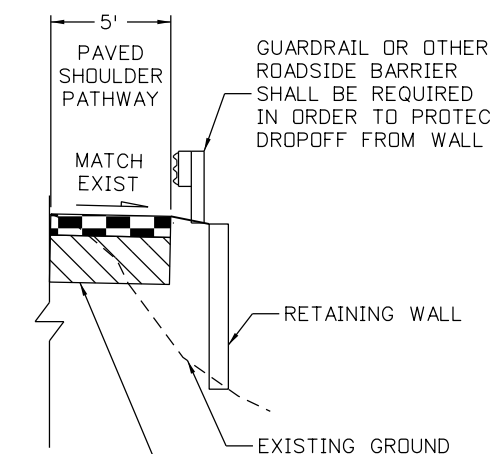
INSET D - FILL WALL

STA. 762+48.80 TO STA. 763+73.44



HIGHWAY - WIDENED SHOULDERS

SH 9
STA. 100+00.00 TO STA. 273+60.61



INSET A - FILL WALL

STA. 185+92.15 TO STA. 187+70.78
STA. 236+11.17 TO STA. 241+23.79
STA. 246+49.34 TO STA. 246+87.93
STA. 248+43.59 TO STA. 248+93.59

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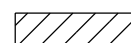
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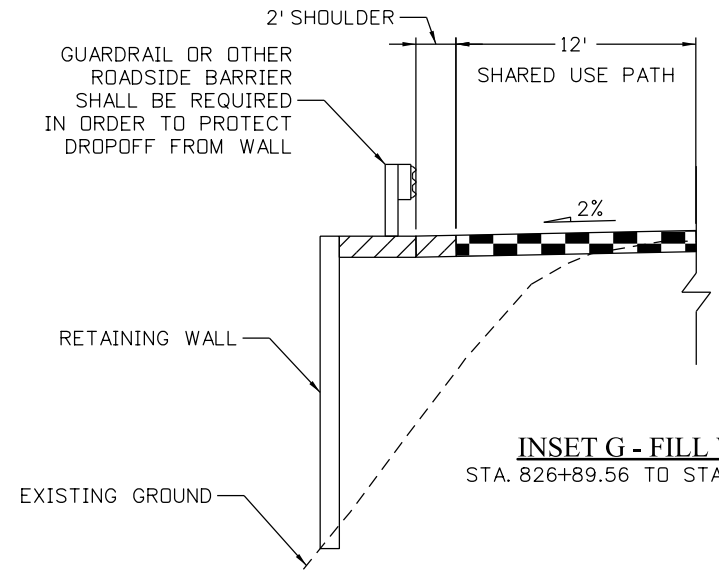


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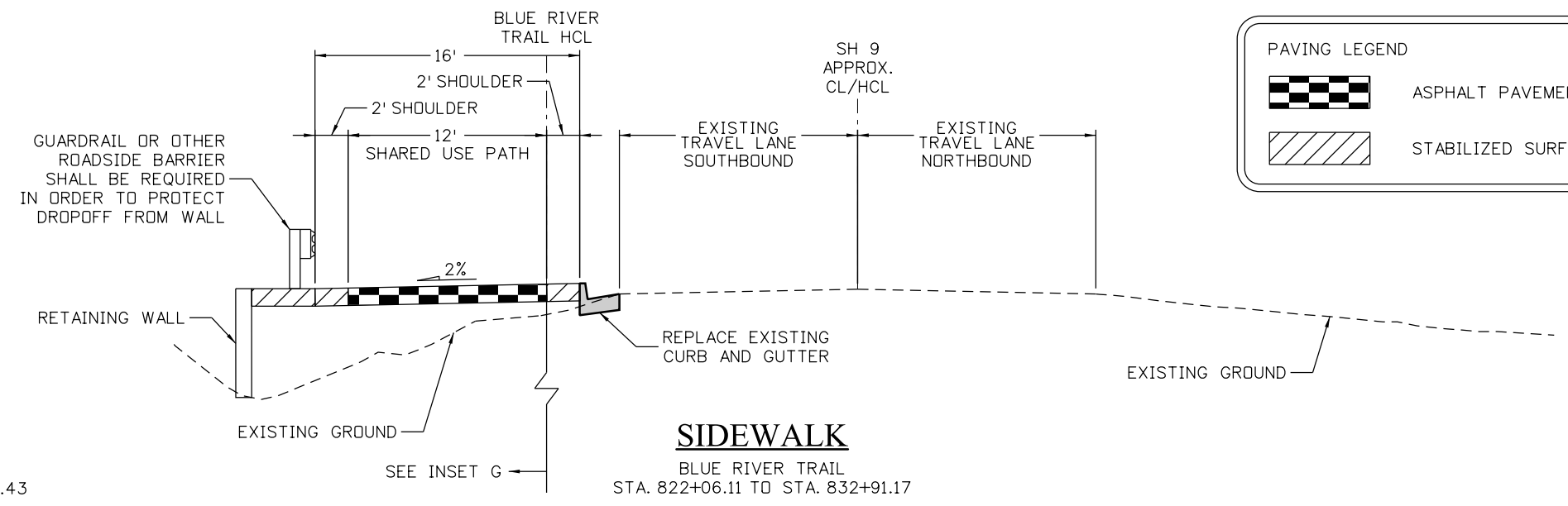
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 ASPHALT PAVEMENT

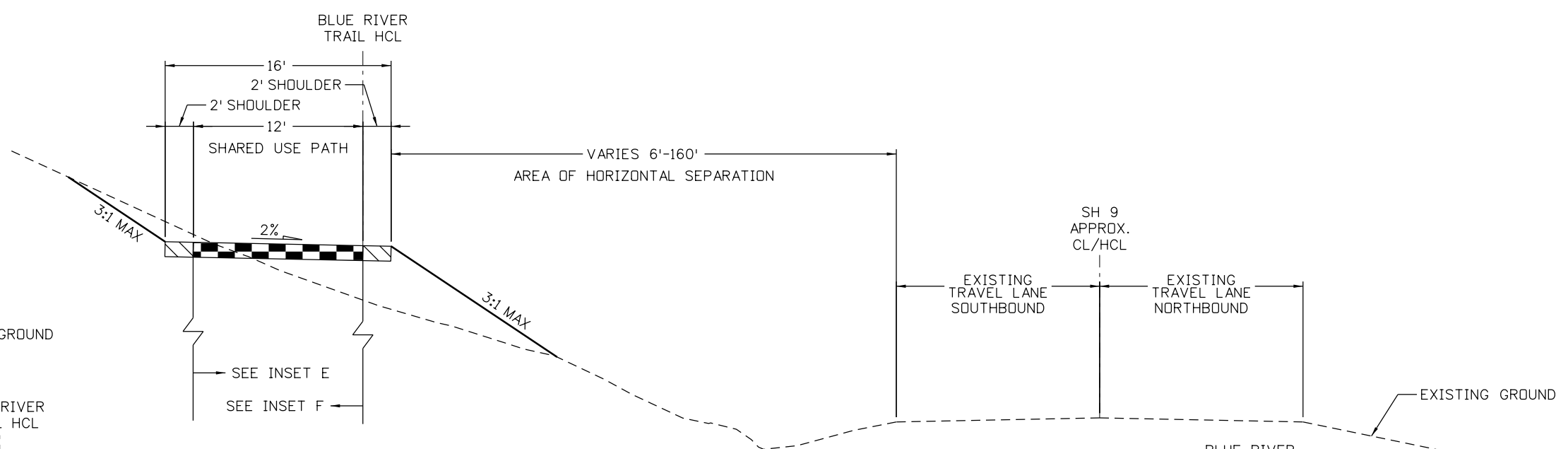
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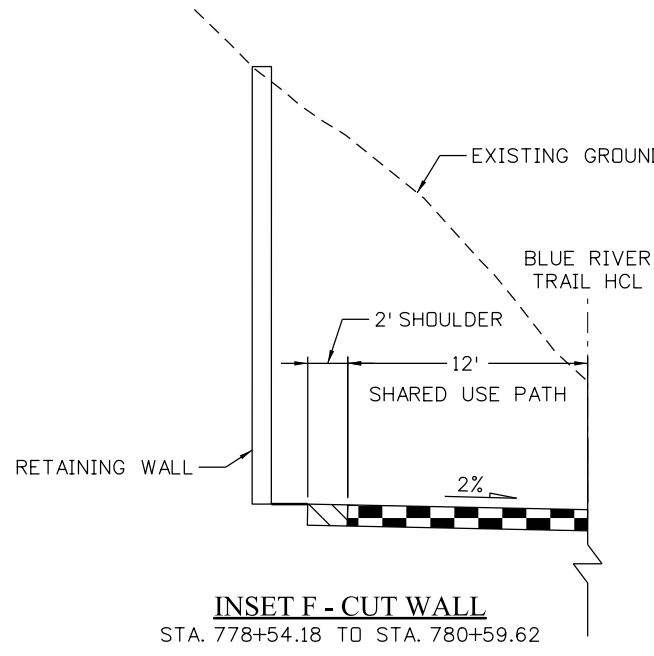
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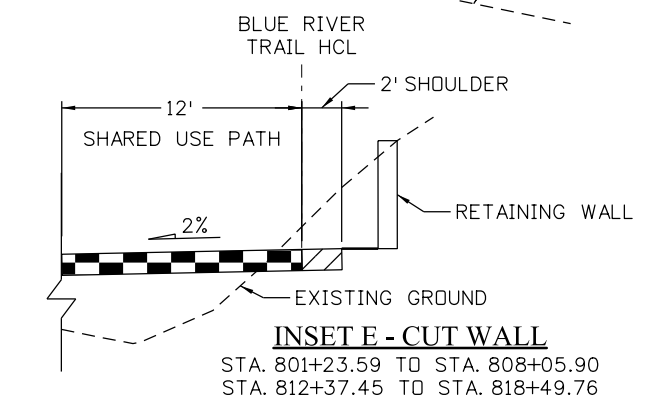
SIDEWALK
BLUE RIVER TRAIL
STA. 822+06.11 TO STA. 832+91.17



SHARED USE TRAIL
BLUE RIVER TRAIL
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STA. 775+60.00 TO STA. 821+89.70



INSET F - CUT WALL
STA. 778+54.18 TO STA. 780+59.62



INSET E - CUT WALL
STA. 801+23.59 TO STA. 808+05.90
STA. 812+37.45 TO STA. 818+49.76



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Detailer:	EDT	Subset Sheets:	02 of 02
Sheet Subset:	TYPICAL		

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XXXXXXXXX
22768
Sheet Number 3



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101+00

102+00

103+00

104+00

MATCH LINE - 105+00.00

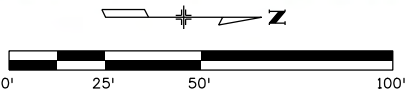
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TOE OF FILL

CD 9

SHOULDER WIDENING (TYP.)

EXISTING EDGE OF PAVEMENT



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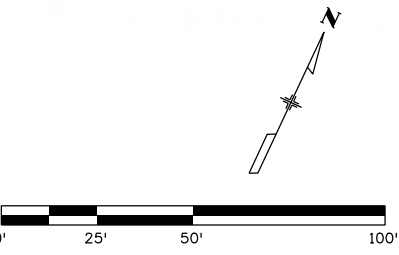
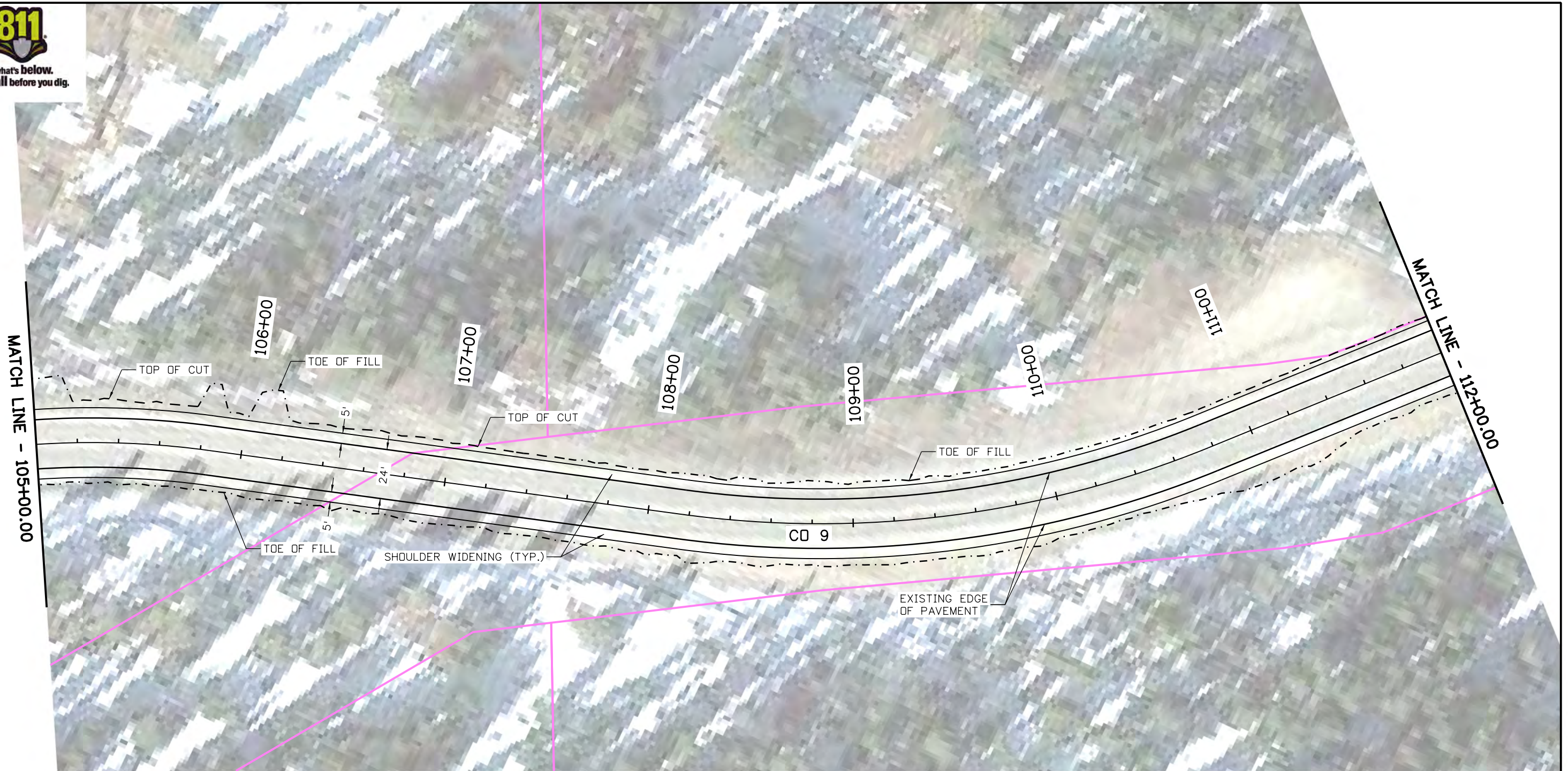
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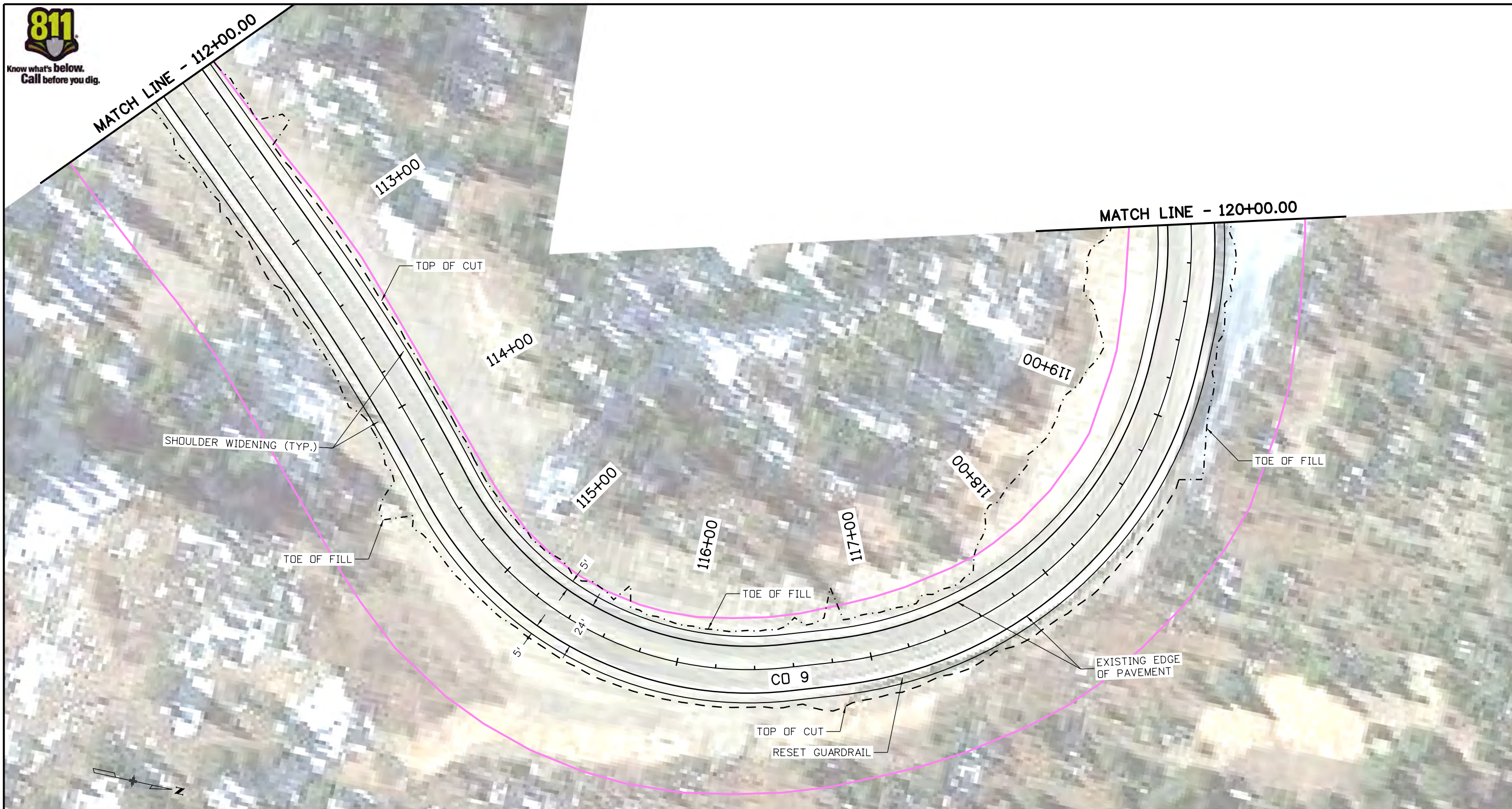
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Designer:	CSA	Structure
Detailer:	NG	Numbers
Sheet Subset:	TRAIL	Subset Sheets: 02 of 73

Project No./Code
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23001
Sheet Number

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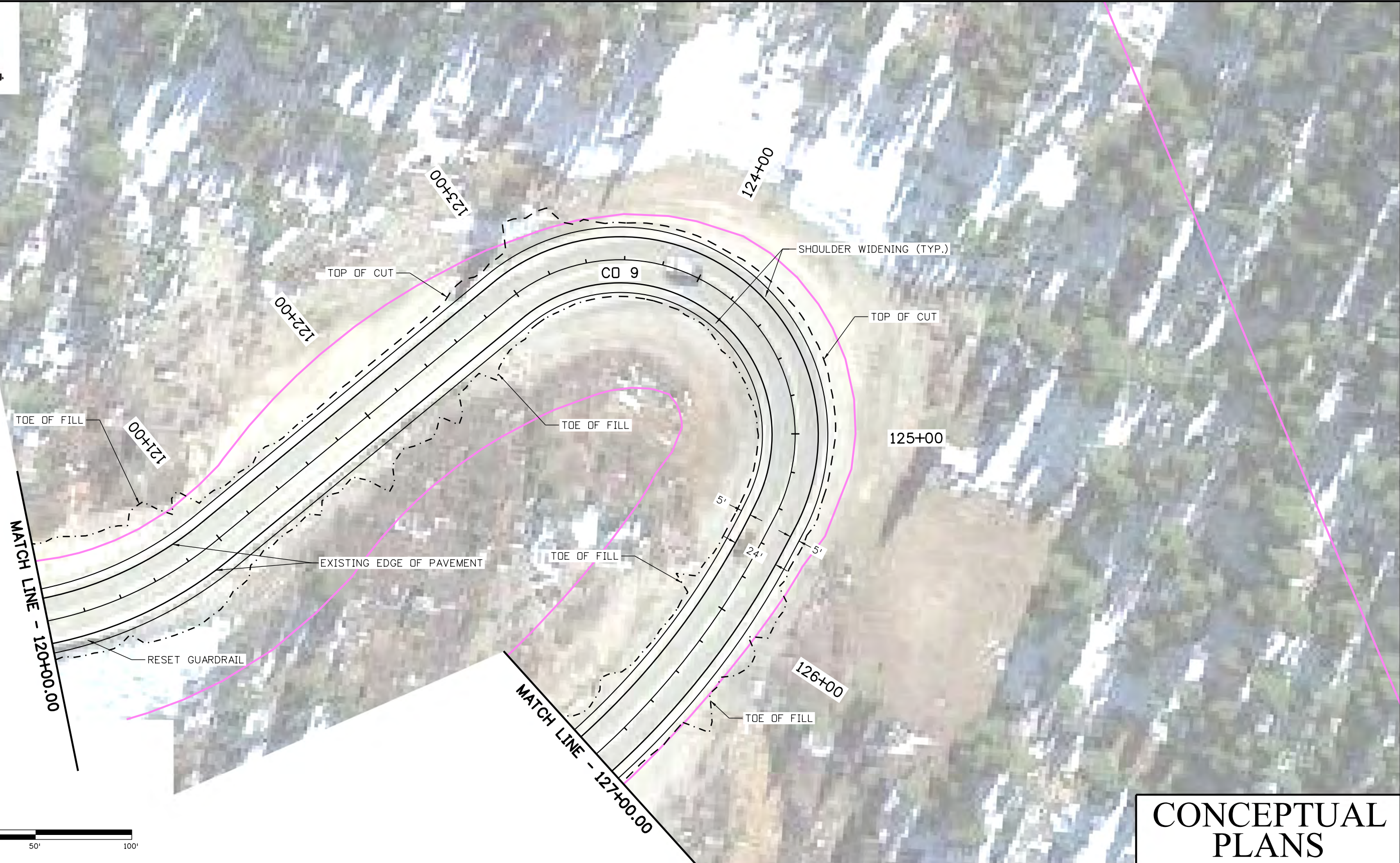
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Project No./Code
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23001
Sheet Number 6

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Sheet Number 7

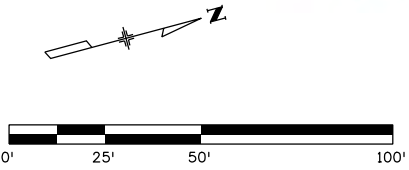


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CONCEPTUAL PLANS

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Print Date: 3/12/2020
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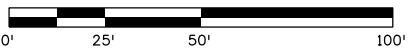
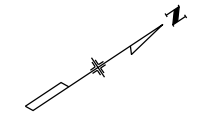
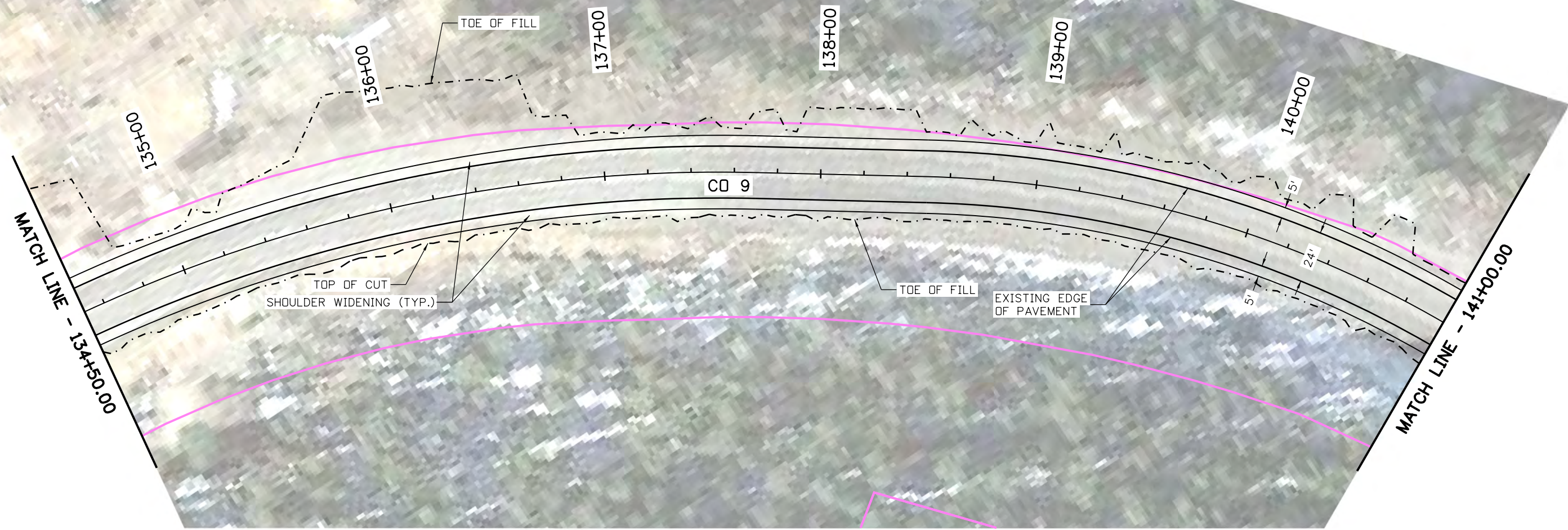
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Detailer:	NG	Subset Sheets:	05 of 73
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Sheet Number 8




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CONCEPTUAL PLANS

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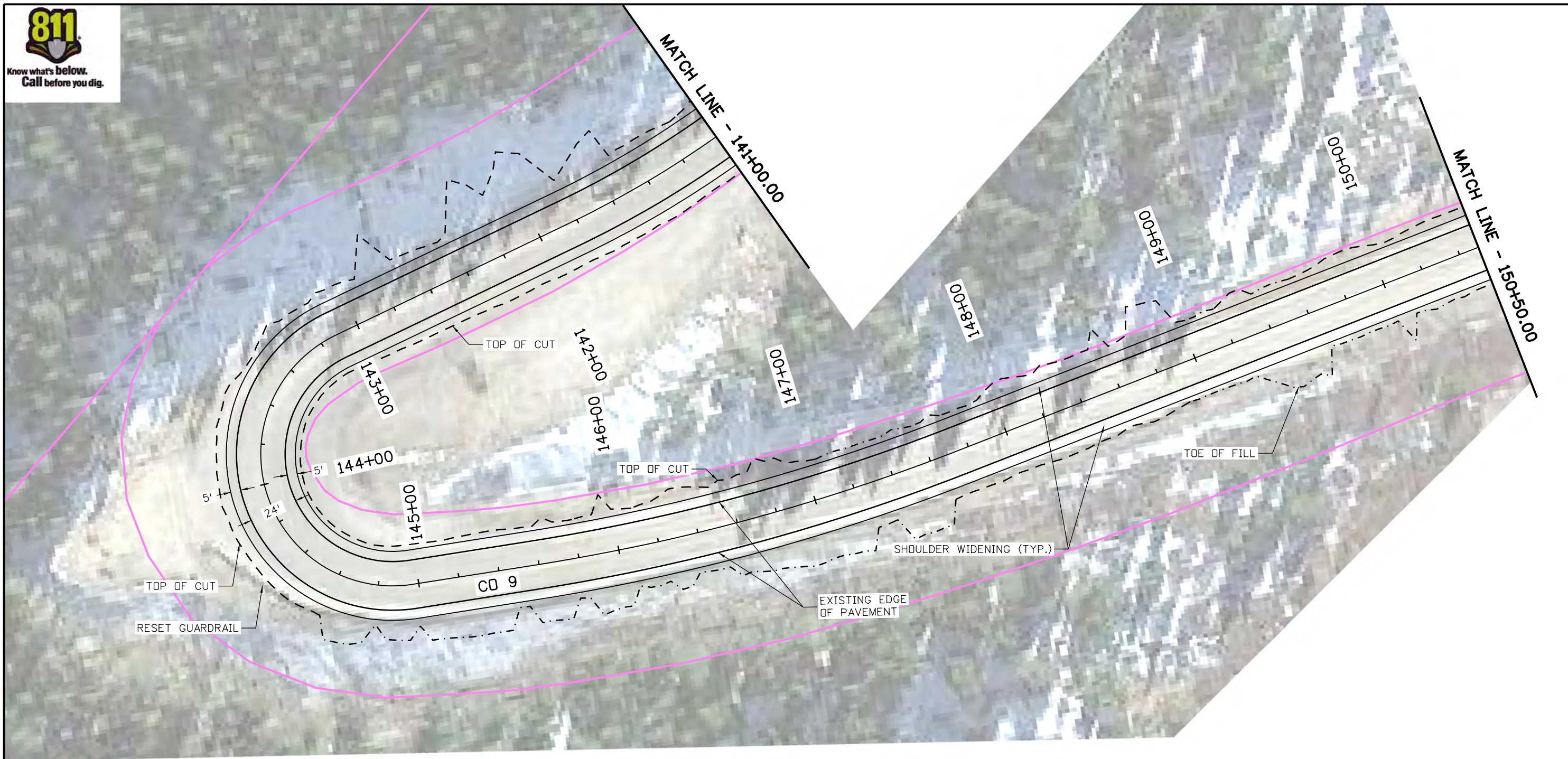
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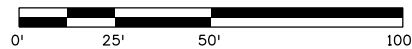
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MATCH LINE - 150+50.00



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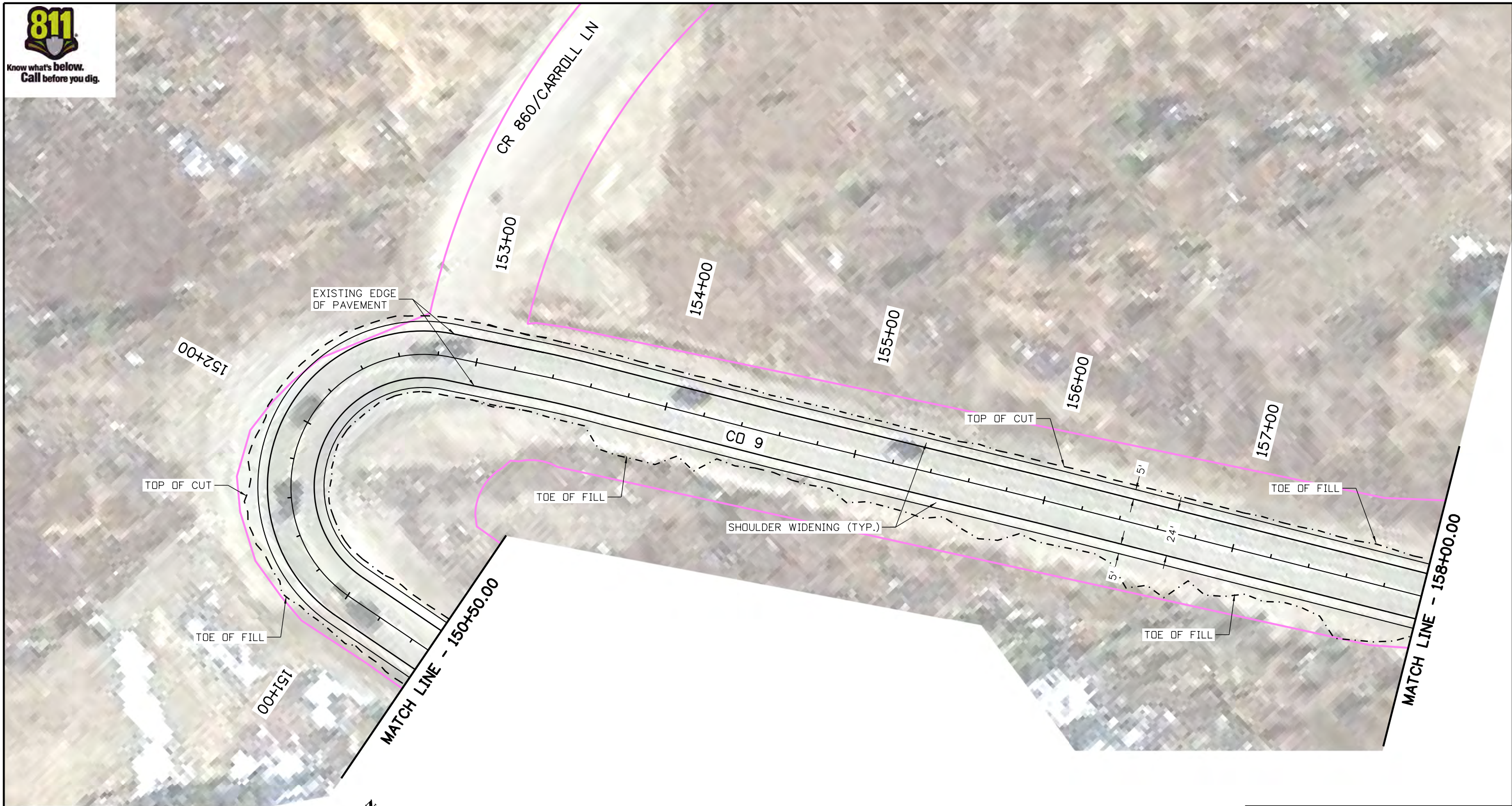
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Project No./Code
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Sheet Number 10

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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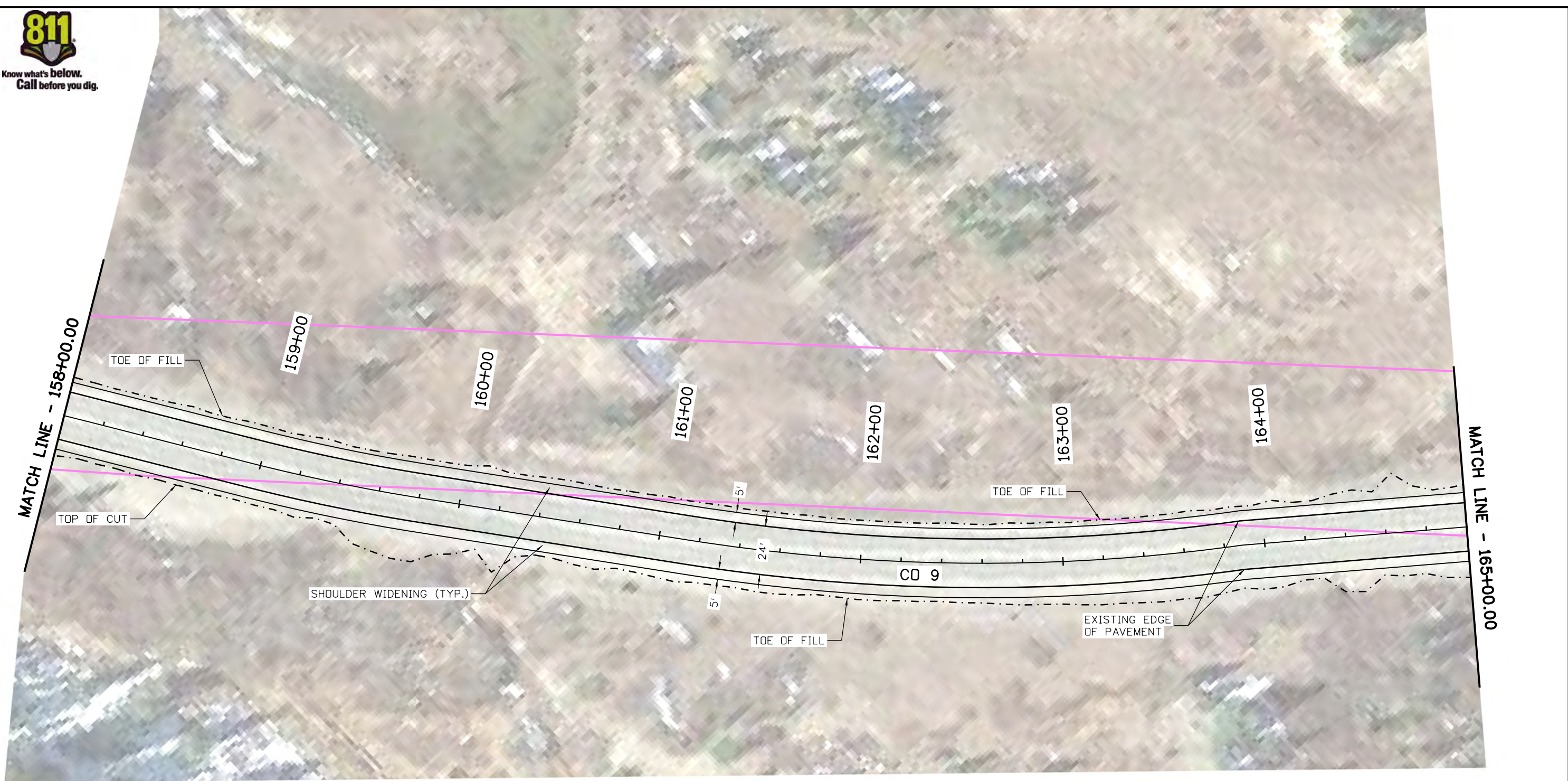
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Project No./Code
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Sheet Number 11

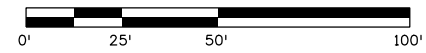
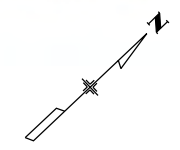
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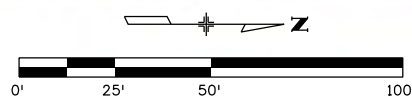
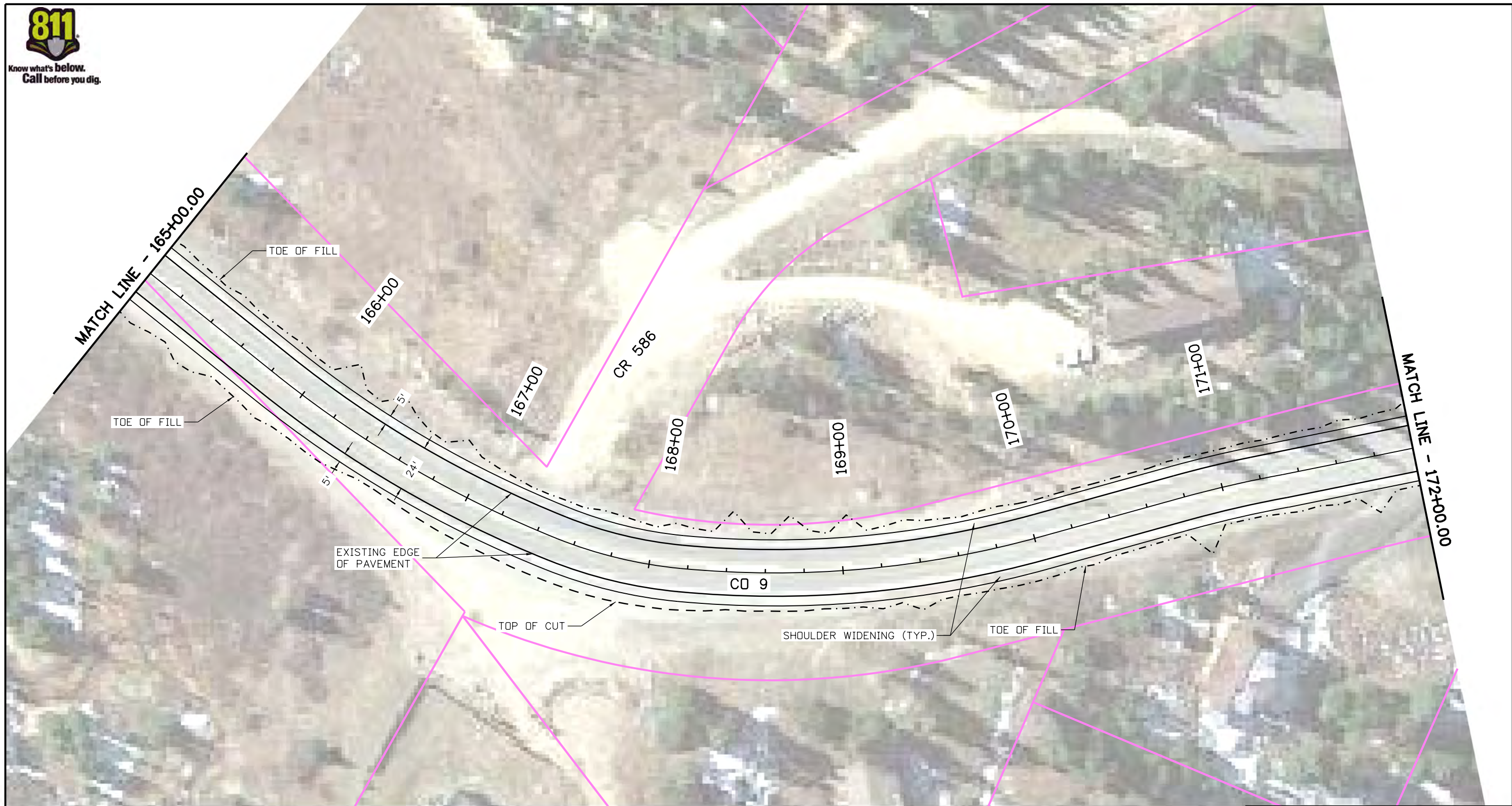
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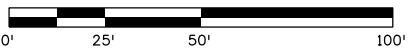
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Sheet Number 13

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CONCEPTUAL PLANS

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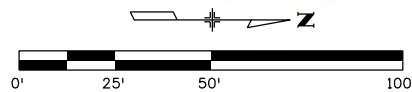
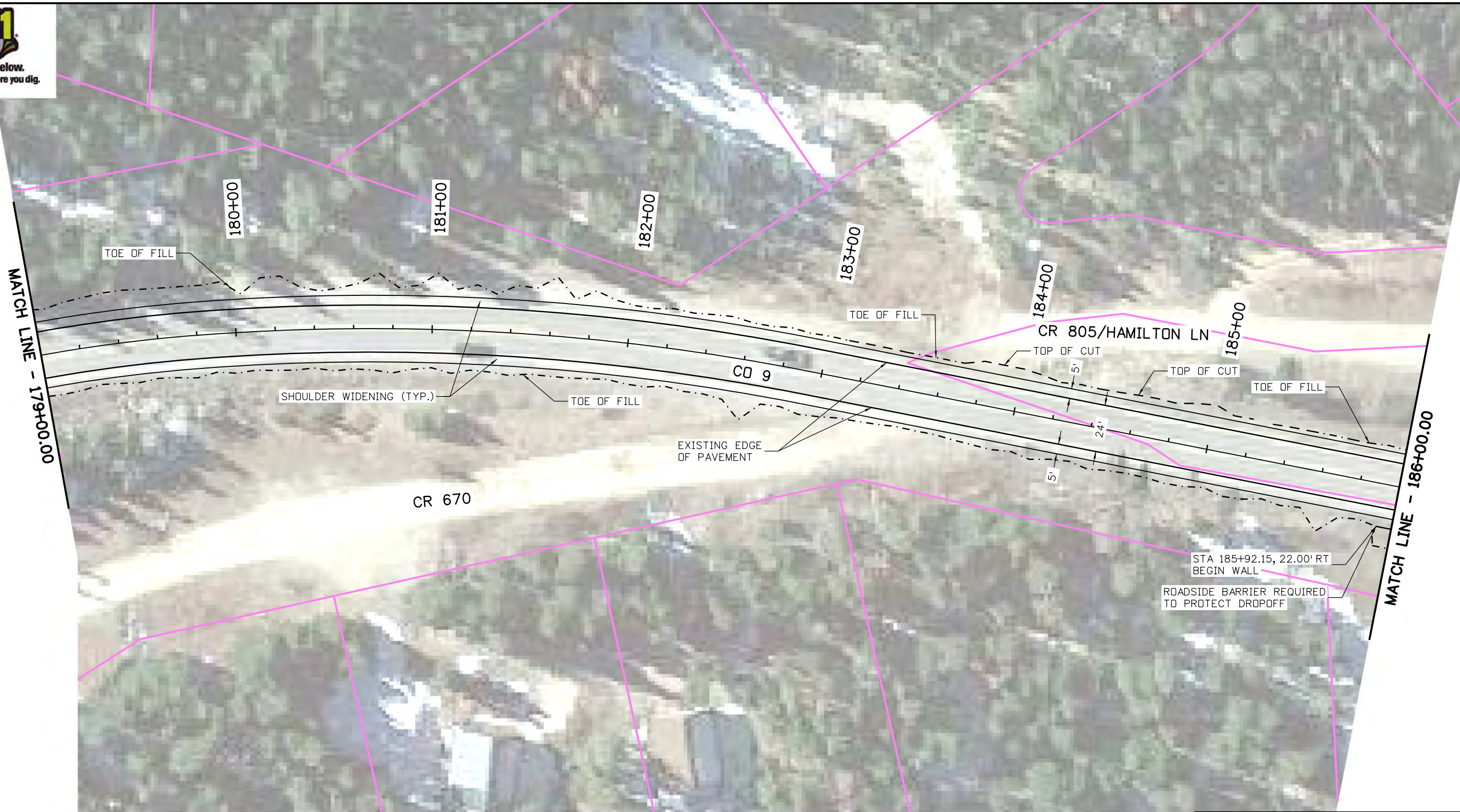
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Sheet Number 14

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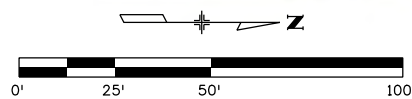
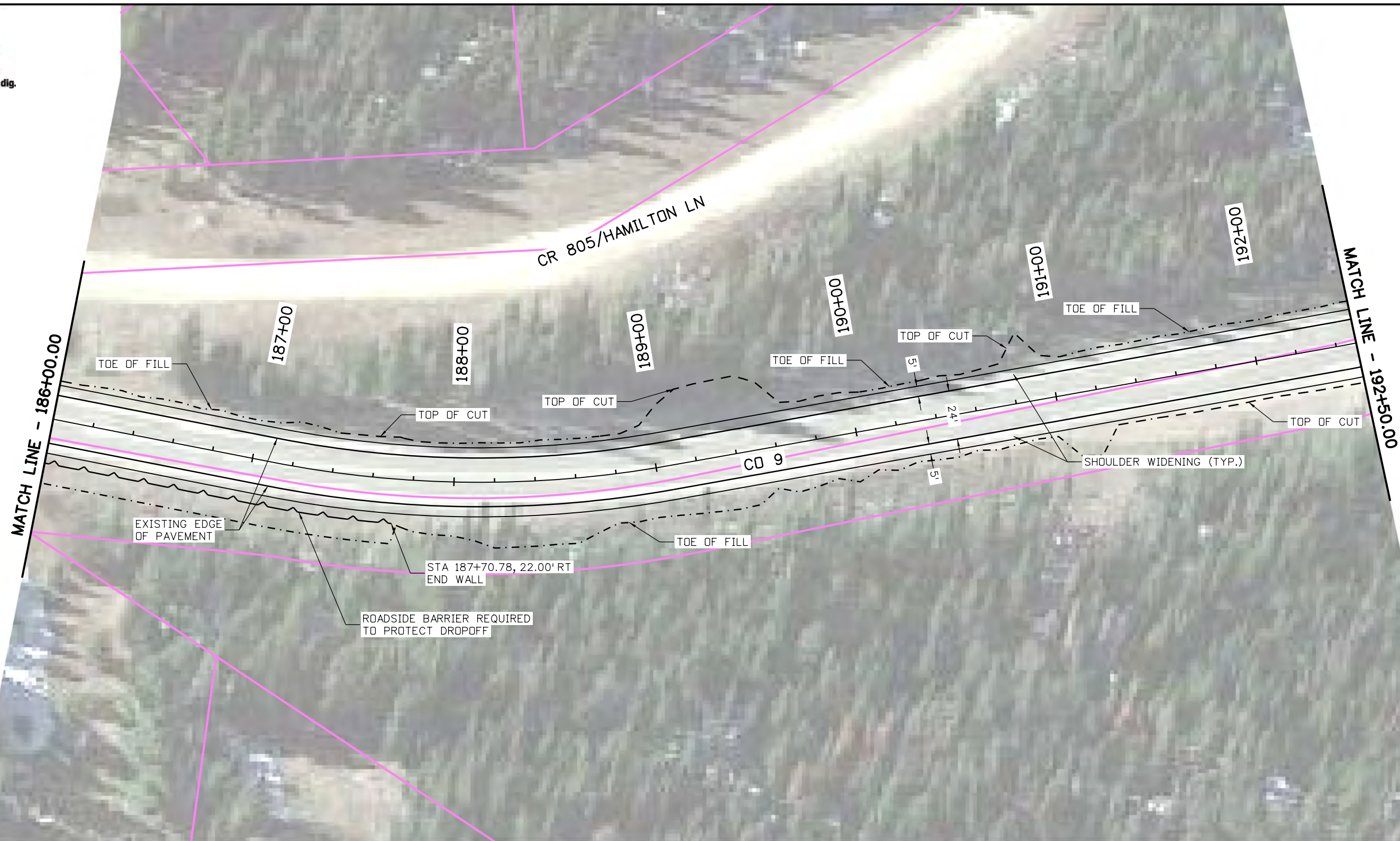
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Sheet Subset:	TRAIL		

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CONCEPTUAL PLANS

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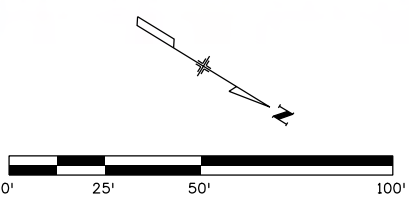
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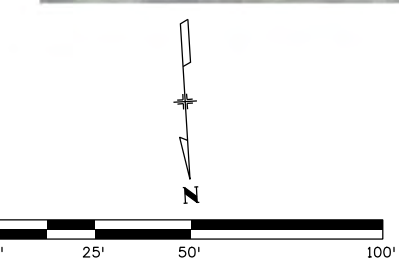
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23001
Sheet Number 17

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Stolfus 5690 DTC BOULEVARD, SUITE 560E GREENWOOD VILLAGE, CO 80111 PHONE: 303-221-2330 FAX: 303-221-2331 WWW.STOLFUSANDASSOCIATES.COM

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
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Sheet Subset:	TRAIL	Subset Sheets:	15 of 73

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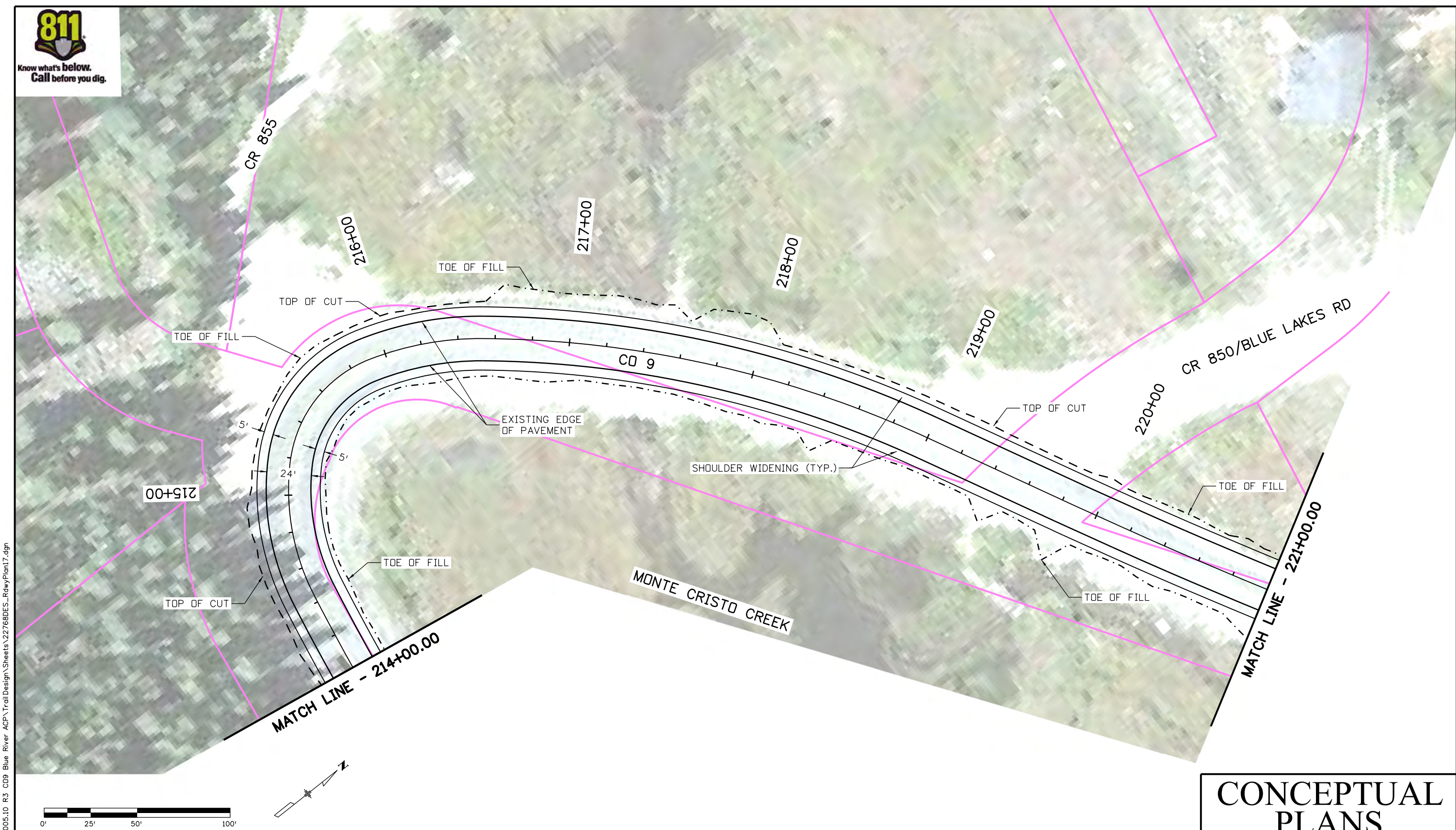
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Detailer:	NG		
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Project No./Code
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23001
Sheet Number 19

811

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CD 9 BLUE RIVER TRAIL PLANS			
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Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	17 of 73

Project No./Code
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23001
Sheet Number 20

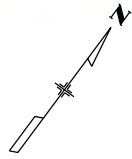
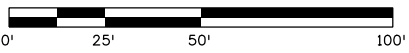


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MATCH LINE - 221+00.00

MATCH LINE - 228+50.00



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CD 9 BLUE RIVER TRAIL PLANS			
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Project No./Code
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23001
Sheet Number 21

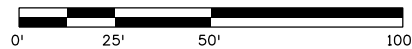
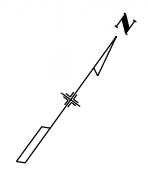


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CD 9 BLUE RIVER TRAIL PLANS

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Detailer:	NG	Subset Sheets:	19 of 73
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Sheet Number 22

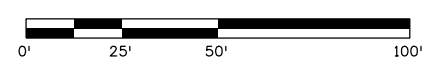
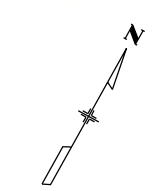
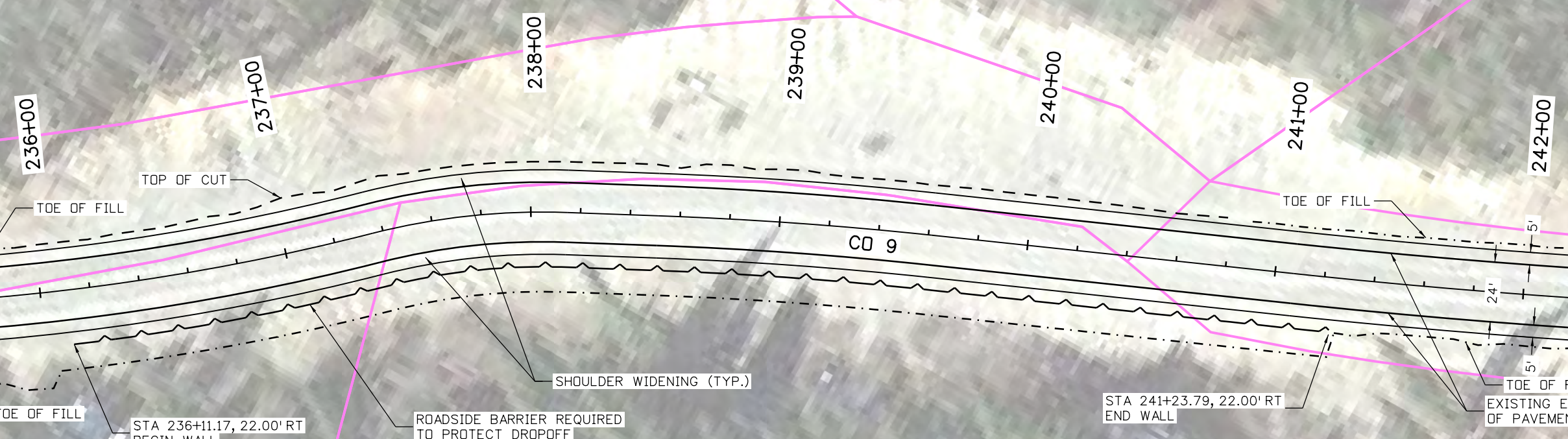


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MATCH LINE - 235+50.00

MATCH LINE - 242+50.00



CONCEPTUAL PLANS

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Date:	Comments	Init.

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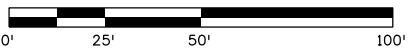
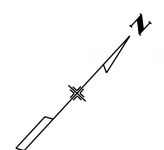
As Constructed
No Revisions:
Revised:
Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	20 of 73
Sheet Subset:	TRAIL		

Project No./Code
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23001
Sheet Number 23



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CONCEPTUAL PLANS

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Revised:
Void:

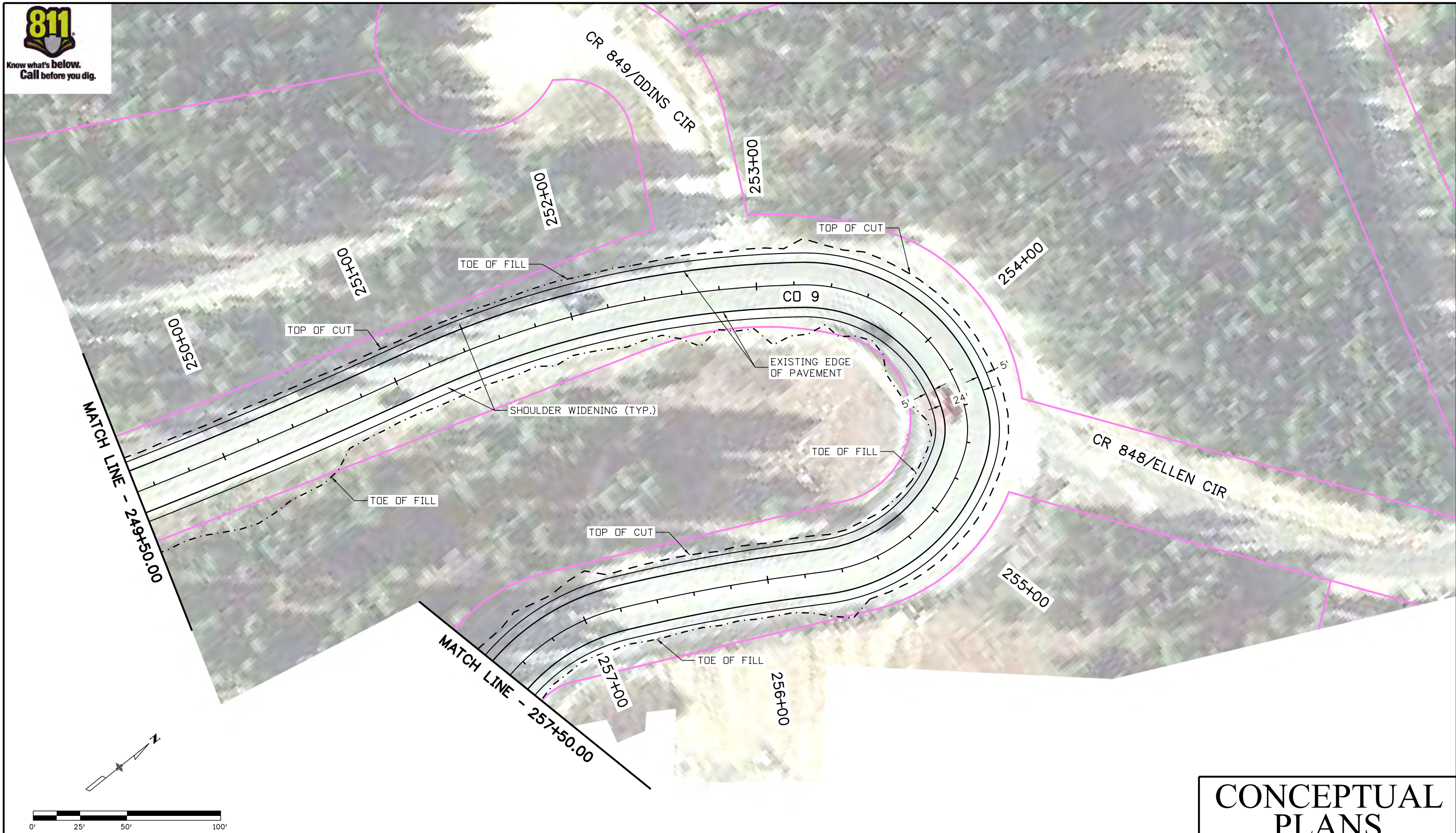
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Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	21 of 73

Project No./Code
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23001
Sheet Number 24

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811

Know what's below.
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CONCEPTUAL PLANS

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Void:

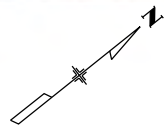
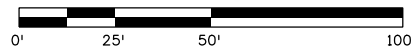
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Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	22 of 73
Sheet Subset:	TRAIL		

Project No./Code
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23001
Sheet Number 25

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CONCEPTUAL PLANS

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Void:

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Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 23 of 73

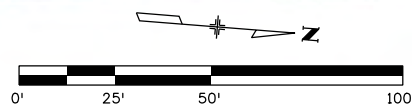
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23001
Sheet Number 26

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CONCEPTUAL PLANS

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	24 of 73
Sheet Subset:	TRAIL		

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23001
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CONCEPTUAL PLANS

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File Name: 22768DES_RdwyPlan25.dgn
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Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	25 of 73

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Know what's below.
Call before you dig.

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505+00

506+00

507+00

508+00

509+00

510+00

MATCH LINE - 511+00.00

TOP OF CUT

16'

BLUE RIVER TRAIL

TOE OF FILL

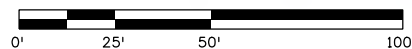
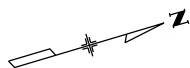
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TOP OF CUT

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CD 9



CONCEPTUAL PLANS

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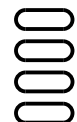
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Horiz. Scale: 1:50

Vert. Scale: As Noted



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CD 9 BLUE RIVER TRAIL PLANS

Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	26 of 73

Project No./Code

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23001

Sheet Number 29

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Know what's below.
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MATCH LINE - 511+00.00

512+00

513+00

514+00

515+00

516+00

517+00

MATCH LINE - 518+00.00

TOP OF CUT

BLUE RIVER TRAIL

TOE OF FILL

TOP OF CUT

TOE OF FILL

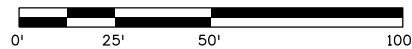
TOP OF CUT

TOP OF CUT

TOE OF FILL

16'

CD 9



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Print Date: 3/12/2020
File Name: 22768DES_RdwyPlan27.dgn
Horiz. Scale: 1:50 Vert. Scale: As Noted
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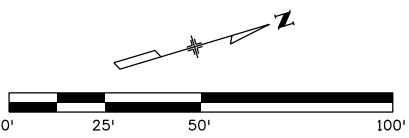
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Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	27 of 73

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23001
Sheet Number 30



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CONCEPTUAL PLANS

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	28 of 73
Sheet Subset:	TRAIL		

Project No./Code
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23001
Sheet Number 31

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Know what's below.
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MATCH LINE - 532+00.00

526+00

527+00

528+00

529+00

530+00

531+00

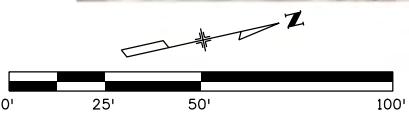
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TOE OF FILL

BLUE RIVER TRAIL

EXISTING EDGE OF PAVEMENT


CD 9



CONCEPTUAL PLANS

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Print Date: 3/12/2020
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 Horiz. Scale: 1:50 Vert. Scale: As Noted



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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	29 of 73

Project No./Code	
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23001	
Sheet Number	32



Know what's below.
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533+00

534+00

535+00

536+00

537+00

538+00

MATCH LINE - 539+00.00

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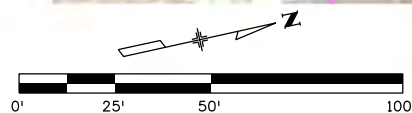
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TOE OF FILL

BLUE RIVER TRAIL

CD 9

EXISTING EDGE OF PAVEMENT



CONCEPTUAL PLANS

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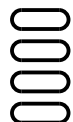
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Horiz. Scale: 1:50

Vert. Scale: As Noted



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CD 9 BLUE RIVER TRAIL PLANS

Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	30 of 73
Sheet Subset:	TRAIL		

Project No./Code

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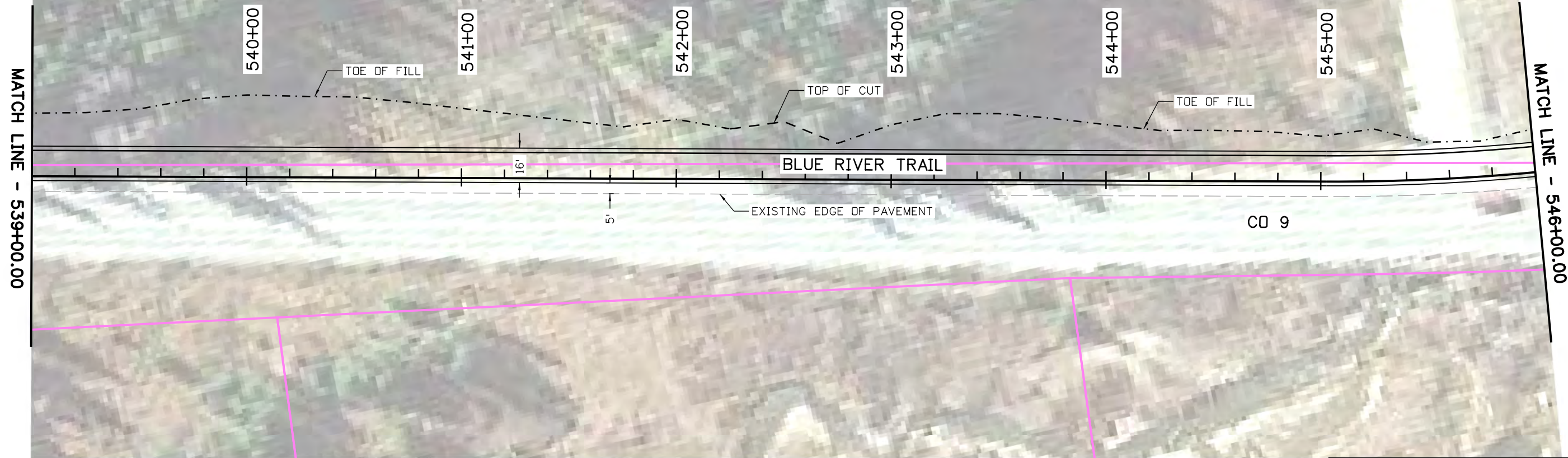
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Sheet Number 33

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CONCEPTUAL PLANS

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Print Date: 3/12/2020

File Name: 22768DES_RdwyPlan31.dgn

Horiz. Scale: 1:50

Vert. Scale: As Noted



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Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	31 of 73
Sheet Subset:	TRAIL		

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CONCEPTUAL PLANS


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Print Date: 3/12/2020
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Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	32 of 73
Sheet Subset:	TRAIL		

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CONCEPTUAL PLANS

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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
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Detailer:	NG	Subset Sheets:	33 of 73
Sheet Subset:	TRAIL		

Project No./Code
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23001
Sheet Number 36

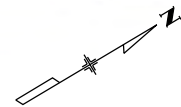
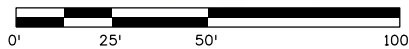
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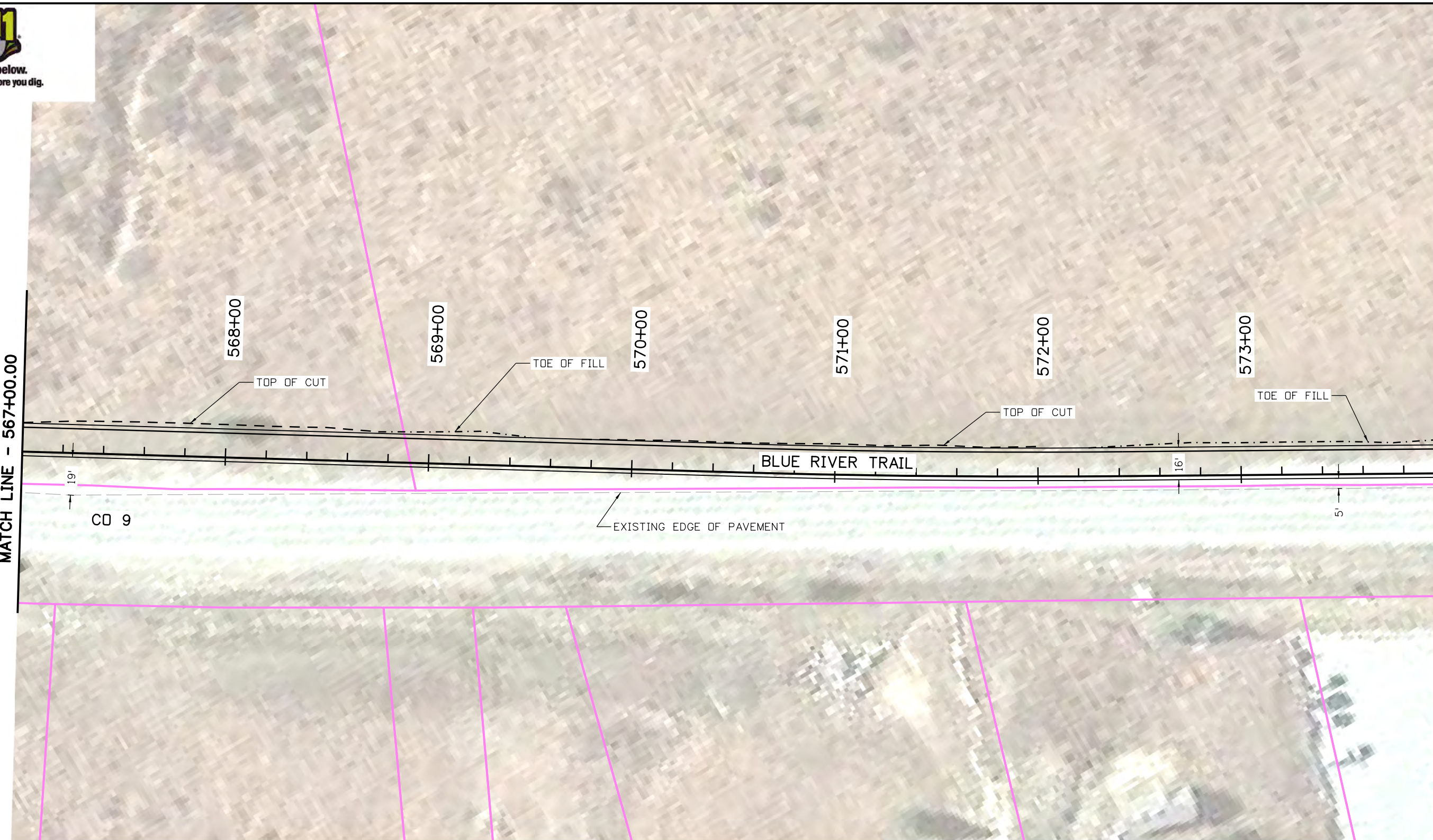
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Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 34 of 73

Project No./Code
XXXXXXXXXX
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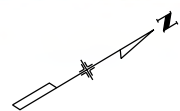
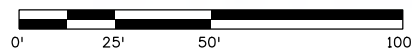


Know what's below.
Call before you dig.

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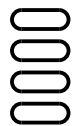
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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
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Detailer:	NG	Subset Sheets:	35 of 73
Sheet Subset:	TRAIL		

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23001
Sheet Number 38

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Know what's below.
Call before you dig.

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MATCH LINE - 581+00.00

575+00

576+00

577+00

578+00

579+00

580+00

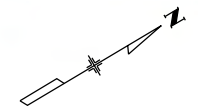
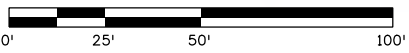
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BLUE RIVER TRAIL

EXISTING EDGE OF PAVEMENT


CD 9

WHISPERING PINES LN



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Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan36.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted



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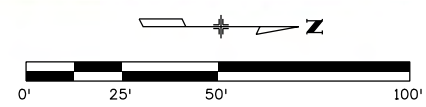
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Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	36 of 73

Project No./Code
XXXXXXXX
23001
Sheet Number 39

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CONCEPTUAL PLANS

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Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	37 of 73

Project No./Code
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Sheet Number 40

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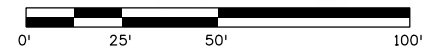


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MATCH LINE - 595+00.00



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Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	38 of 73

Project No./Code
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Sheet Number 41



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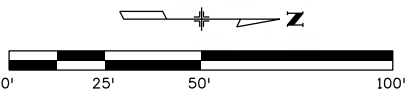
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CR 589/CALLE DE PLATA RD



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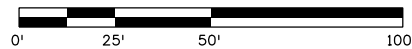
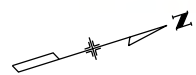
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Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 39 of 73

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Sheet Number 42



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Detailer:	NG		
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CONCEPTUAL PLANS

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	41 of 73
Sheet Subset:	TRAIL		

Project No./Code
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23001
Sheet Number 44

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Know what's below.
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MATCH LINE - 623+00.00

617+00

618+00

619+00

620+00

621+00

622+00

DRIVEWAY ACCESS SHALL BE MAINTAINED

DRIVEWAY ACCESS SHALL BE MAINTAINED

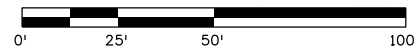
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TOE OF FILL

BLUE RIVER TRAIL

CD 9

EXISTING EDGE OF PAVEMENT



CONCEPTUAL PLANS

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Horiz. Scale: 1:50

Vert. Scale: As Noted



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CD 9 BLUE RIVER TRAIL PLANS

Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	42 of 73
Sheet Subset:	TRAIL		

Project No./Code

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23001

Sheet Number 45

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Call before you dig.

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MATCH LINE - 630+00.00

624+00

625+00

626+00

627+00

628+00

629+00

TOE OF FILL

TOP OF CUT

DRIVEWAY ACCESS SHALL BE MAINTAINED

TOP OF CUT

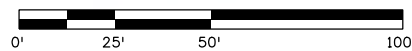
BLUE RIVER TRAIL

CD 9

EXISTING EDGE OF PAVEMENT

16'

12'



CONCEPTUAL PLANS

Print Date: 3/12/2020

File Name: 22768DES_RdwyPlan43.dgn

Horiz. Scale: 1:50

Vert. Scale: As Noted



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CD 9 BLUE RIVER TRAIL PLANS

Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	43 of 73
Sheet Subset:	TRAIL		

Project No./Code

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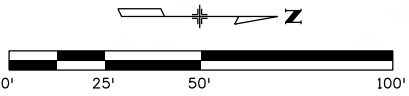
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Sheet Number 46

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CONCEPTUAL PLANS

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File Name: 22768DES_RdwyPlan44.dgn
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Detailer:	NG	Numbers	
Sheet Subset:	TRAIL	Subset Sheets:	44 of 73

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Know what's below.
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CONCEPTUAL PLANS

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	45 of 73

Project No./Code
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23001
Sheet Number 48

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Know what's below.
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CONCEPTUAL PLANS

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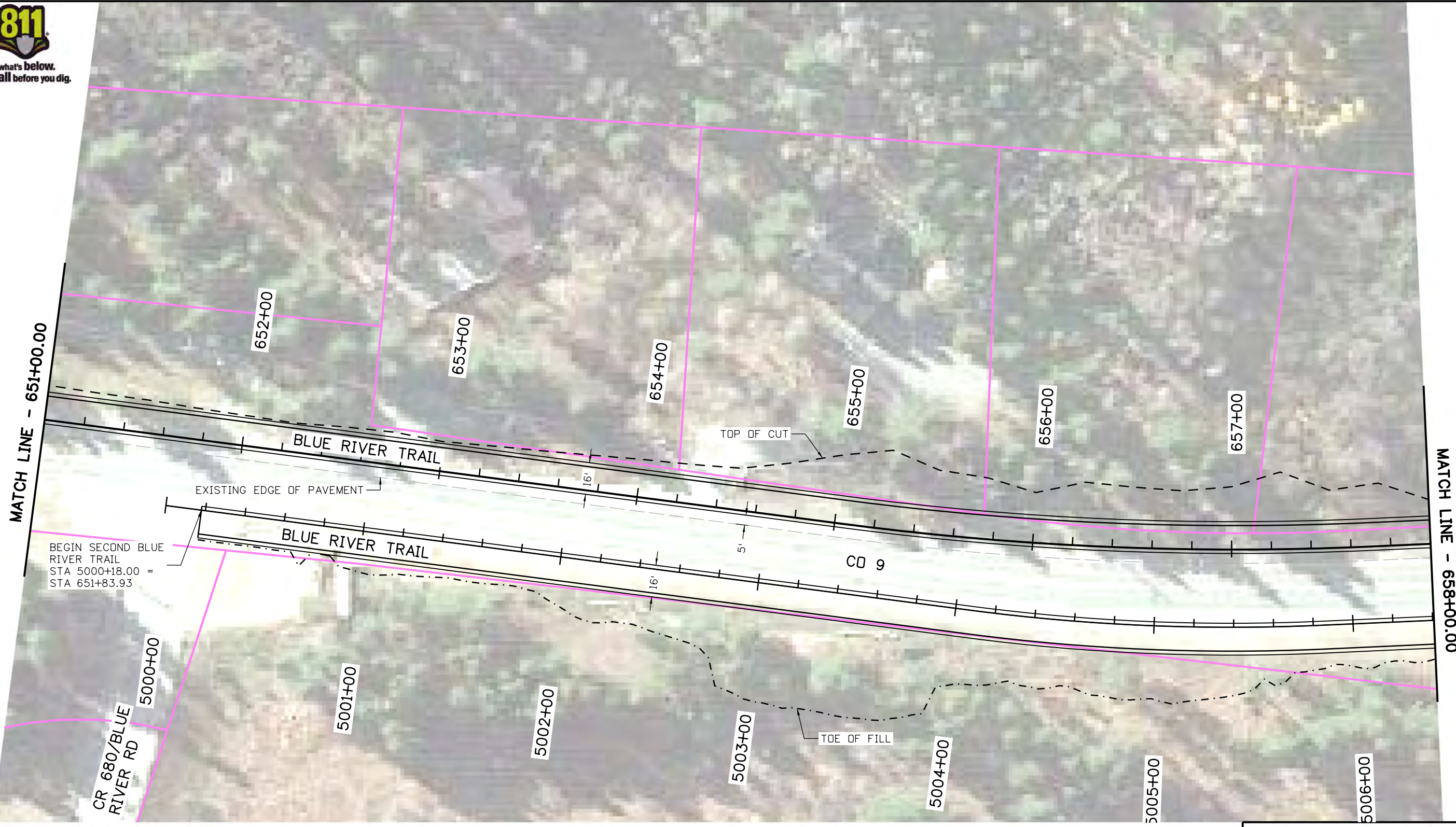
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Detailer:	NG	
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Project No./Code
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23001
Sheet Number 49

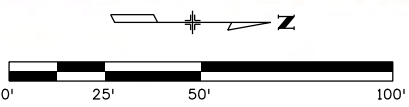


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BEGIN SECOND BLUE RIVER TRAIL
STA 5000+18.00 =
STA 651+83.93

CR 680/BLUE RIVER RD



CONCEPTUAL PLANS

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Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	47 of 73

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MATCH LINE - 665+00.00



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CD 9 BLUE RIVER TRAIL PLANS			
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Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	48 of 73

Project No./Code
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23001
Sheet Number 51

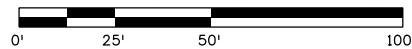


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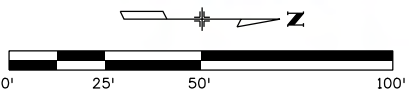
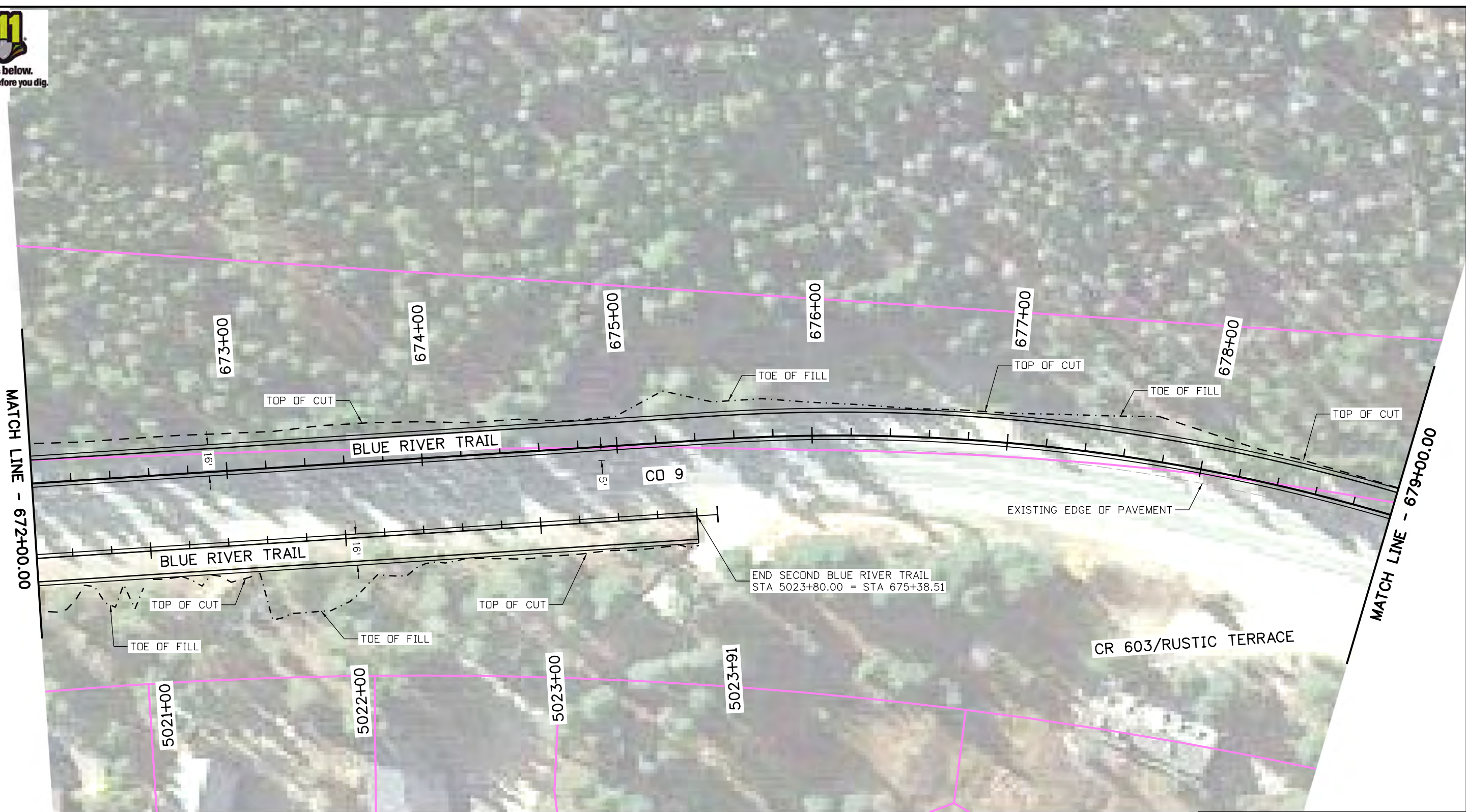
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Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 49 of 73

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XXXXXXXXX
23001
Sheet Number 52

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	50 of 73

Project No./Code
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23001
Sheet Number 53

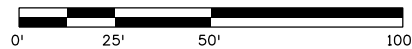
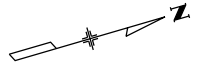


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CD 9 BLUE RIVER TRAIL PLANS

Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	51 of 73
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Project No./Code

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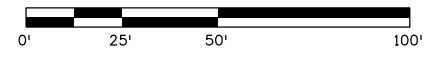
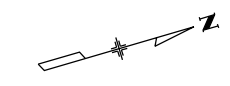
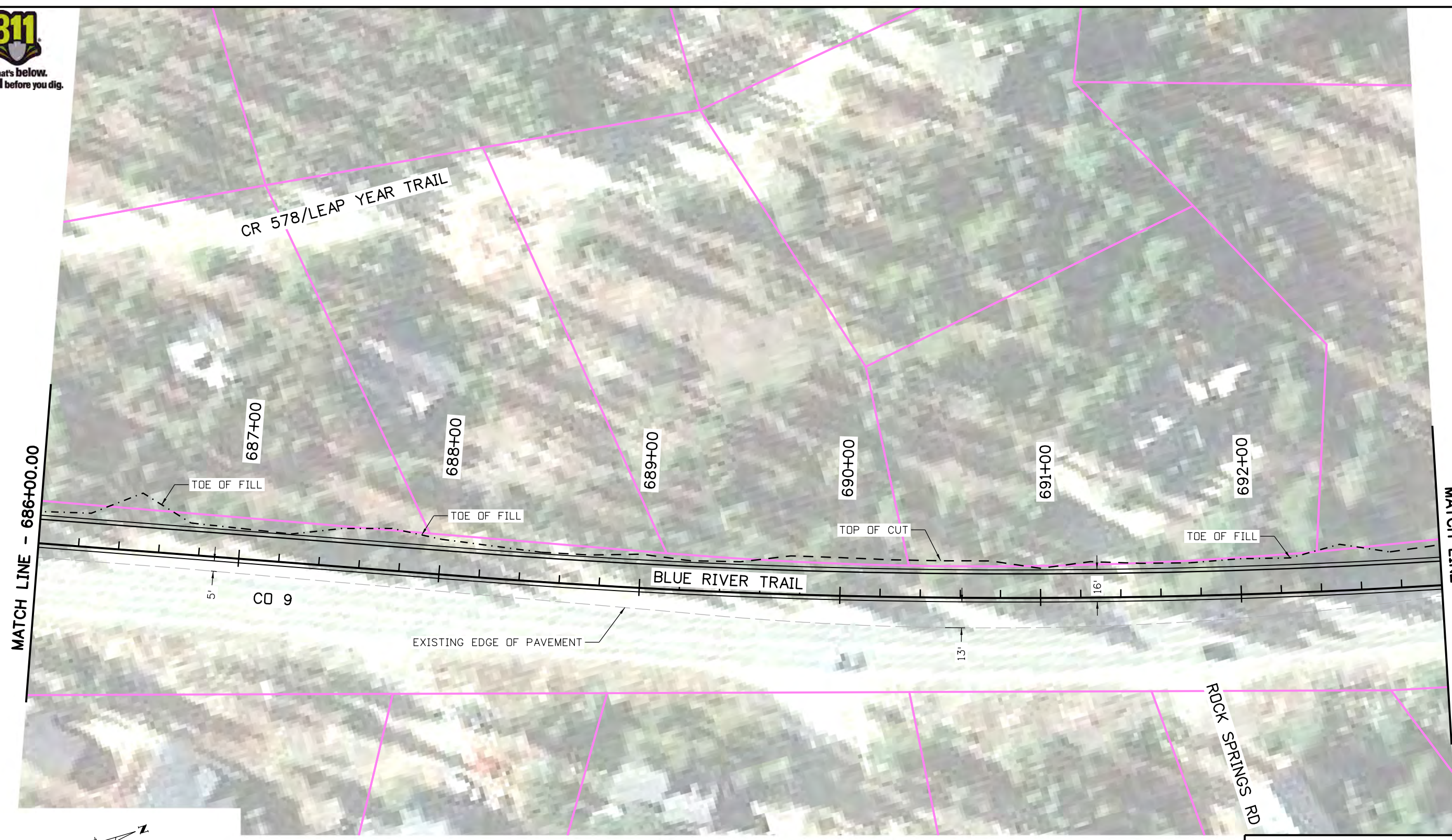
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Sheet Number 54

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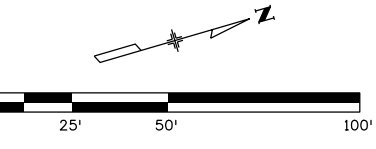
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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	52 of 73

Project No./Code	XXXXXXXXXX
	23001
Sheet Number	55



CONCEPTUAL PLANS

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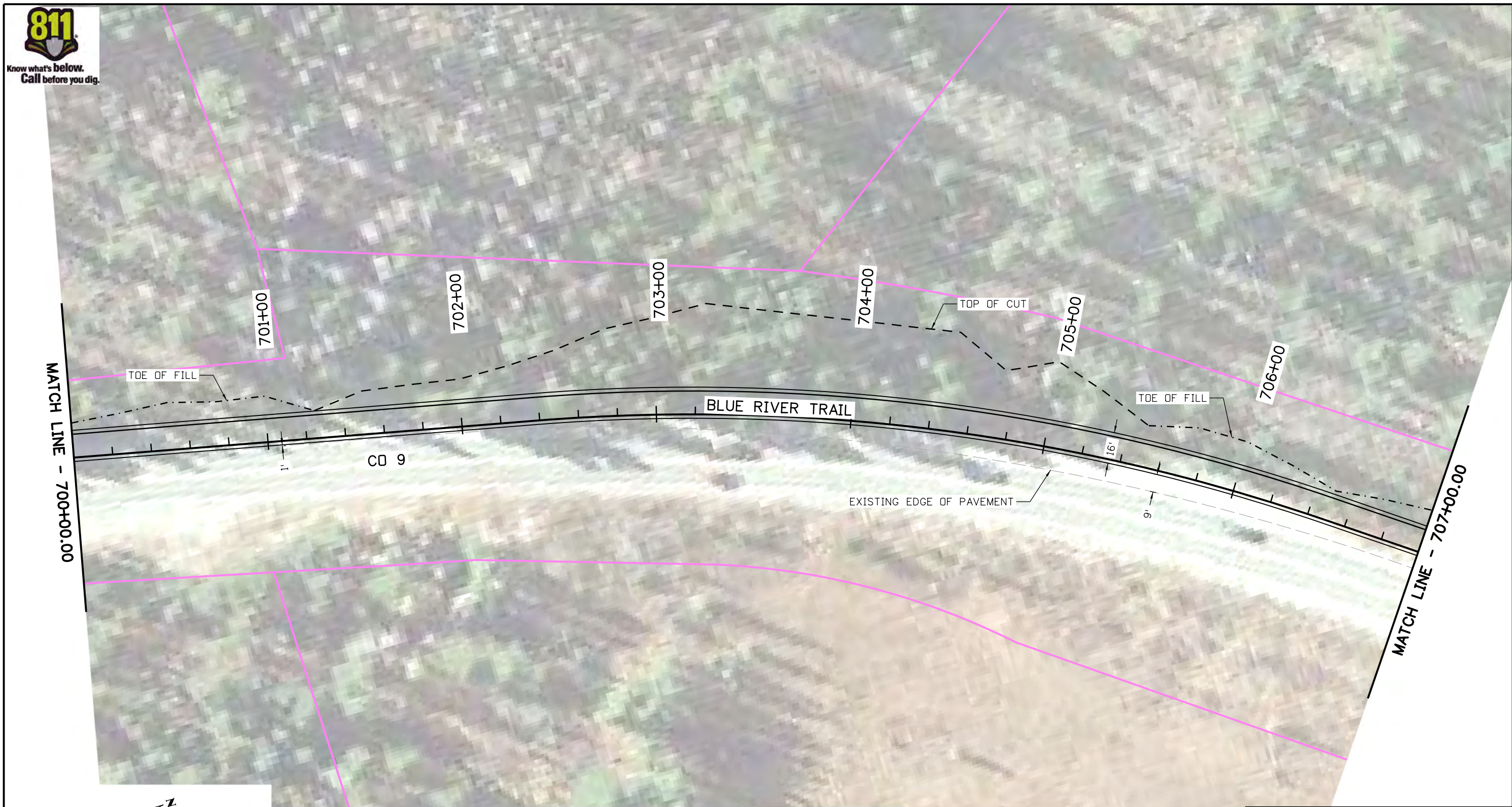
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Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	53 of 73

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 File Name: 22768DES_RdwyPlan54.dgn
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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	54 of 73
Sheet Subset:	TRAIL		

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Print Date: 3/12/2020
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No Revisions:
Revised:
Void:

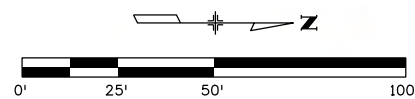
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Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 55 of 73

Project No./Code
XXXXXXXXXX
23001
Sheet Number 58

edevostidd 13:15:41 P:1000.005.10 R3 CD9 Blue River ACP Trail Design Sheets\22768DES_RdwyPlan55.dgn



Know what's below.
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CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan56.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	56 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 59

edevstidd 13:15:50 P:\1000.005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_RdwyPlan56.dgn



Know what's below.
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CONCEPTUAL PLANS

Print Date: 3/12/2020
File Name: 22768DES_RdwyPlan57.dgn
Horiz. Scale: 1:50 Vert. Scale: As Noted
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Date:	Comments	Init.

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	57 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXX
23001
Sheet Number 60

edeostidd 13115:58 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan57.dgn



Know what's below.
Call before you dig.

GOLD NUGGET DR

728+00 729+00 730+00 731+00 732+00 733+00 734+00

TOP OF CUT

TOE OF FILL

STA 728+98.70, 19.00' LT
BEGIN WALL

TOP OF CUT

MATCH LINE - 727+50.00

MATCH LINE - 734+50.00

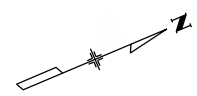
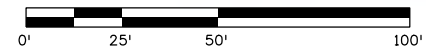
BLUE RIVER TRAIL

CD 9

EXISTING EDGE OF PAVEMENT

16'

13'



CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan58.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS		
Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 58 of 73

Project No./Code
XXXXXXXXX
23001
Sheet Number 61

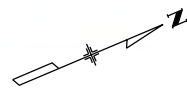
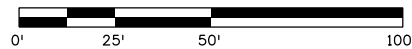
edeostidd 13:16:07 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan58.dgn



Know what's below.
Call before you dig.

MATCH LINE - 734+50.00

MATCH LINE - 741+00.00



CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan59.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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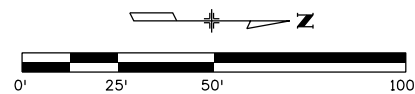
CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	59 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 62

e:\devstid\131614 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan59.dgn



Know what's below.
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CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan60.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	60 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXX
23001
Sheet Number 63

edevostidd 13:16:21 P:\1000.005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_RdwyPlan60.dgn



Know what's below.
Call before you dig.



MATCH LINE - 748+00.00

MATCH LINE - 754+50.00

CD 9

BLUE RIVER TRAIL

CR 574/CRDWN DR

CR 571/LAKESHORE LDDP

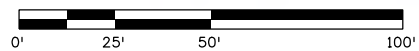
TOP OF CUT

TOE OF FILL

ROAD ACCESS SHALL BE MAINTAINED

TOE OF FILL

EXISTING EDGE OF PAVEMENT



CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan61.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Revised:
Void:

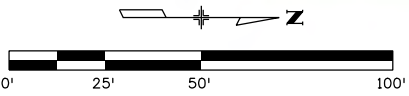
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Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	61 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXX
23001
Sheet Number 64

e:\dev\stid\13146:30 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan61.dgn



Know what's below.
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CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan62.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	62 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXX
23001
Sheet Number 65

edevostidd 13:16:37 P:\1000.005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_RdwyPlan62.dgn



Know what's below.
Call before you dig.

MATCH LINE - 761+50.00



MATCH LINE - 768+50.00

CONCEPTUAL PLANS


edeostidd 13:16:47 P:\1000.005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_RdwyPlan63.dgn

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan63.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	63 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 66

811

Know what's below.
Call before you dig.

edevostidd 13:16:54 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan64.dgn

MATCH LINE - 768+50.00

MATCH LINE - 775+50.00

CR 790/GREEN MOUNTAIN DR

CR 550/HORIZON LN

769+00

770+00

771+00

772+00

773+00

774+00

775+00

TOP OF CUT

TOE OF FILL

EXTEND CULVERT

BLUE RIVER TRAIL

EXISTING EDGE OF PAVEMENT

CD 9

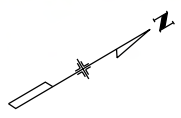
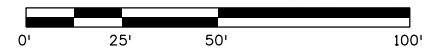
DRIVEWAY ACCESS SHALL BE MAINTAINED

14'

16'


3'

12'



CONCEPTUAL PLANS


Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan64.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted



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Date:	Comments	Init.

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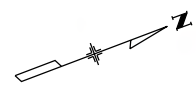
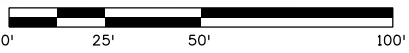
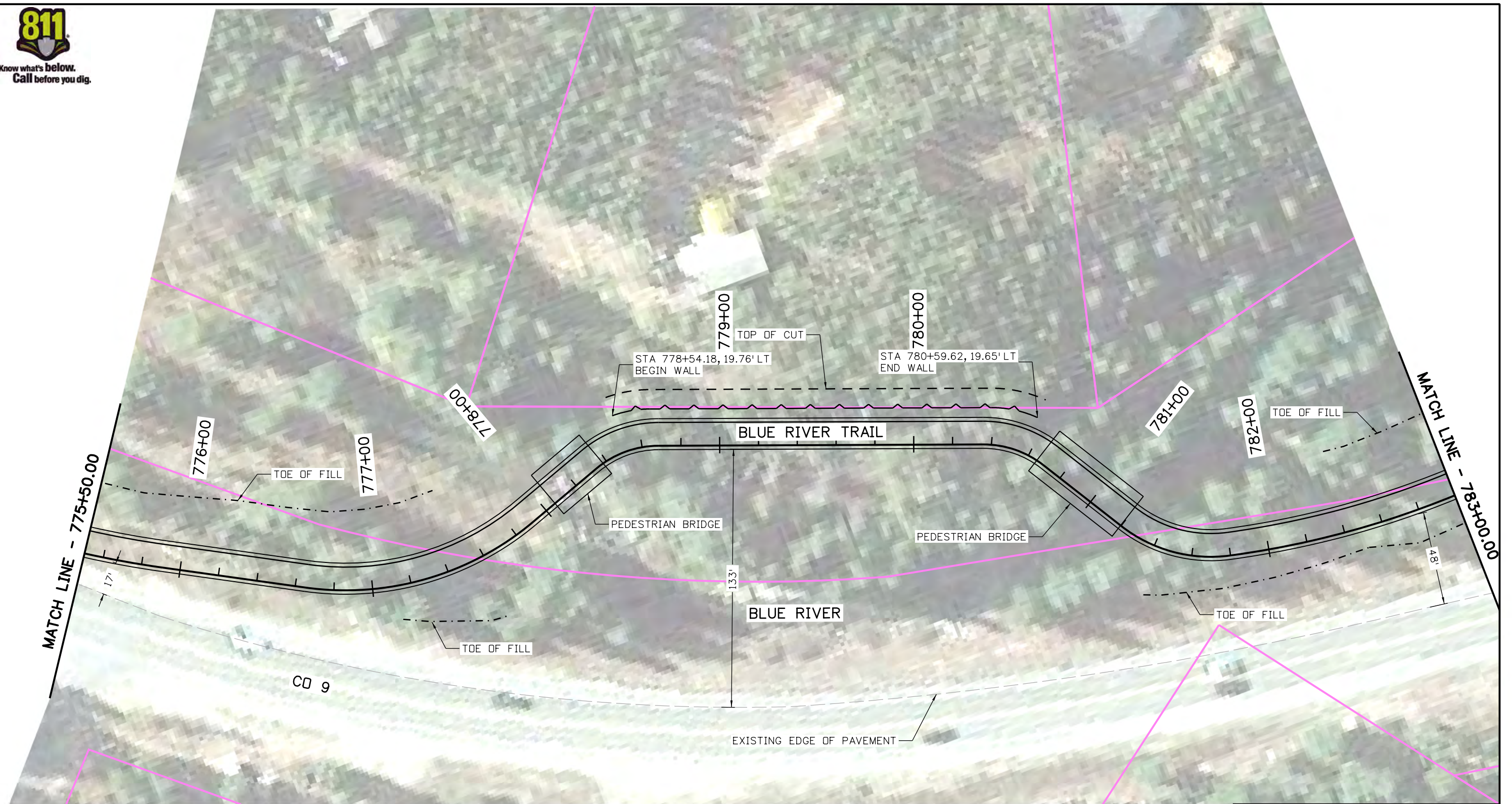
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No Revisions:
Revised:
Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	64 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 67



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CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan65.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Date:	Comments	Init.

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Revised:
Void:

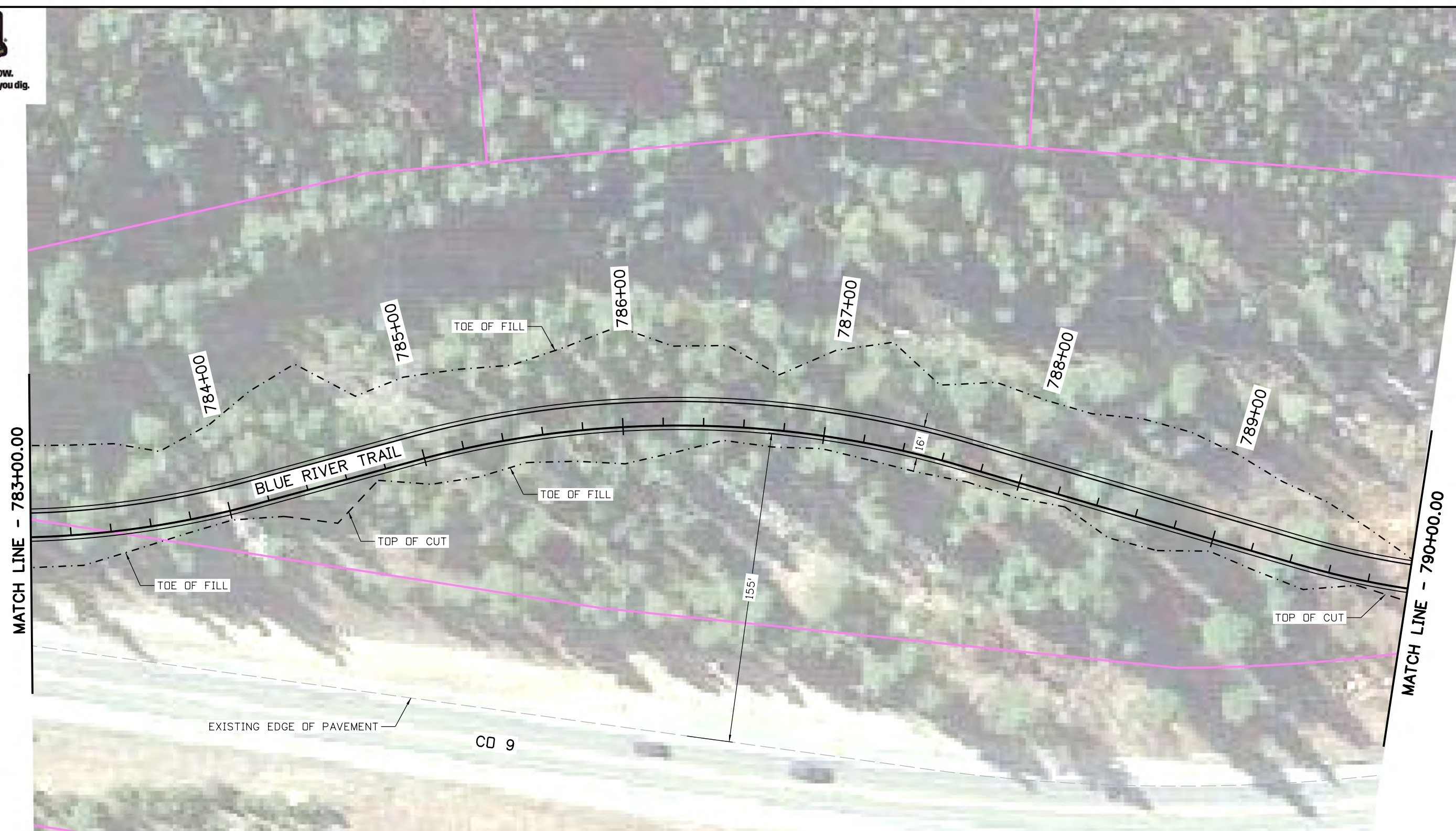
CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	65 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 68

edevostidd 13:17:00 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan65.dgn



Know what's below.
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edevostidd 13:17:09 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan66.dgn


CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan66.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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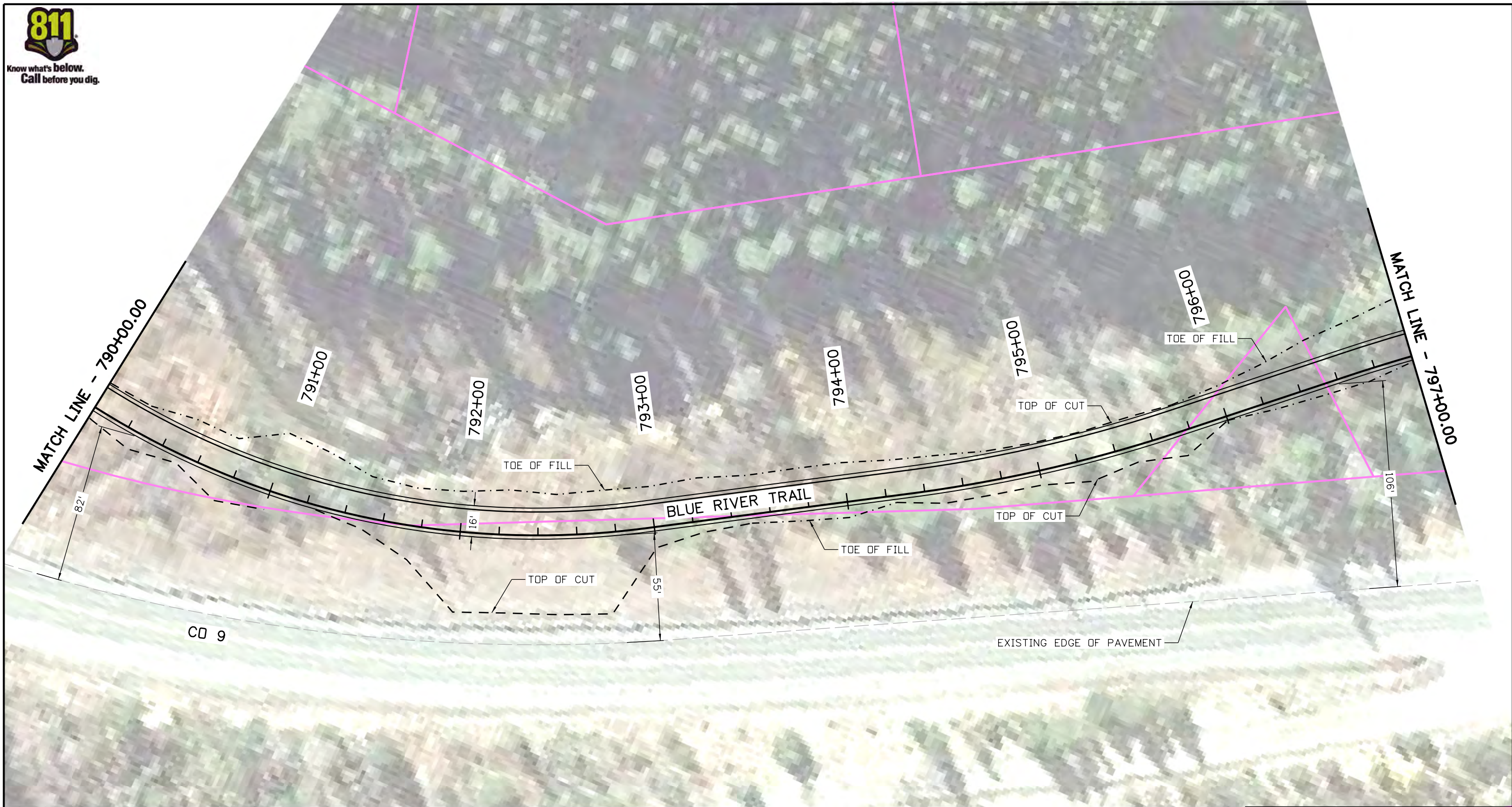
As Constructed
No Revisions:
Revised:
Void:

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	66 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 69



Know what's below.
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CONCEPTUAL PLANS

Print Date: 3/12/2020
File Name: 22768DES_RdwyPlan67.dgn
Horiz. Scale: 1:50 Vert. Scale: As Noted
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Void:

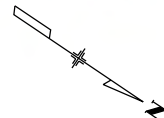
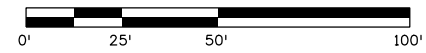
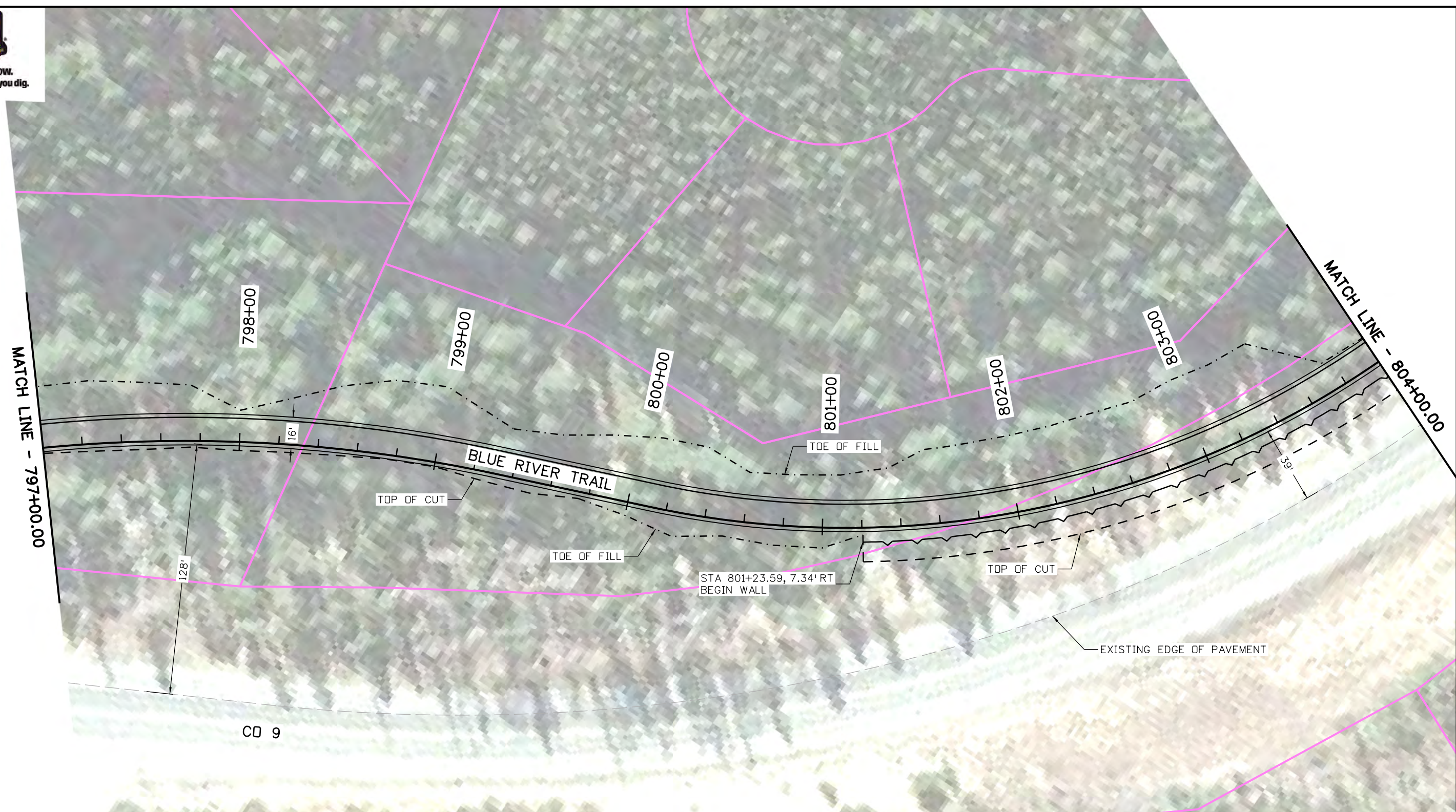
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Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	67 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXX
23001
Sheet Number 70

edevestidd 13:17:17 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan67.dgn



Know what's below.
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CONCEPTUAL PLANS

edevostidd 13:17:25 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan68.dgn

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan68.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Void:	

CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	68 of 73
Sheet Subset:	TRAIL		

Project No./Code	XXXXXXXXX
	23001
Sheet Number	71



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edevostidd 13:17:33 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan69.dgn

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan69.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS		
Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 69 of 73

Project No./Code
XXXXXXXXXX
23001
Sheet Number 72



Know what's below.
Call before you dig.

RIVERPARK DR

BLUE RIVER TRAIL

MATCH LINE - 811+00.00

MATCH LINE - 817+50.00

TOE OF FILL

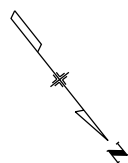
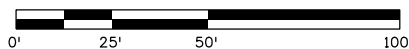
TOE OF FILL

STA 812+37.45, 5.00' RT
BEGIN WALL

EXISTING EDGE OF PAVEMENT

TOP OF CUT

CD 9



CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan70.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Void:

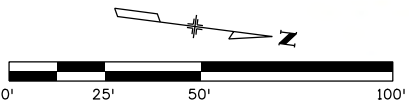
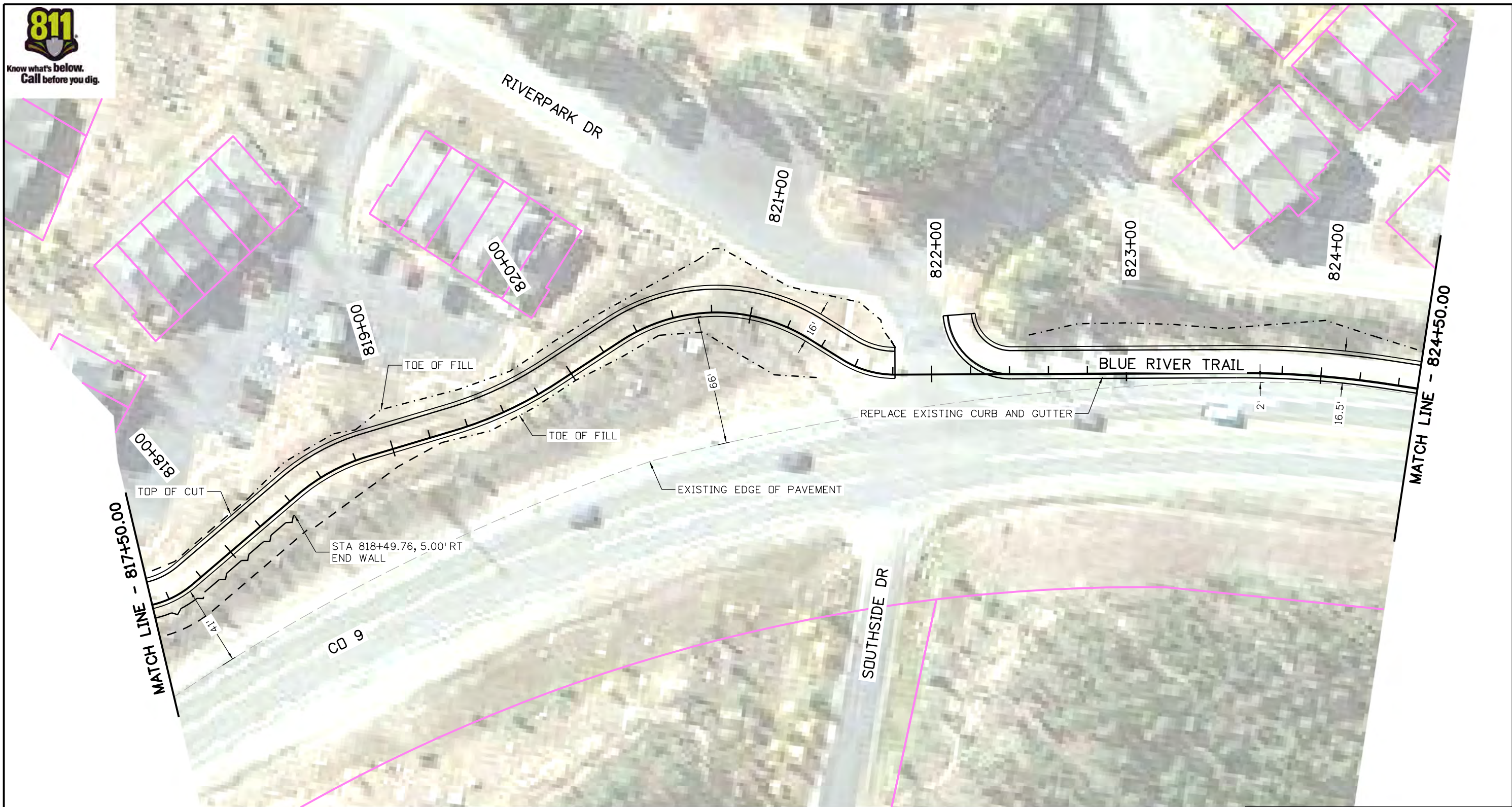
CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	70 of 73
Sheet Subset:	TRAIL		

Project No./Code
XXXXXXXX
23001
Sheet Number 73

edevostidd 13:17:41 P:\1000.005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_RdwyPlan70.dgn



Know what's below.
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CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan71.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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Void:

CD 9 BLUE RIVER TRAIL PLANS		
Designer:	CSA	Structure Numbers
Detailer:	NG	
Sheet Subset:	TRAIL	Subset Sheets: 71 of 73

Project No./Code
XXXXXXXX
23001
Sheet Number 74

edevstidd 13:17:48 P:\1000.005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_RdwyPlan71.dgn



Know what's below.
Call before you dig.



MATCH LINE - 824+50.00

MATCH LINE - 831+00.00

BLUE RIVER TRAIL

CD 9

STA 826+89.56, 15.00' LT
BEGIN WALL

TOE OF FILL

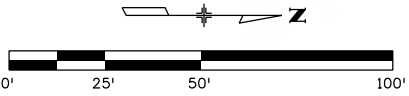
TOP OF CUT

REPLACE EXISTING CURB AND GUTTER

EXISTING EDGE OF PAVEMENT

16.5'

1'



CONCEPTUAL PLANS

Print Date: 3/12/2020
 File Name: 22768DES_RdwyPlan72.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	72 of 73

Project No./Code
XXXXXXXX
23001
Sheet Number 75

edeostidd 13:17:56 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan72.dgn



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CONCEPTUAL PLANS

edeostidd 13:18:03 P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_RdwyPlan73.dgn

Print Date: 3/12/2020
File Name: 22768DES_RdwyPlan73.dgn
Horiz. Scale: 1:50 Vert. Scale: As Noted
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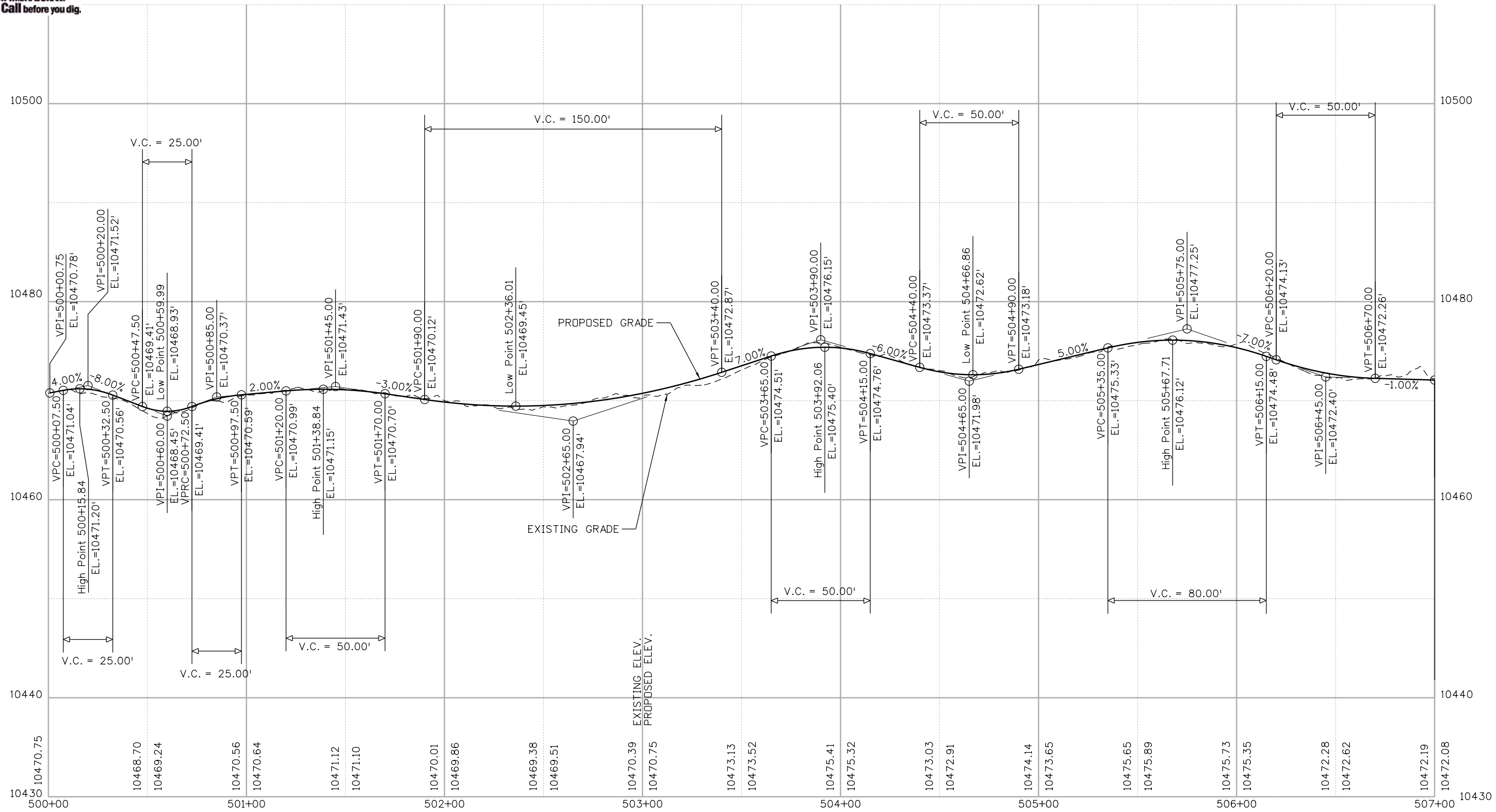
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CD 9 BLUE RIVER TRAIL PLANS			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	TRAIL	Subset Sheets:	73 of 73

Project No./Code
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23001
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N:\0000\005.10 R3 CD9 Blue River ACP\Trail Design\Sheets\22768DES_Profile01.dgn

Print Date: 1/10/2020

File Name: 22768DES_Profile01.dgn

Horiz. Scale: 1:50

Vert. Scale: As Noted

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BLUE RIVER TRAIL PROFILE

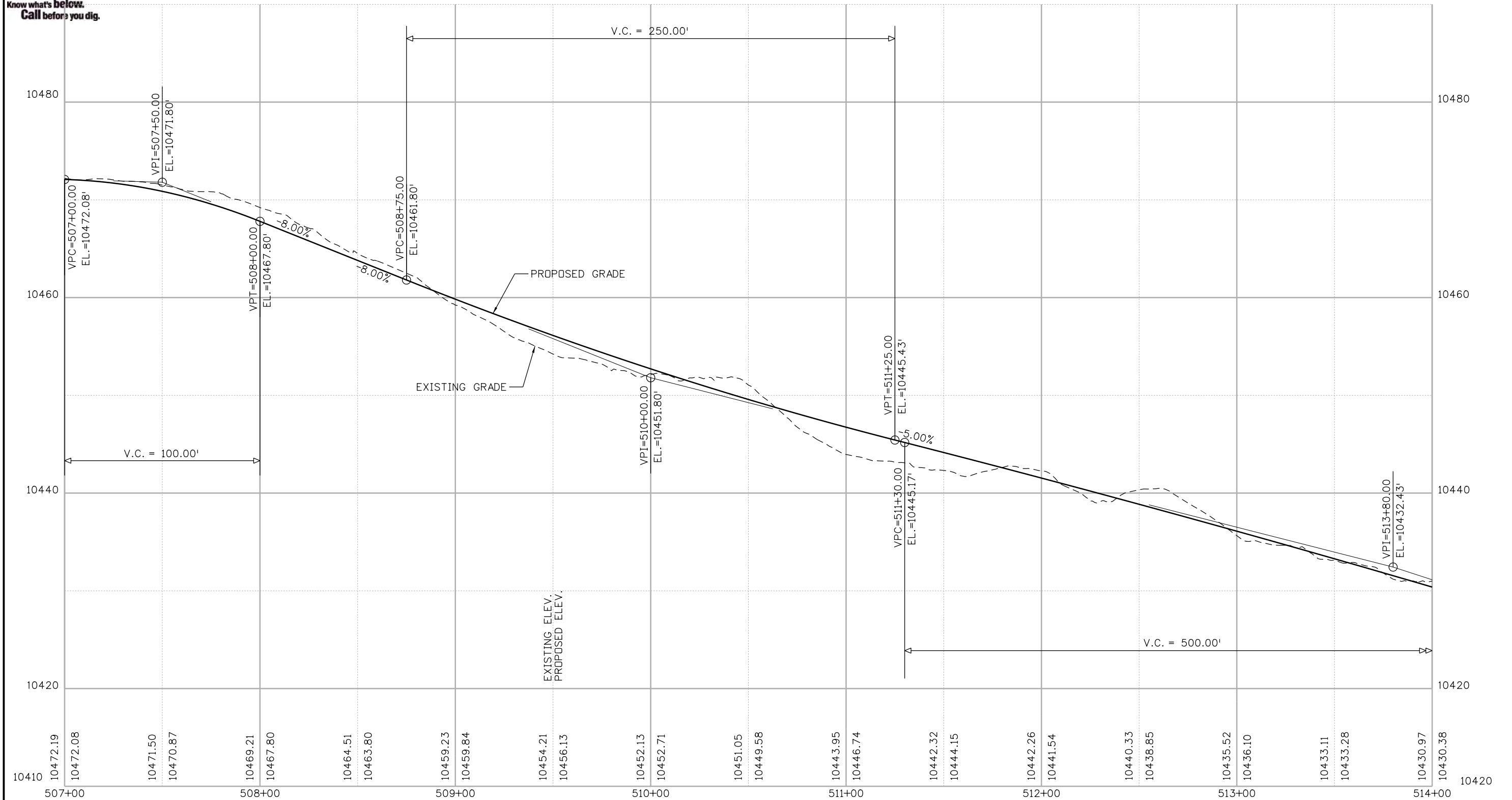
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Detailer:	NG	Subset Sheets:	01 of 48
Sheet Subset:	PROFILE		

Project No./Code

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23001
Sheet Number 77



Know what's below.
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Print Date: 1/10/2020
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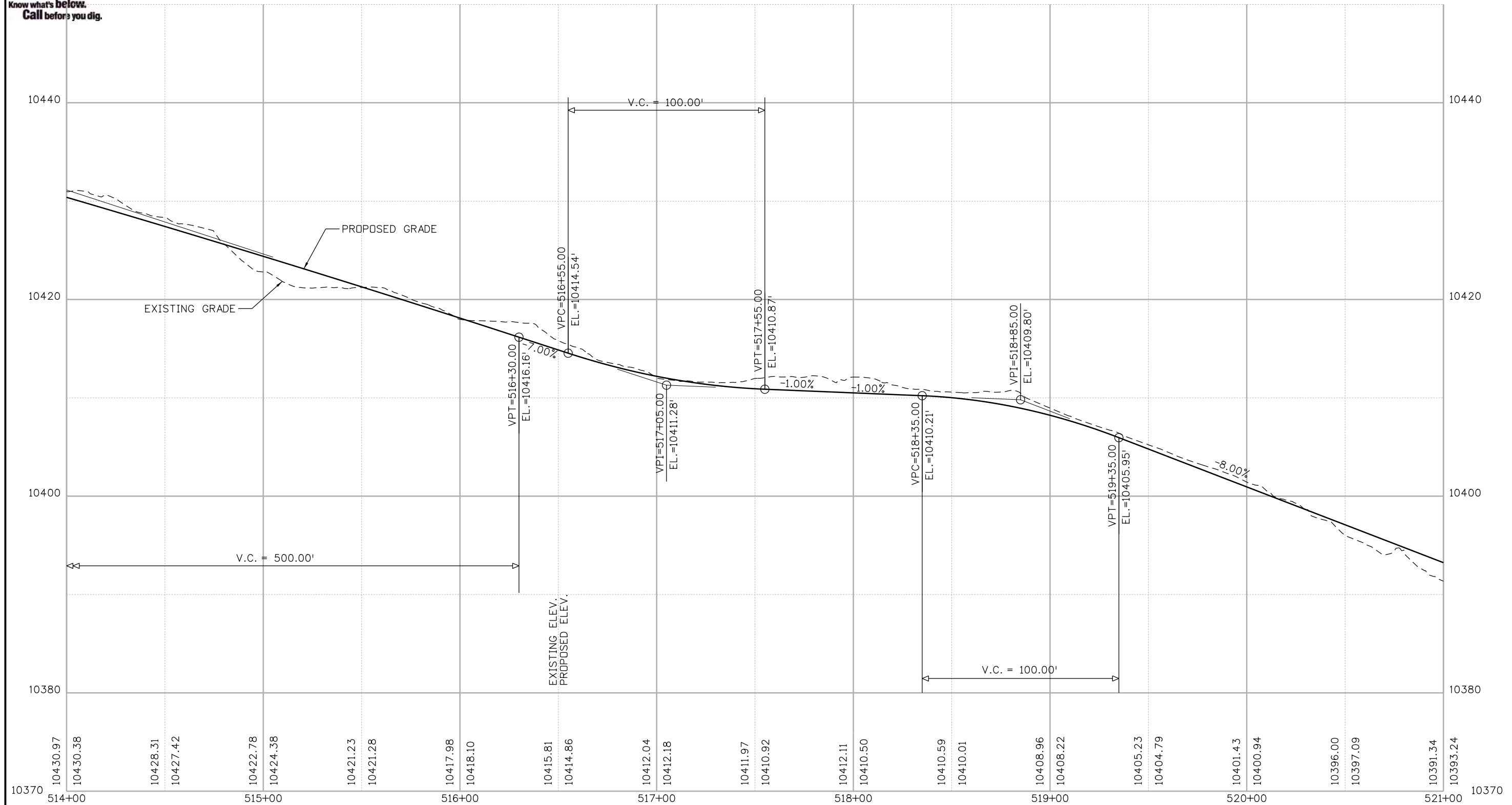
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BLUE RIVER TRAIL PROFILE			
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Detailer:	NG		
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Project No./Code
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Sheet Number 78




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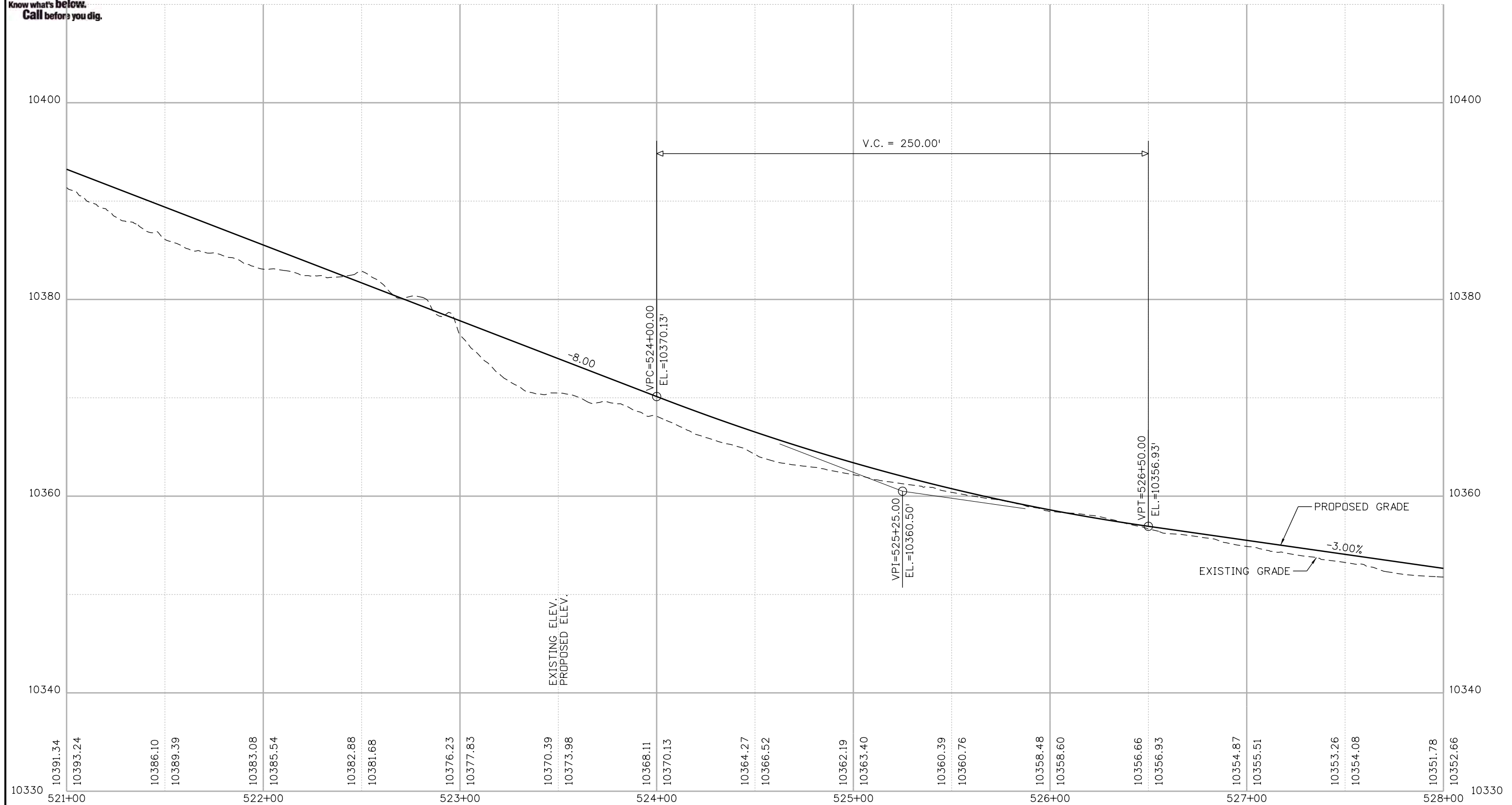
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Detailer:	NG		
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Print Date: 1/10/2020
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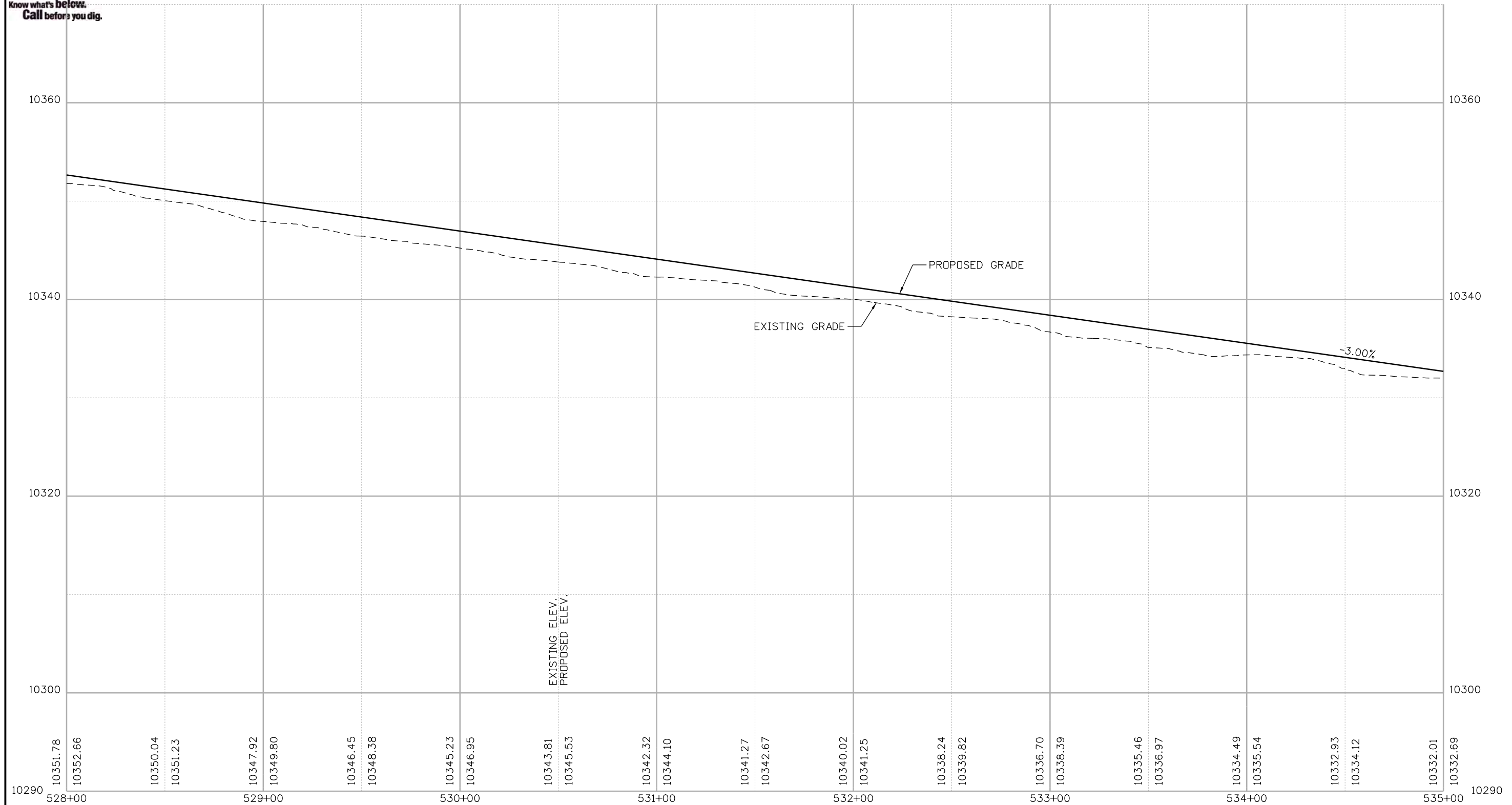
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 04 of 48

Project No./Code
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Print Date: 1/10/2020
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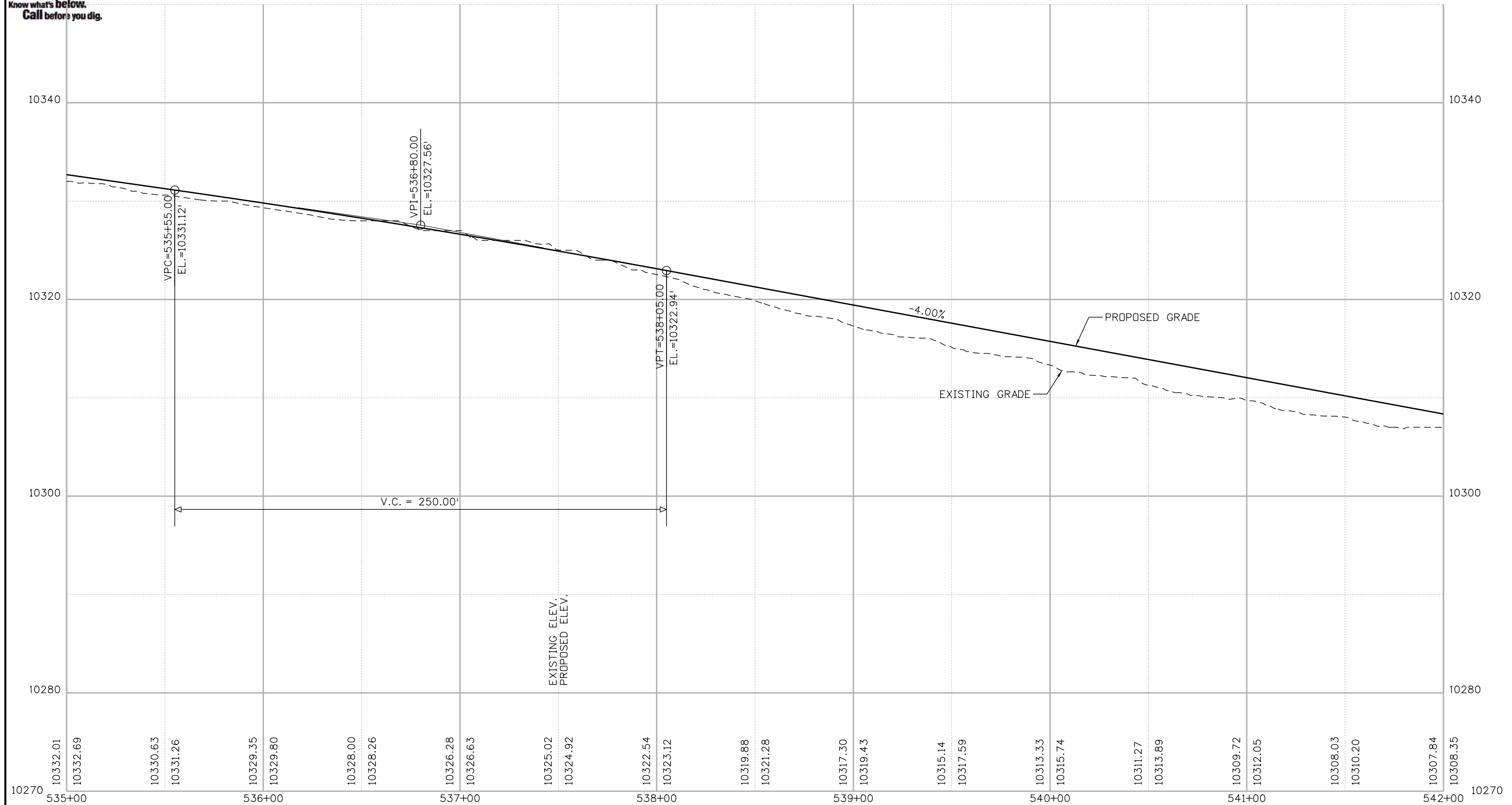
BLUE RIVER TRAIL
PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 05 of 48

Project No./Code	
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23001	
Sheet Number	81



Know what's below.
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N:\kolett 2:50:40 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile06.dgn

Print Date: 1/10/2020
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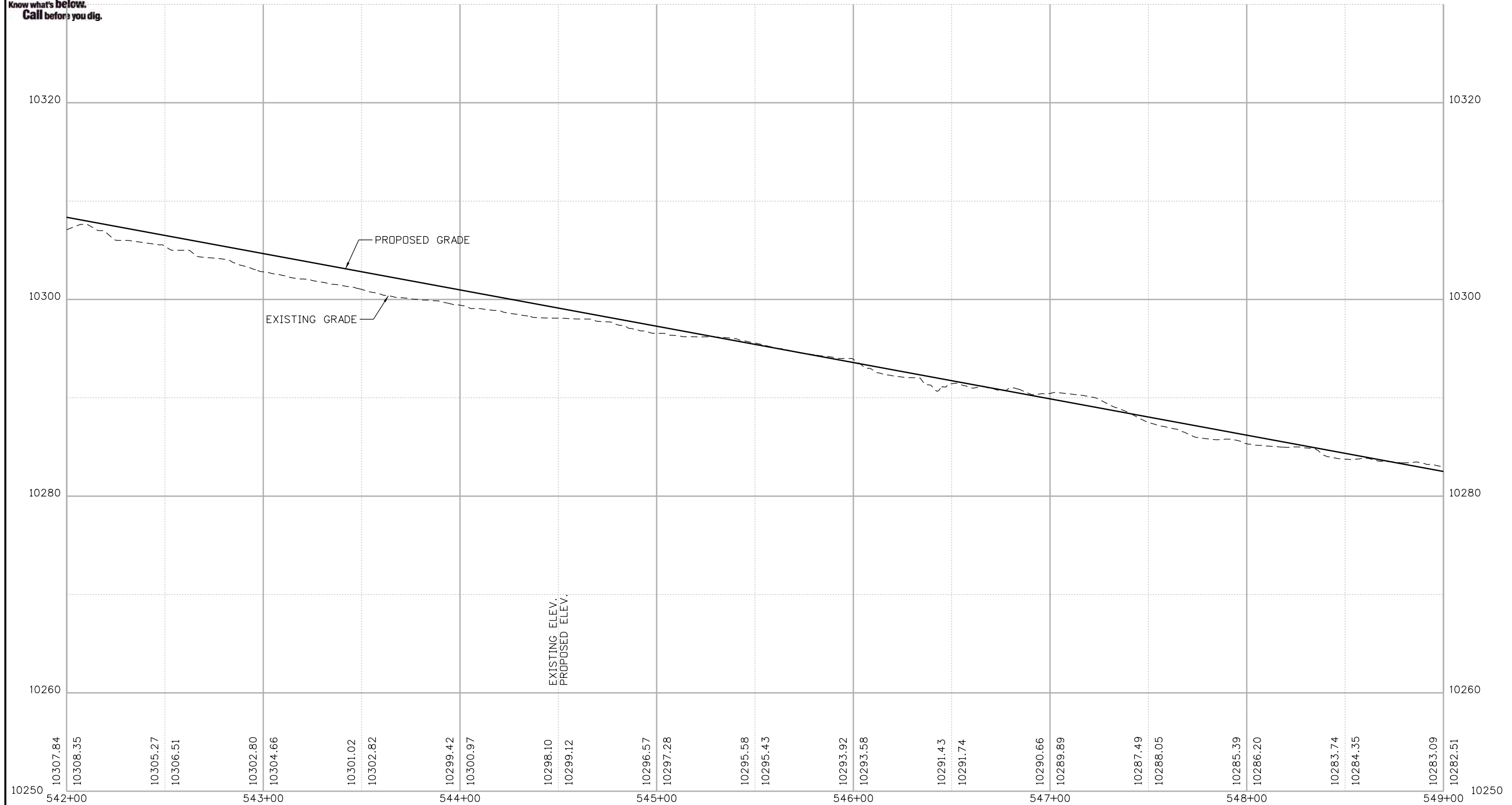
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Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 06 of 48

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23001
Sheet Number 82




Know what's below.
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Print Date: 1/10/2020
 File Name: 22768DES_Profile07.dgn
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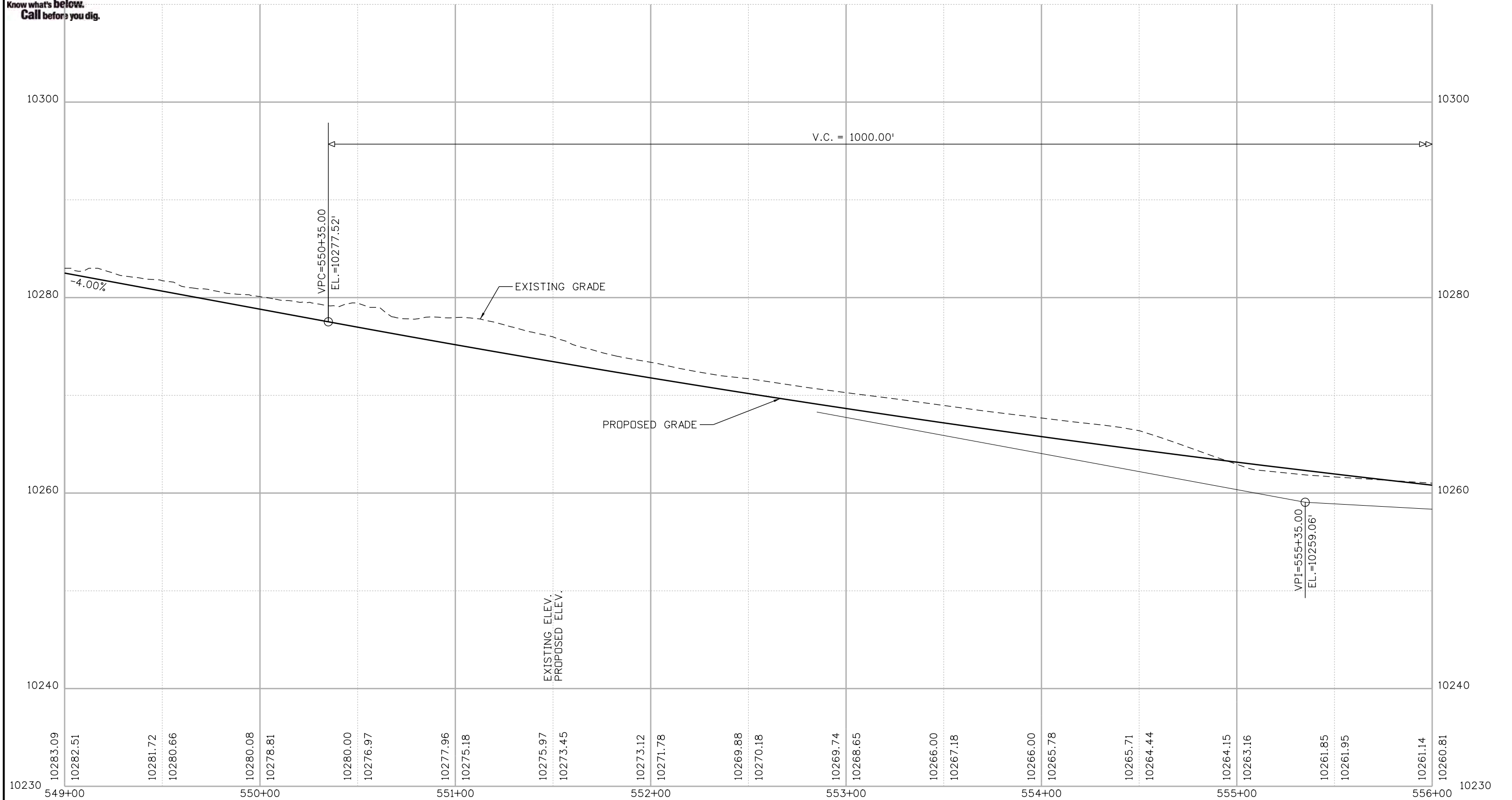
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 07 of 48

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Know what's below.
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Print Date: 1/10/2020
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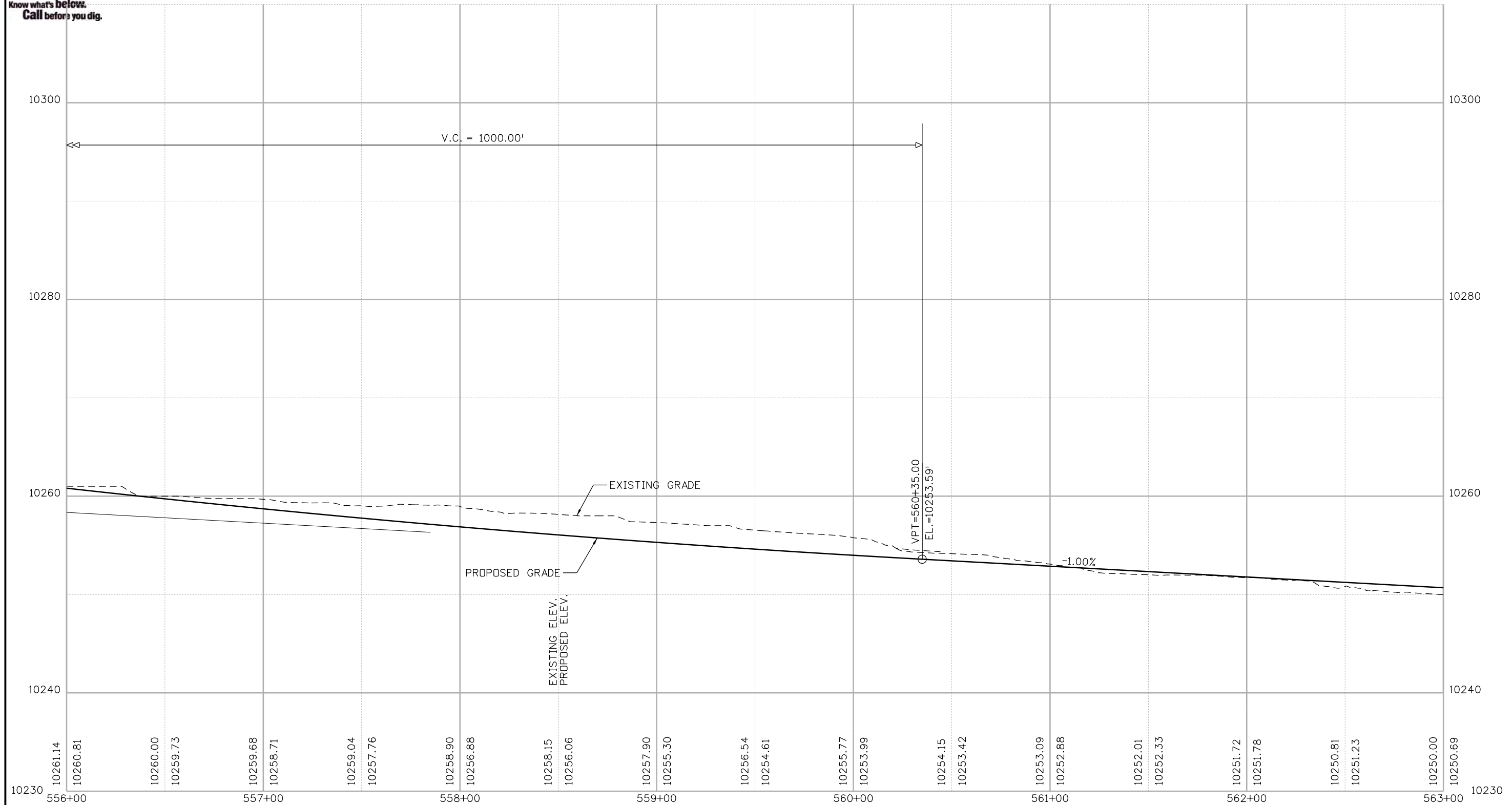
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 08 of 48

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Sheet Number 84



Know what's below.
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Nikolett 2:50:43 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile09.dgn

Print Date: 1/10/2020
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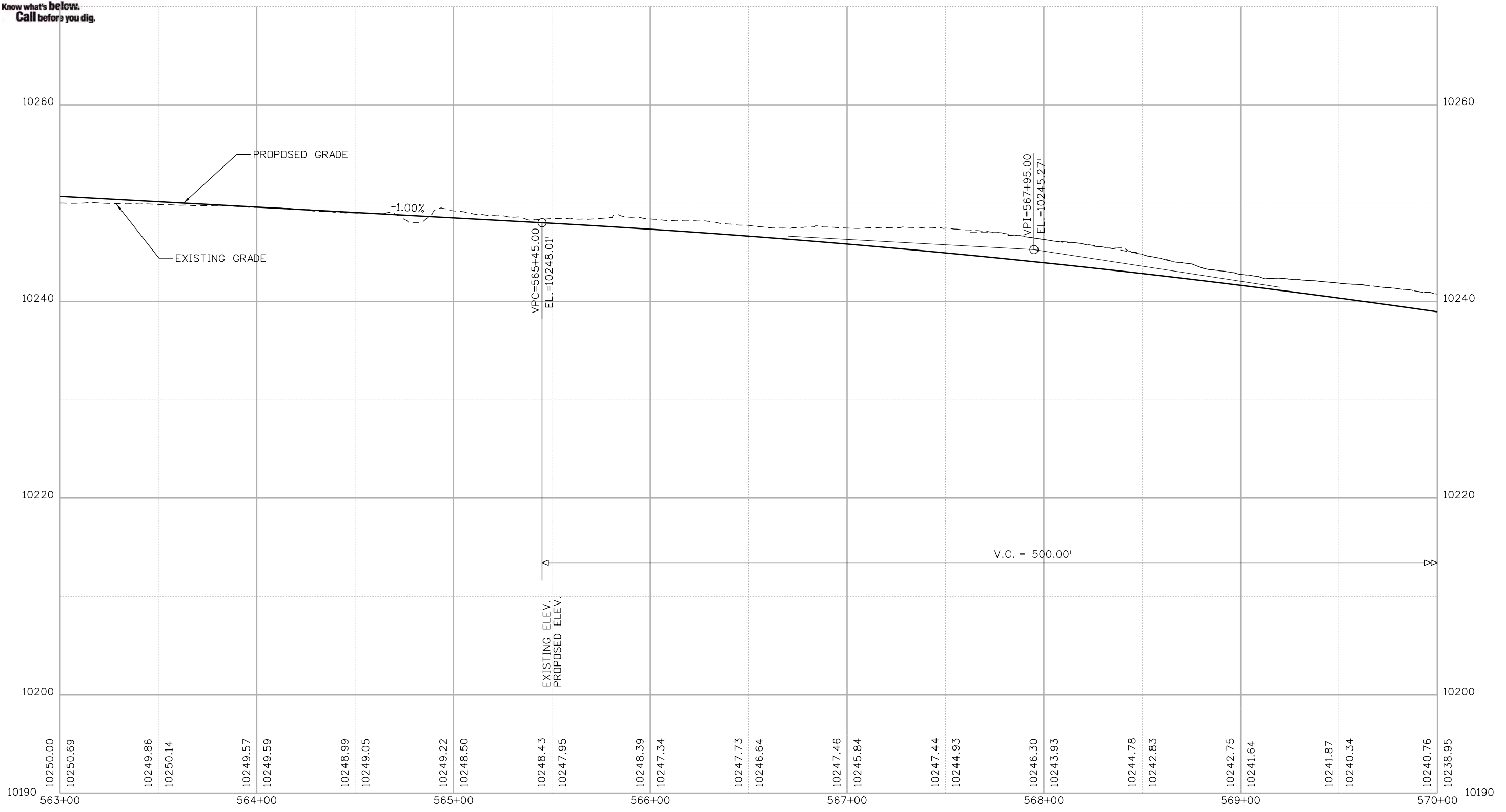
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 09 of 48

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Know what's below.
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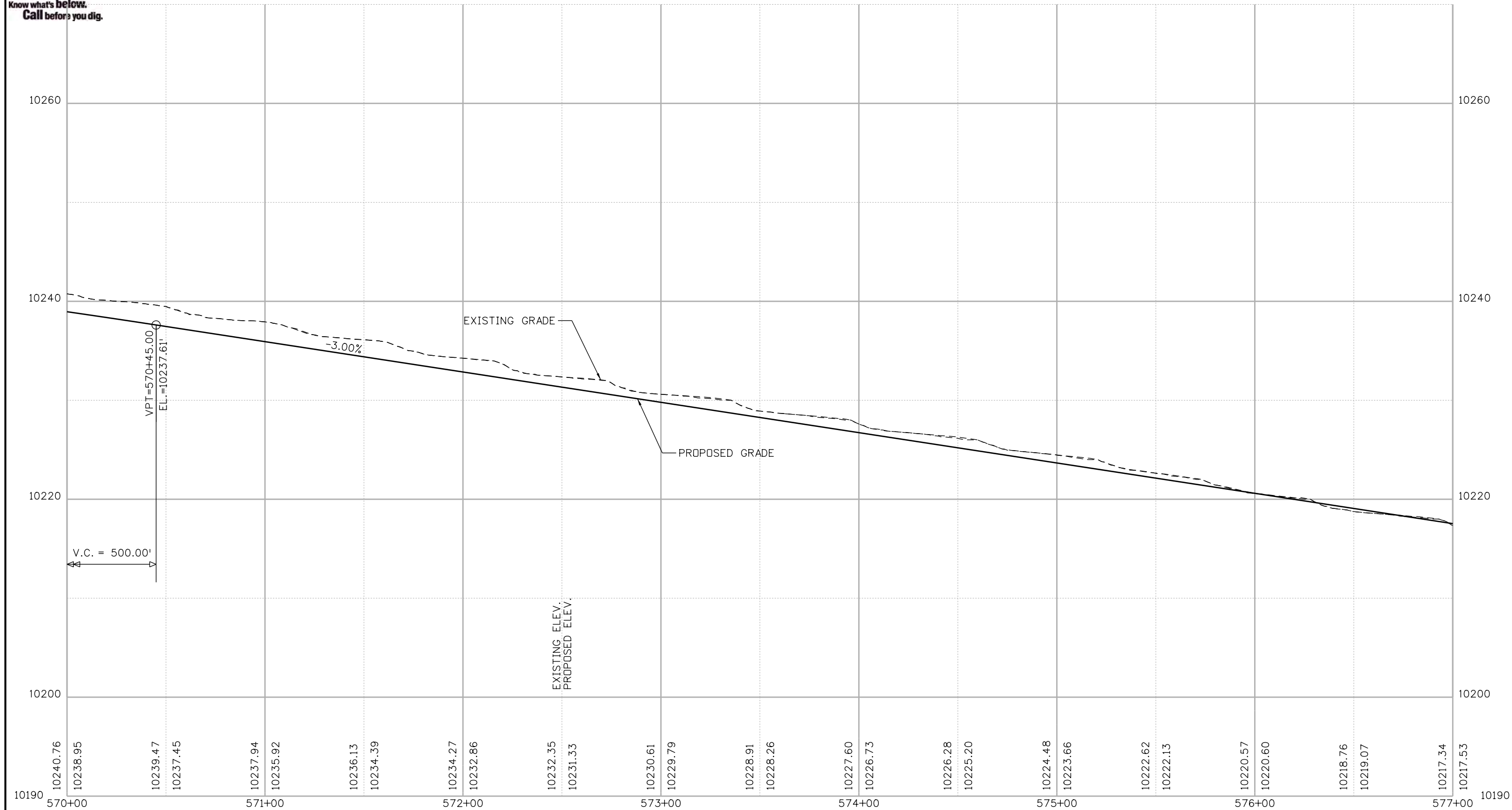
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 10 of 48

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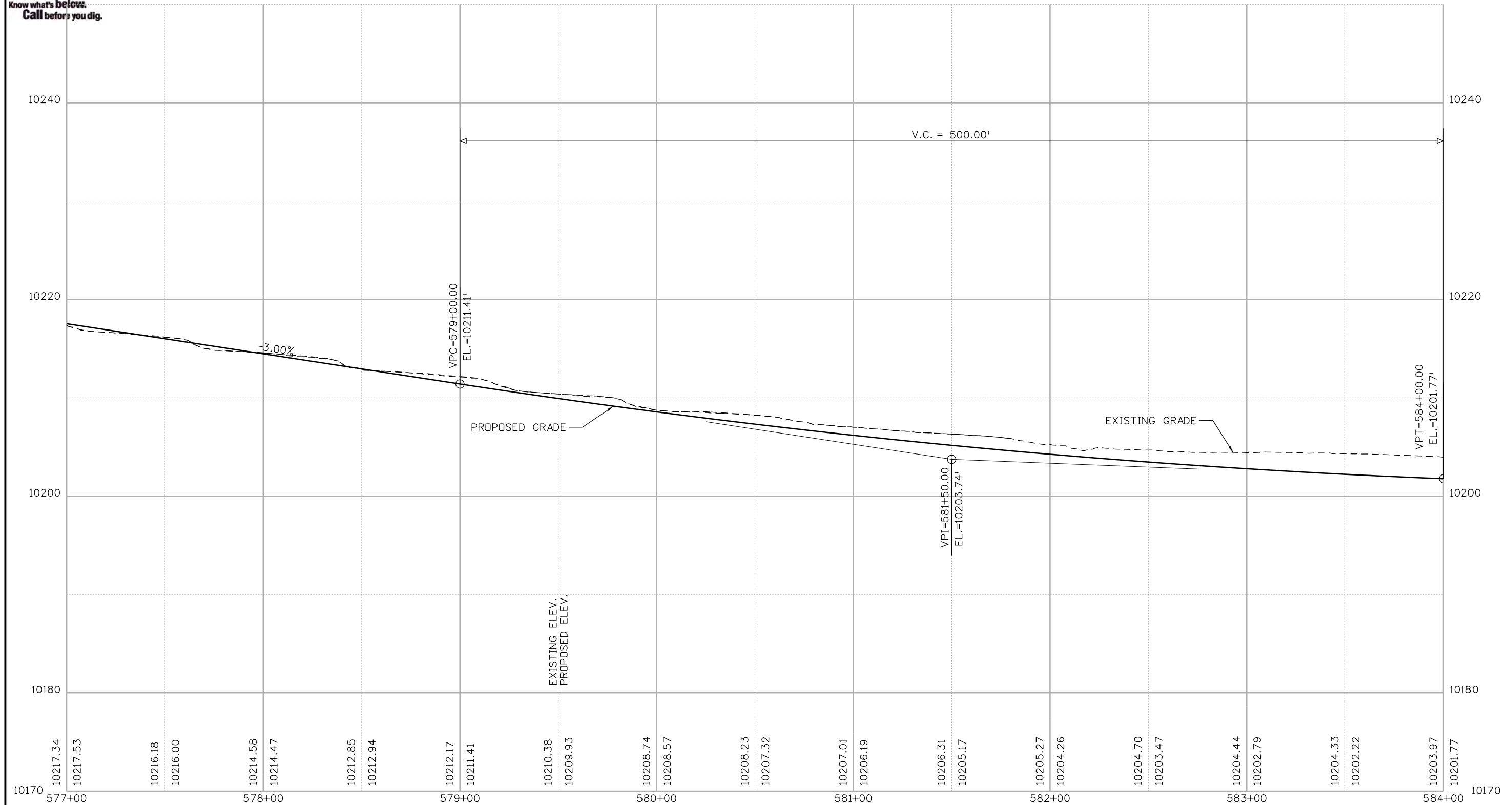
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 11 of 48

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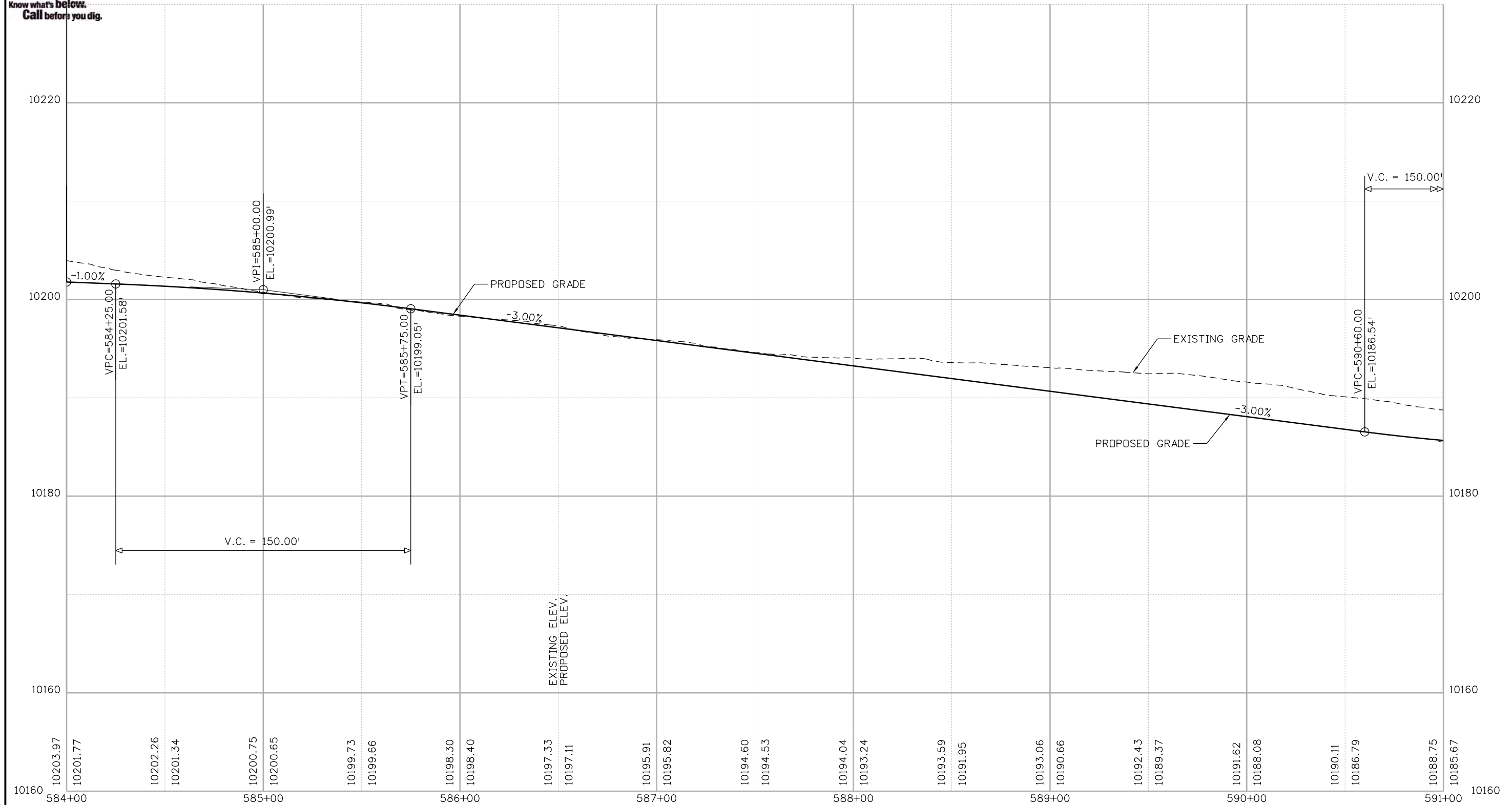
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 12 of 48

Project No./Code
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Print Date: 1/10/2020
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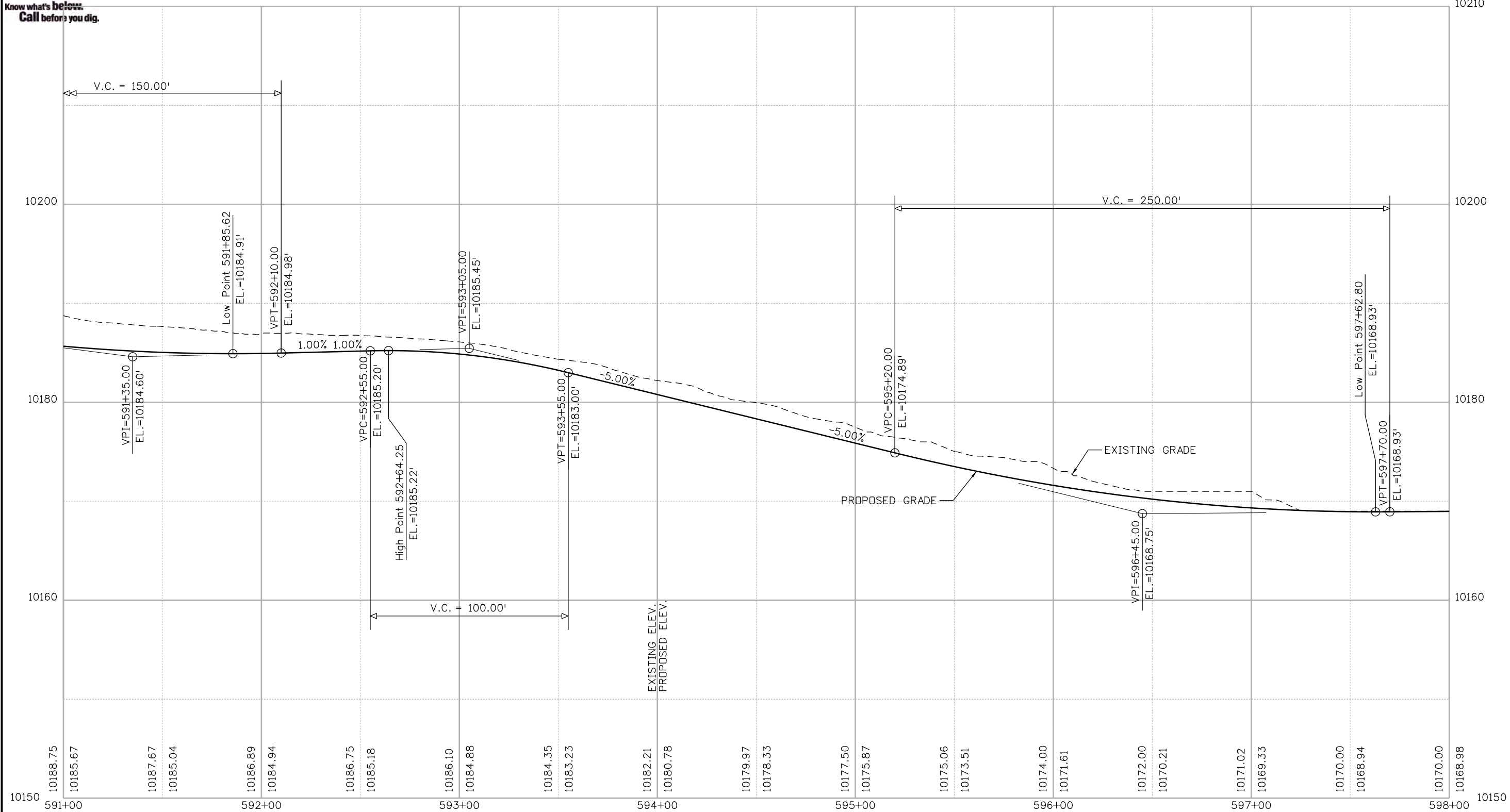
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 13 of 48

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N:\kott 2:50:49 PM P:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile14.dgn

Print Date: 1/10/2020
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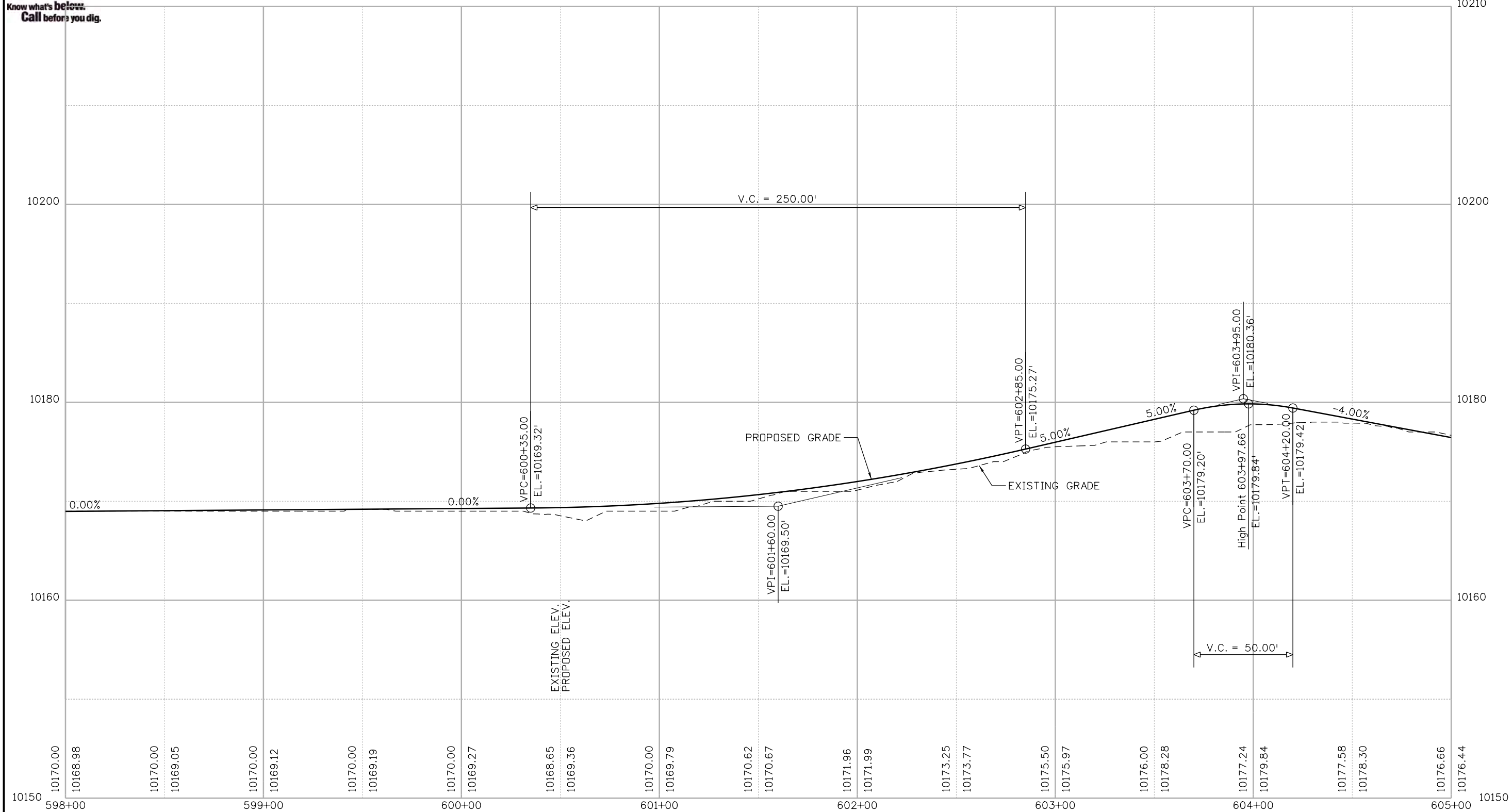
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BLUE RIVER TRAIL PROFILE			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	PROFILE	Subset Sheets:	14 of 48

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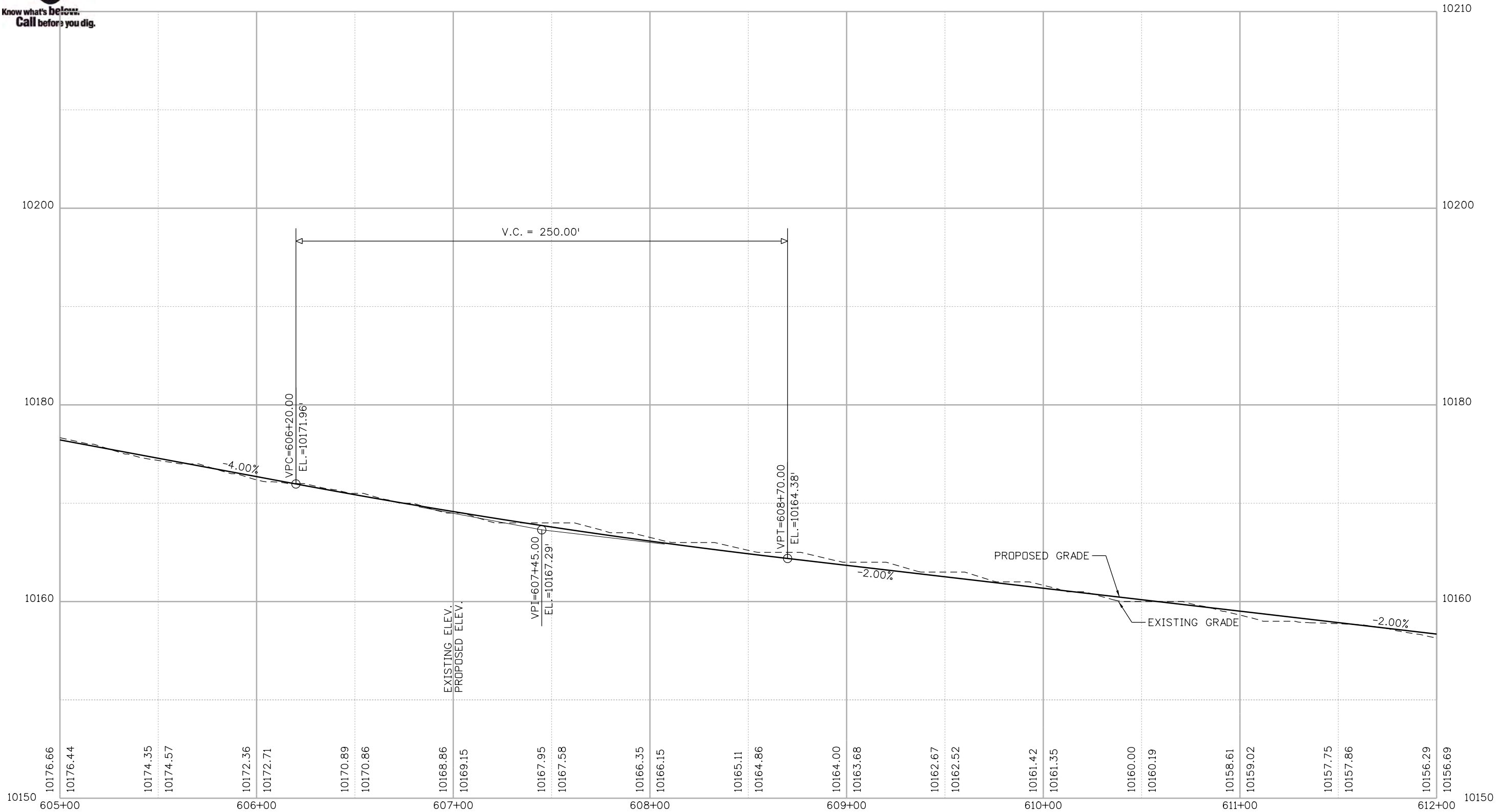
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Structure Numbers
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Sheet Number 91



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Print Date: 1/10/2020
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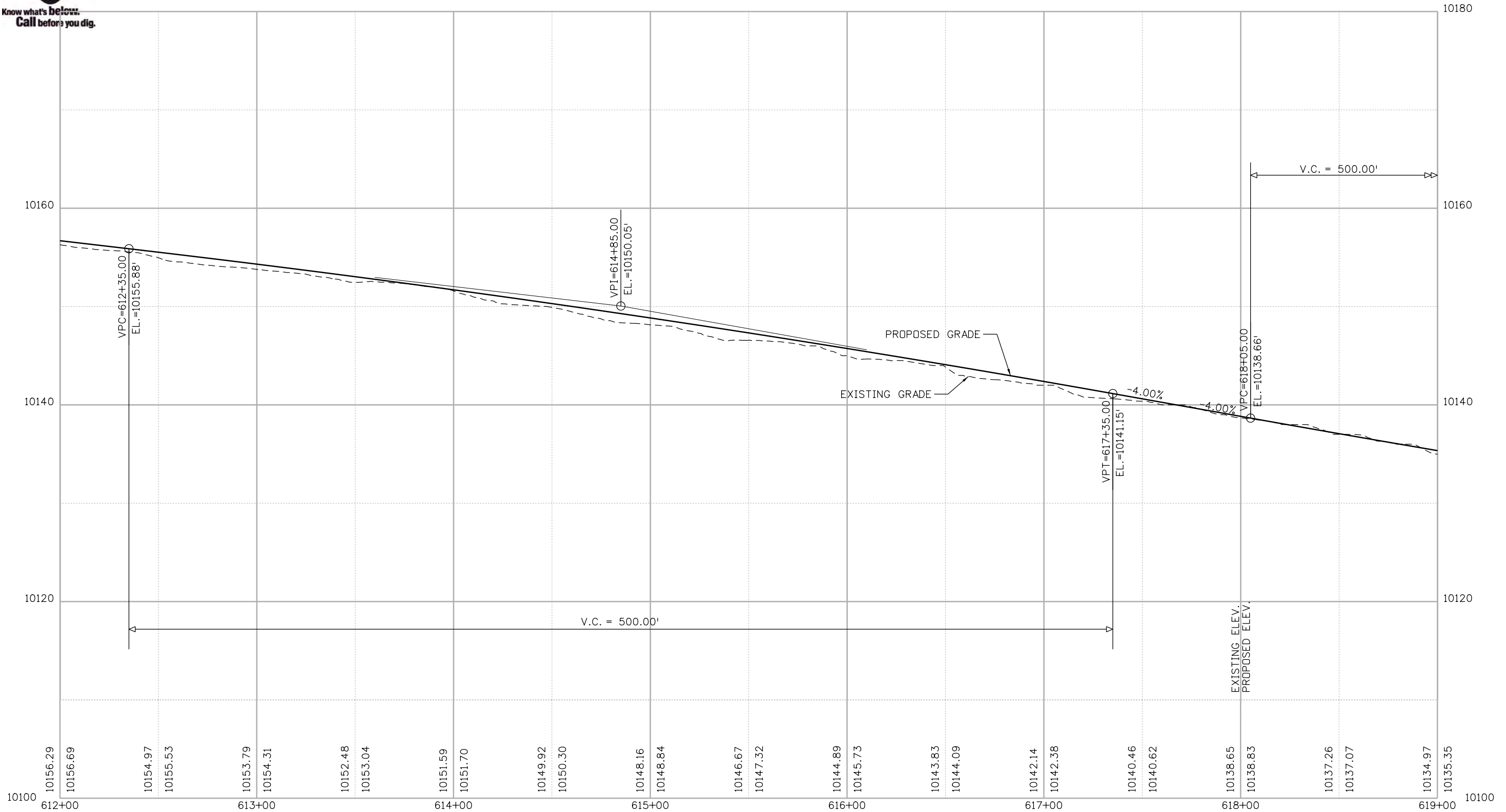
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 16 of 48

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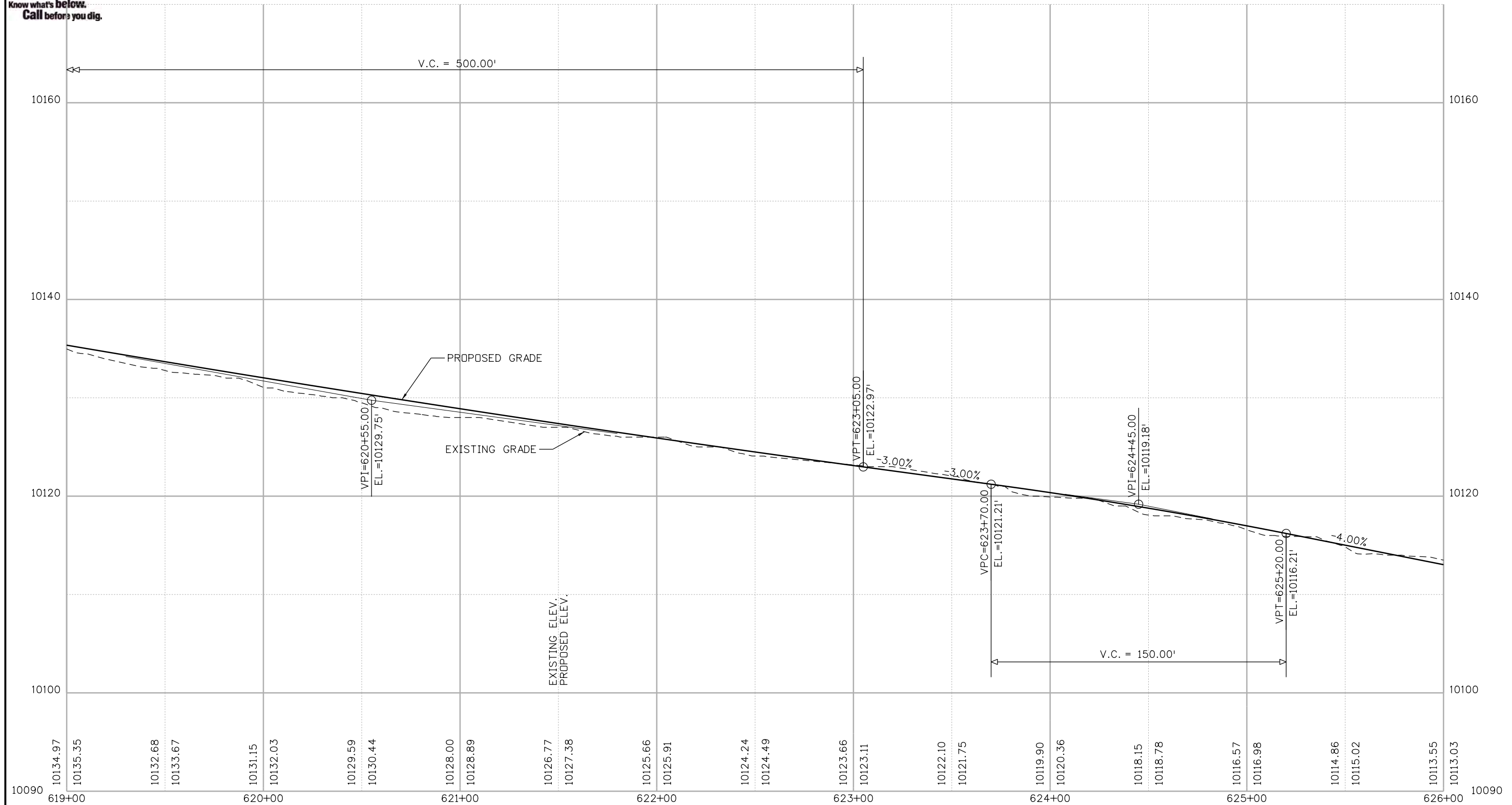
BLUE RIVER TRAIL PROFILE

Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	17 of 48
Sheet Subset:	PROFILE		

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BLUE RIVER TRAIL PROFILE

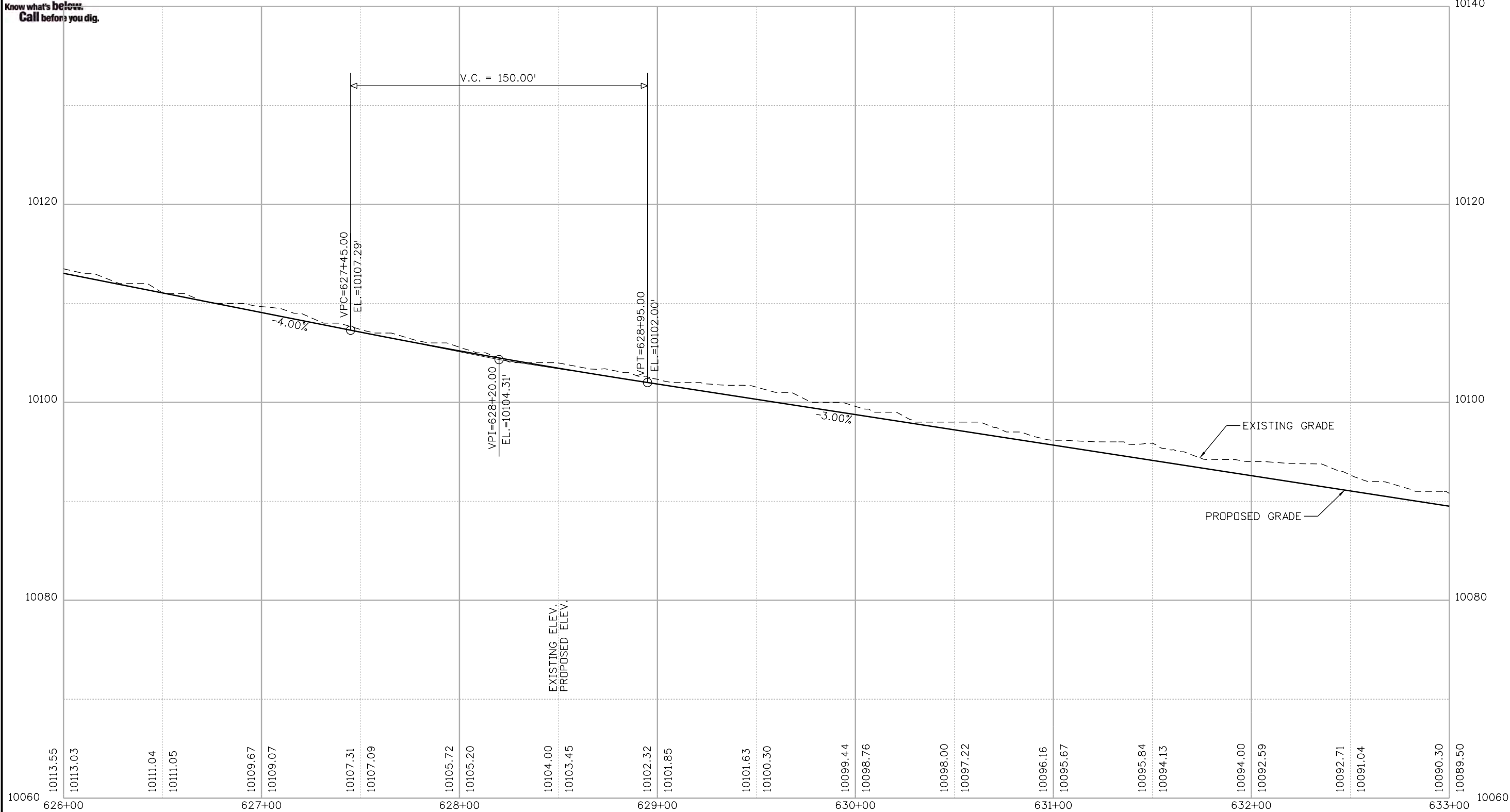
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Structure Numbers
 Subset Sheets: 18 of 48

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Sheet Number 94



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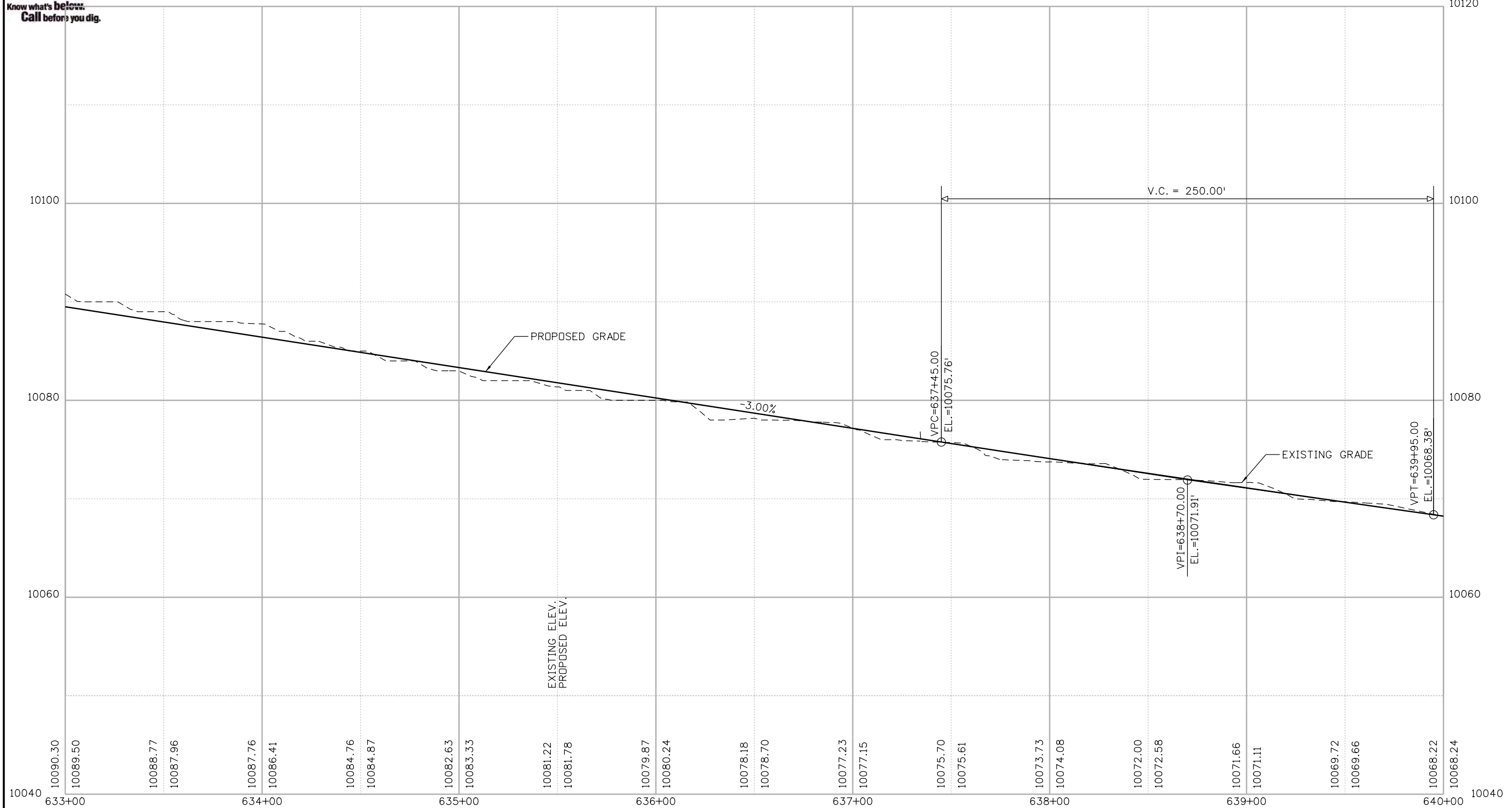
BLUE RIVER TRAIL PROFILE

Designer: CSA Structure Numbers:
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23001
Sheet Number 95



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Nikolett 2:50:55 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile20.dgn

Print Date: 1/10/2020
 File Name: 22768DES_Profile20.dgn
 Horiz. Scale: 1:50 Vert. Scale: As Noted

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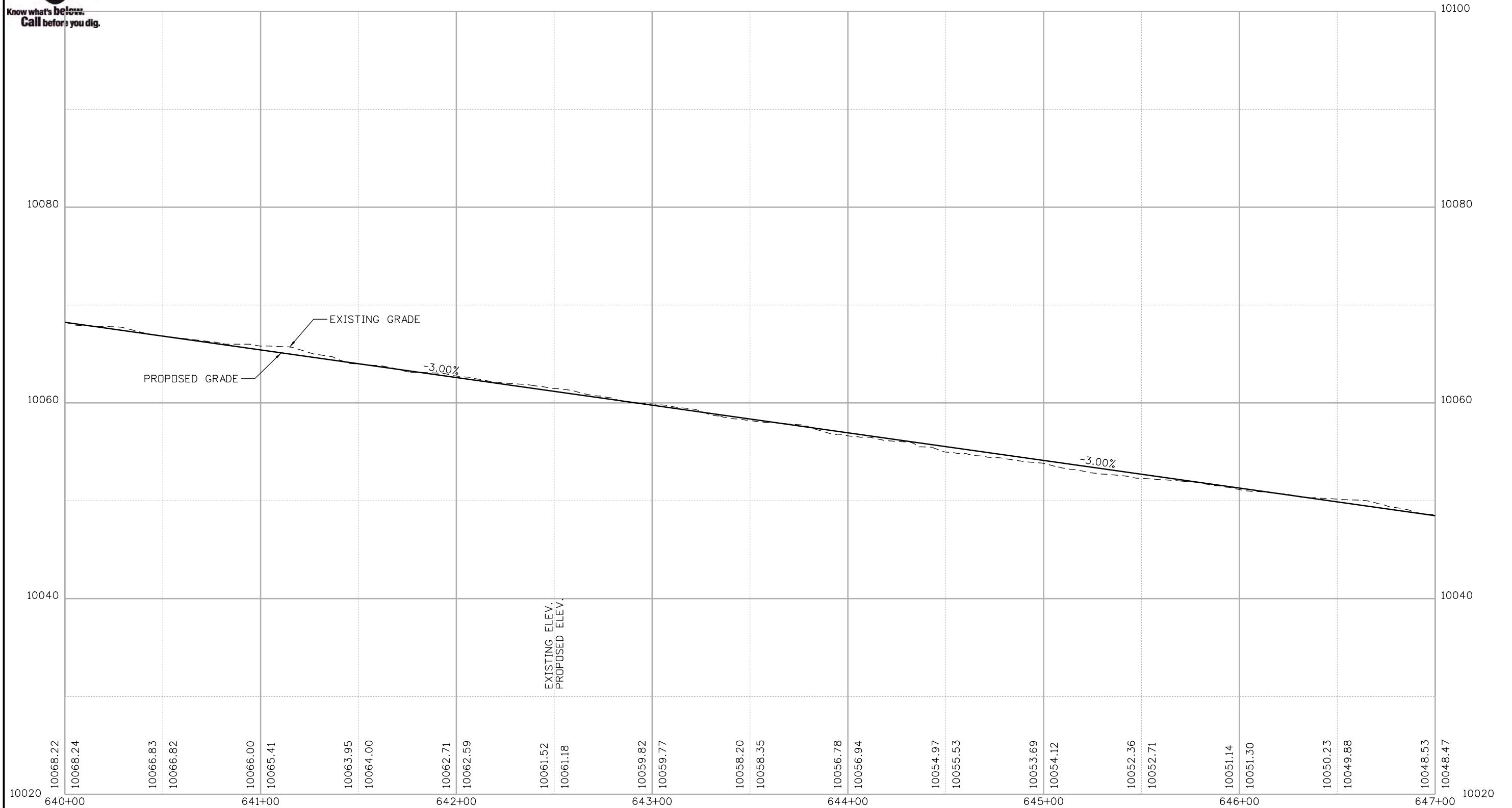
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 20 of 48

Project No./Code
XXXXXXXXXX
23001
Sheet Number 96



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Nikolett 2:50:56 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile21.dgn

Print Date: 1/10/2020
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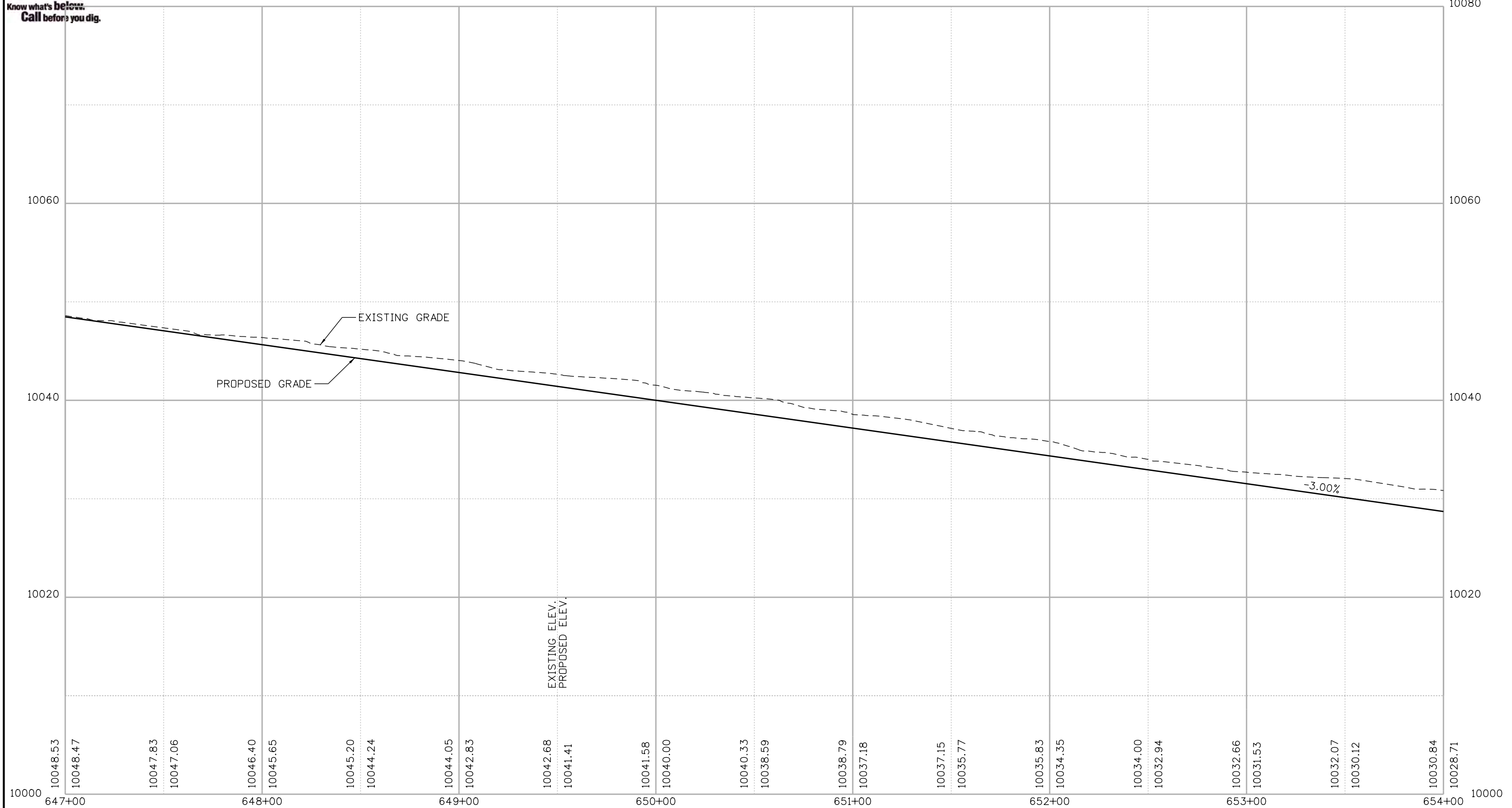
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 21 of 48

Project No./Code
XXXXXXXXXX
23001
Sheet Number 97



Know what's below.
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Nikolett 2:50:57 PM P:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile22.dgn


Print Date: 1/10/2020
 File Name: 22768DES_Profile22.dgn
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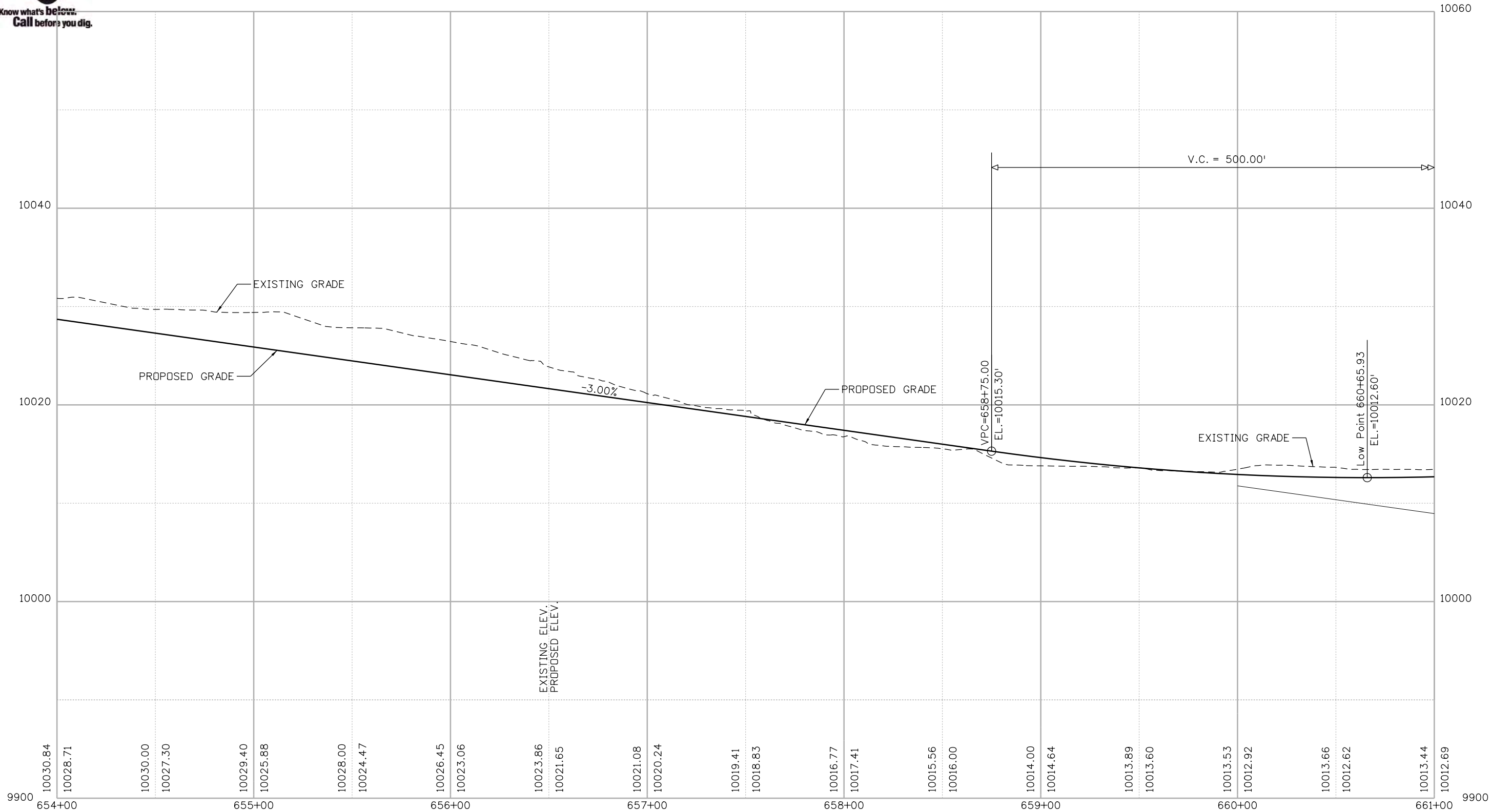
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 22 of 48

Project No./Code
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23001
Sheet Number 98



Know what's below.
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Nikolett 2:50:58 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile23.dgn

Print Date: 1/10/2020
 File Name: 22768DES_Profile23.dgn
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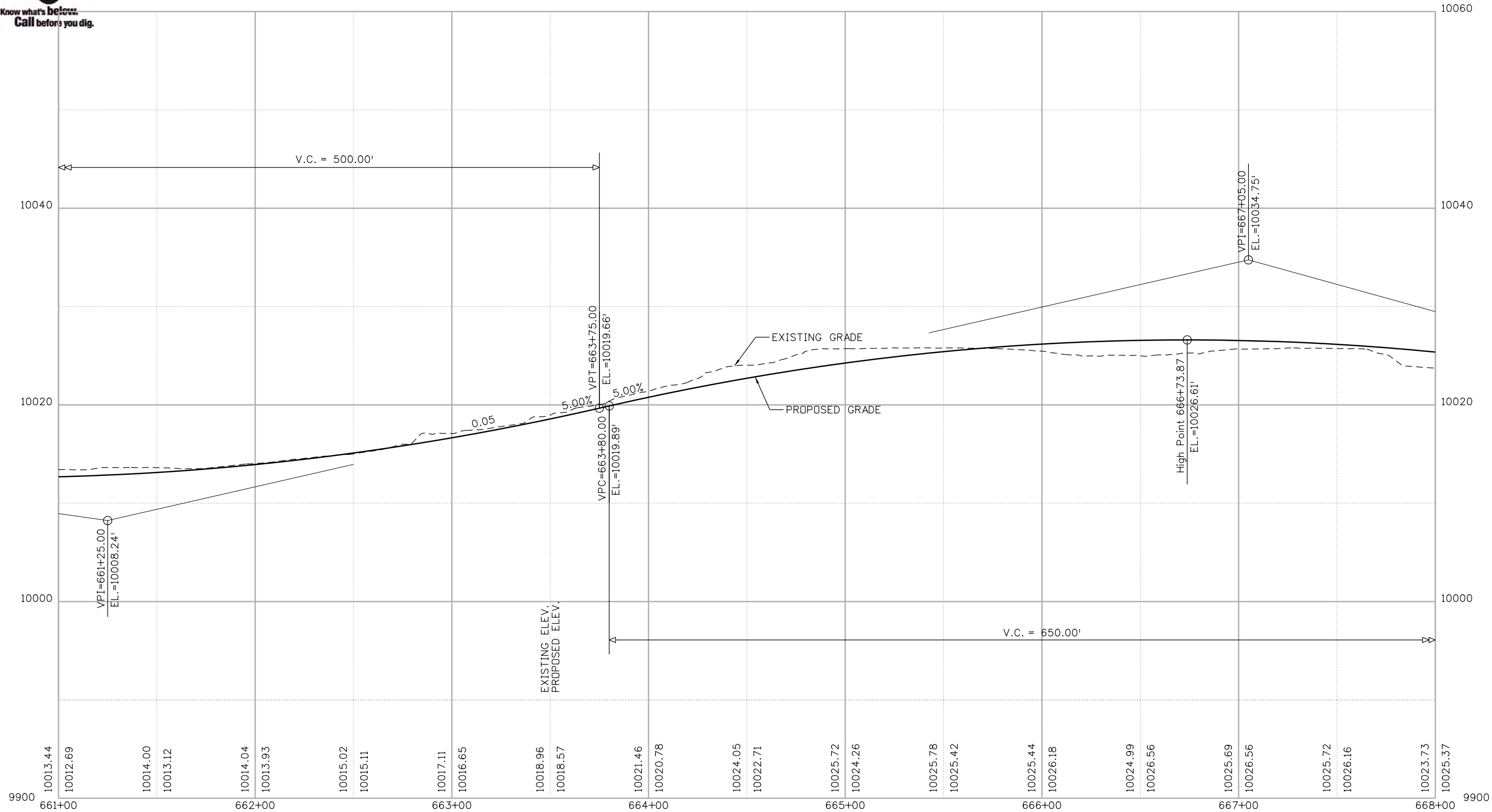
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 23 of 48

Project No./Code
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23001
Sheet Number 99



Know what's below.
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Print Date: 1/10/2020
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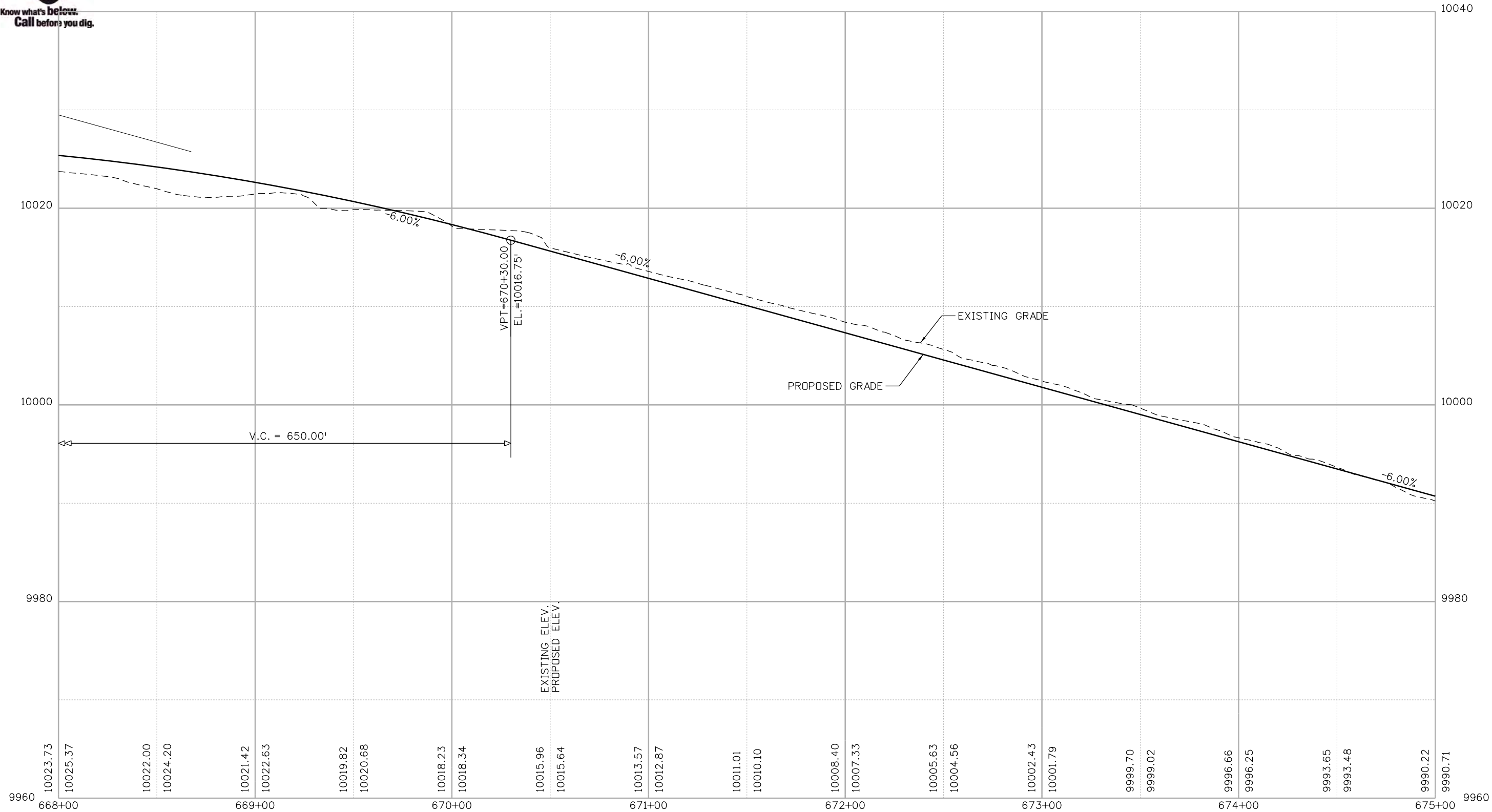
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 24 of 48

Project No./Code
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Know what's below.
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Nikolett 2:51:07 PM F:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile25.dgn

Print Date: 1/10/2020
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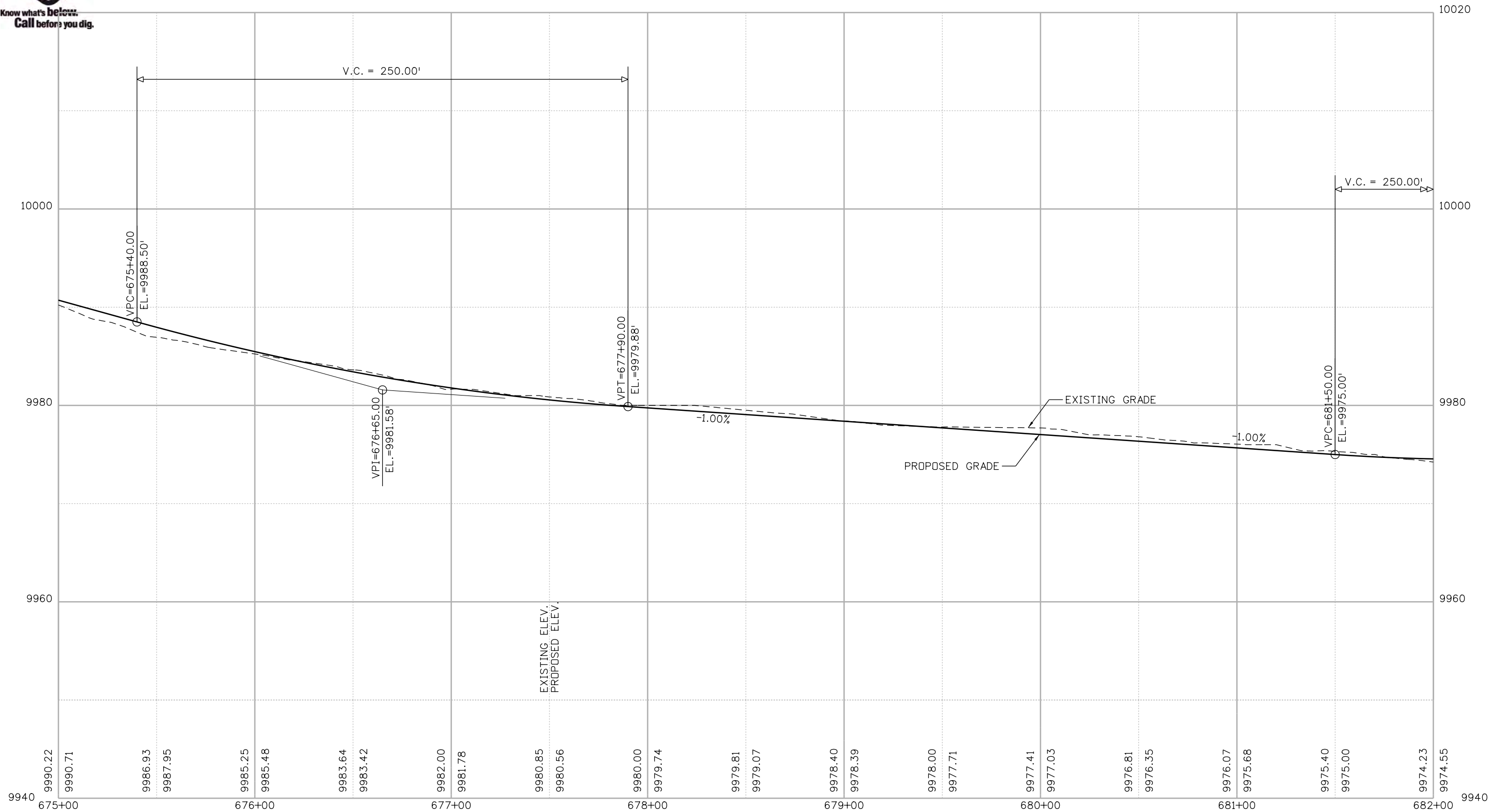
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 25 of 48

Project No./Code
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23001
Sheet Number 101



Know what's below.
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Print Date: 1/10/2020
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 Horiz. Scale: 1:50 Vert. Scale: As Noted

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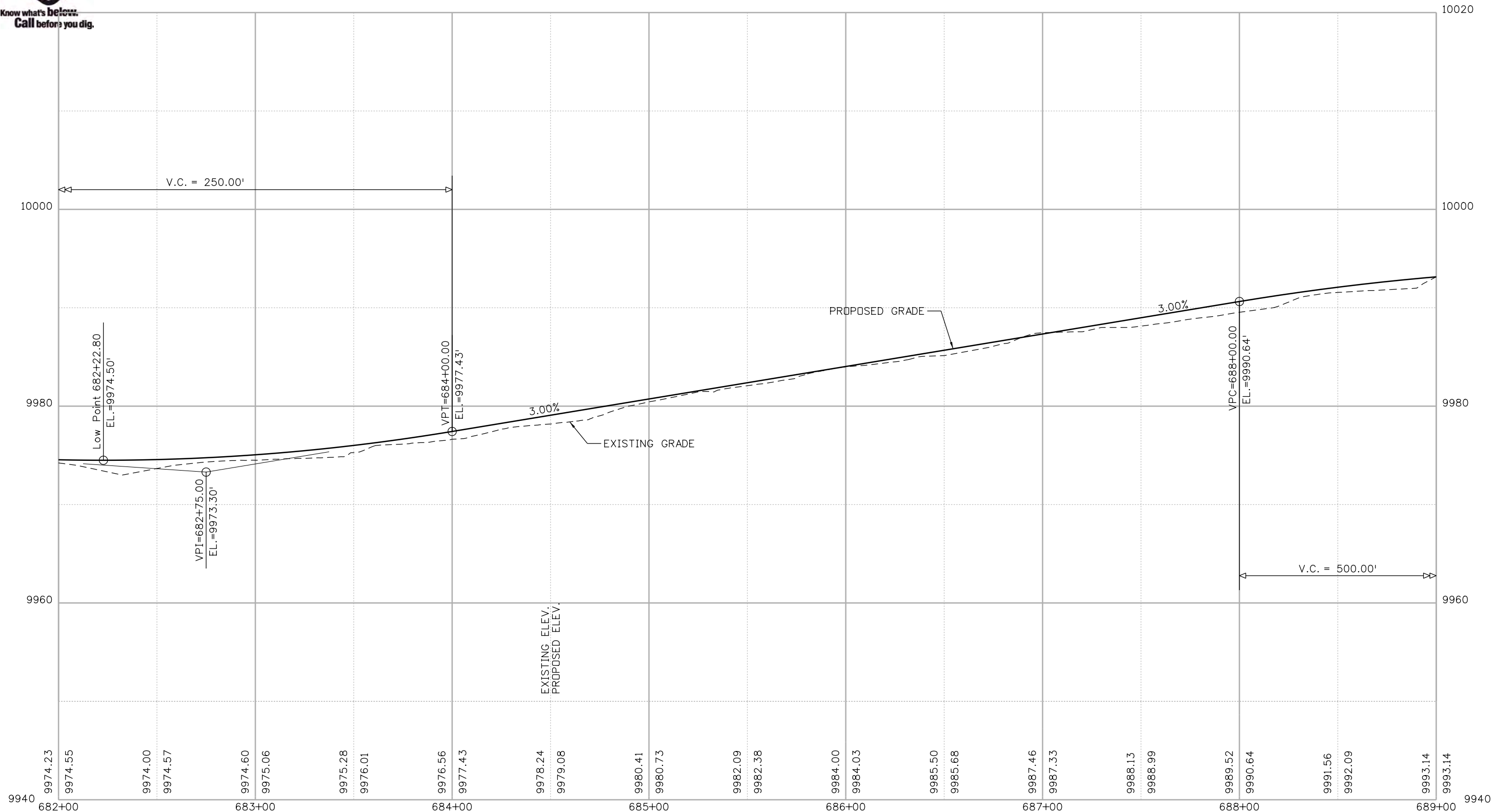
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 26 of 48

Project No./Code
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Sheet Number 102



Know what's below.
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Print Date: 1/10/2020
 File Name: 22768DES_Profile27.dgn
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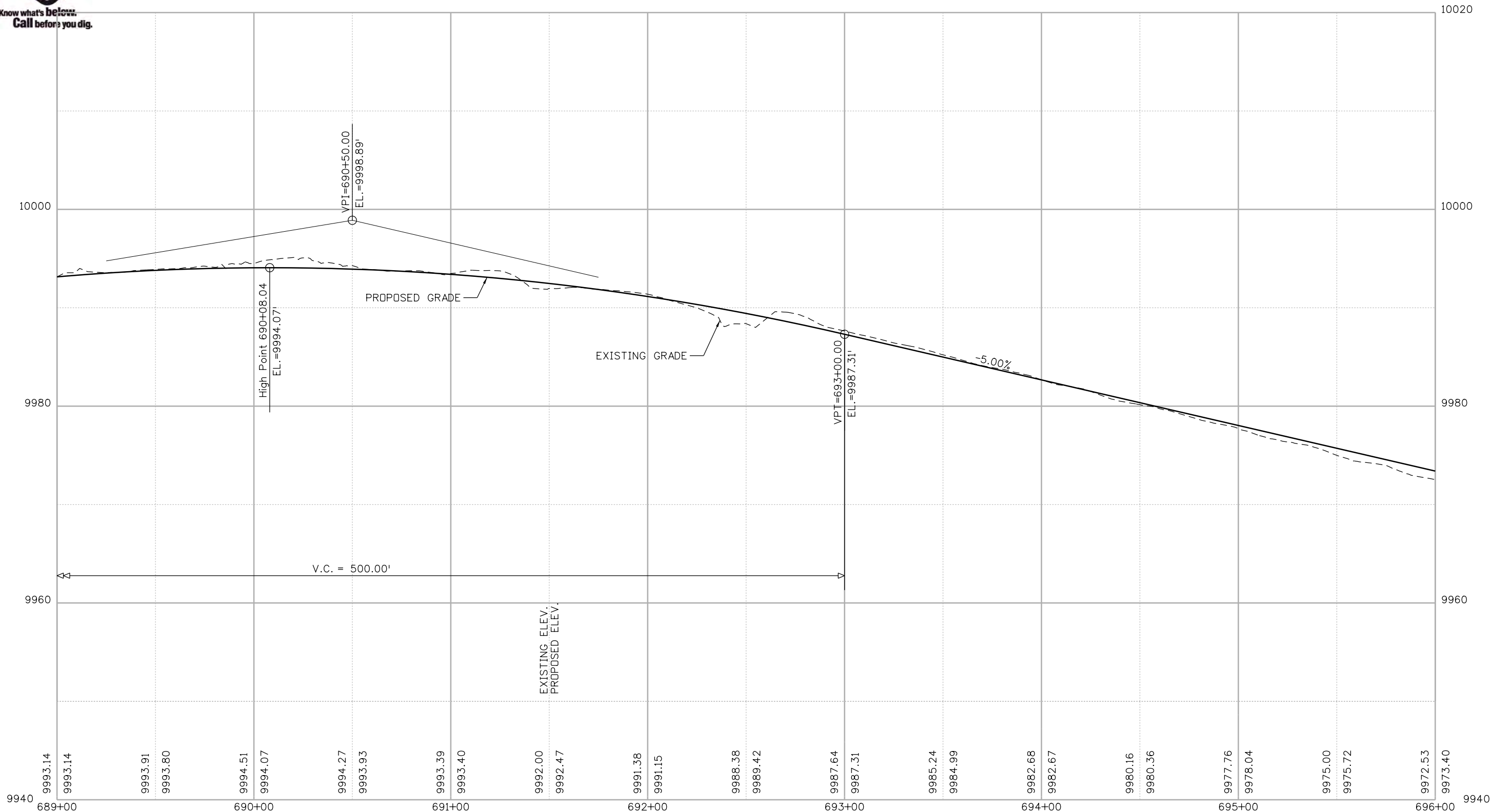
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 27 of 48

Project No./Code
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Know what's below.
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Print Date: 1/10/2020
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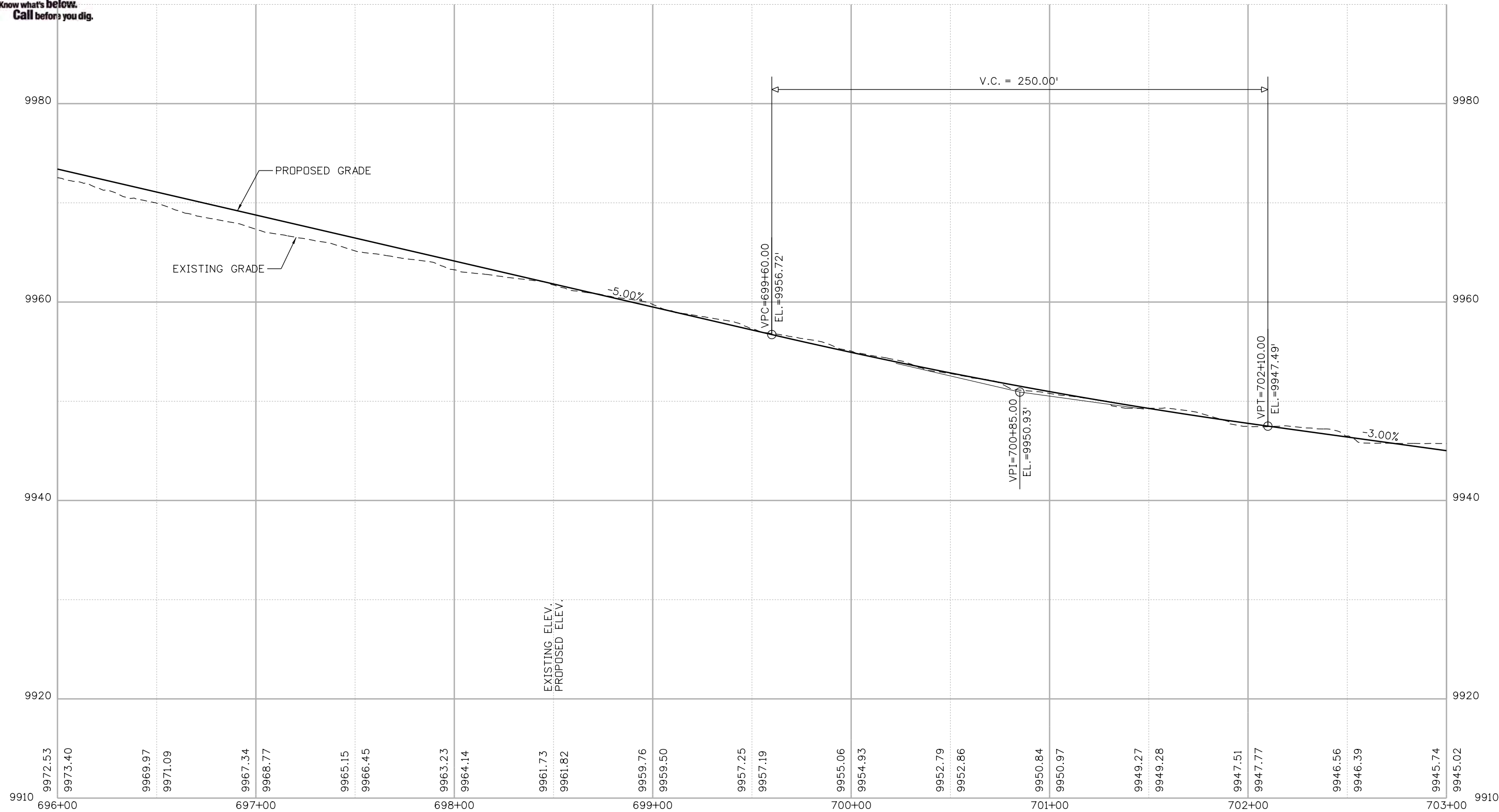
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 28 of 48

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Know what's below.
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N:\kott 2:51:12 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile29.dgn

Print Date: 1/10/2020
 File Name: 22768DES_Profile29.dgn
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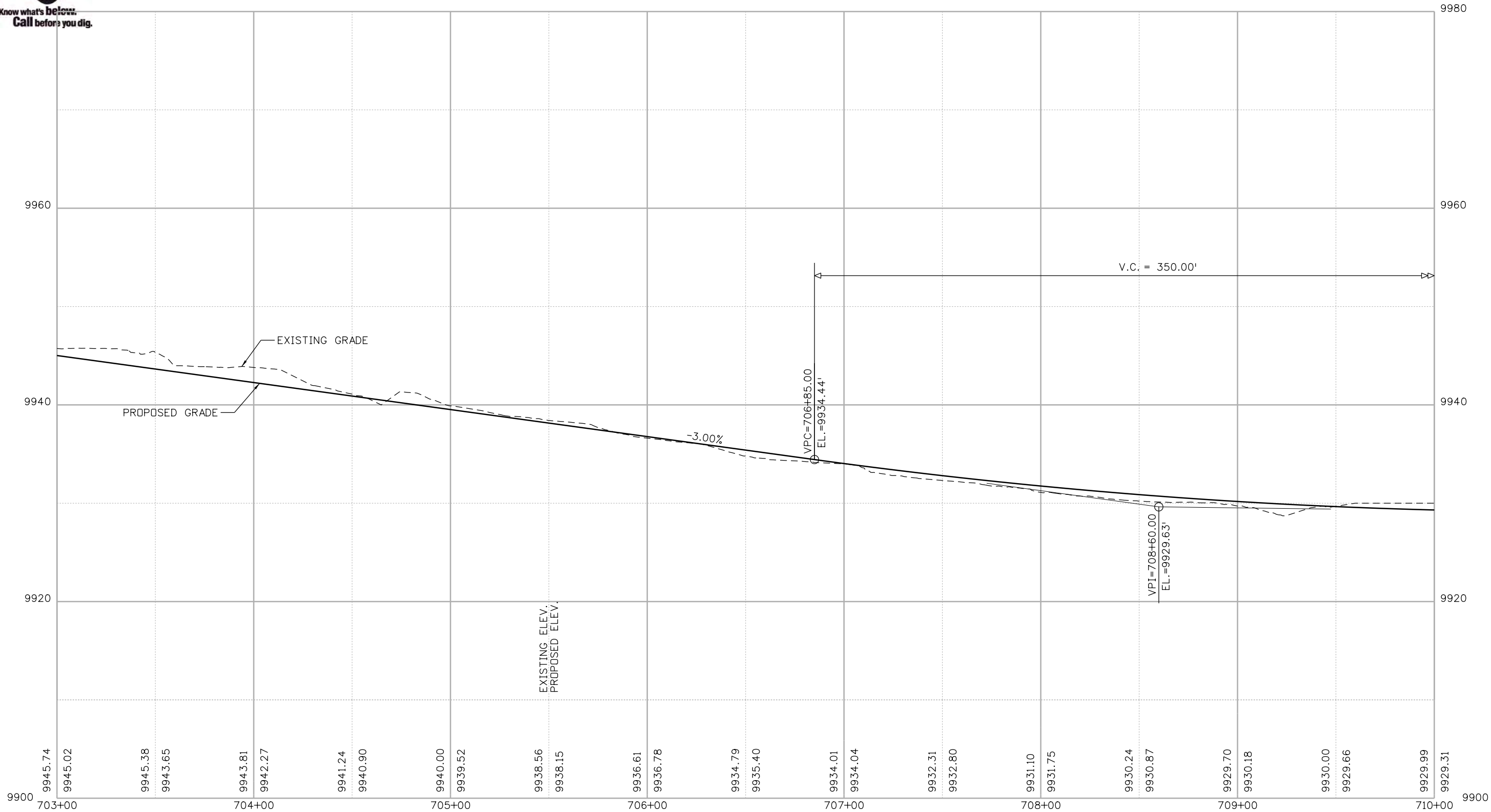
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 29 of 48

Project No./Code
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Sheet Number 105



Know what's below.
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Nikolett 2:51:14 PM P:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile30.dgn

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 File Name: 22768DES_Profile30.dgn
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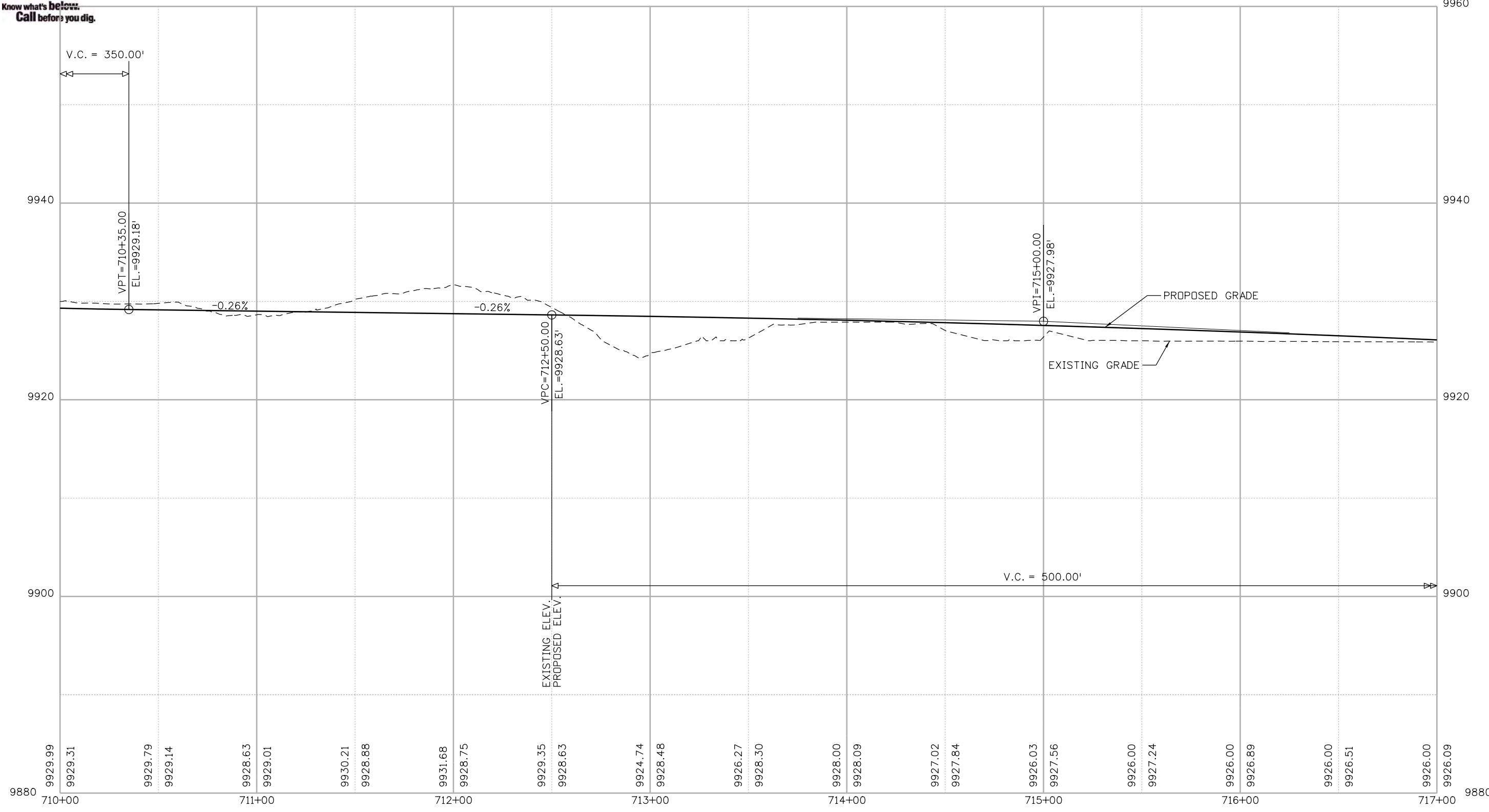
BLUE RIVER TRAIL PROFILE

Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	30 of 48
Sheet Subset:	PROFILE		

Project No./Code	
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Know what's below.
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N:\kolett 2-51:15 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile31.dgn

Print Date: 1/10/2020
 File Name: 22768DES_Profile31.dgn
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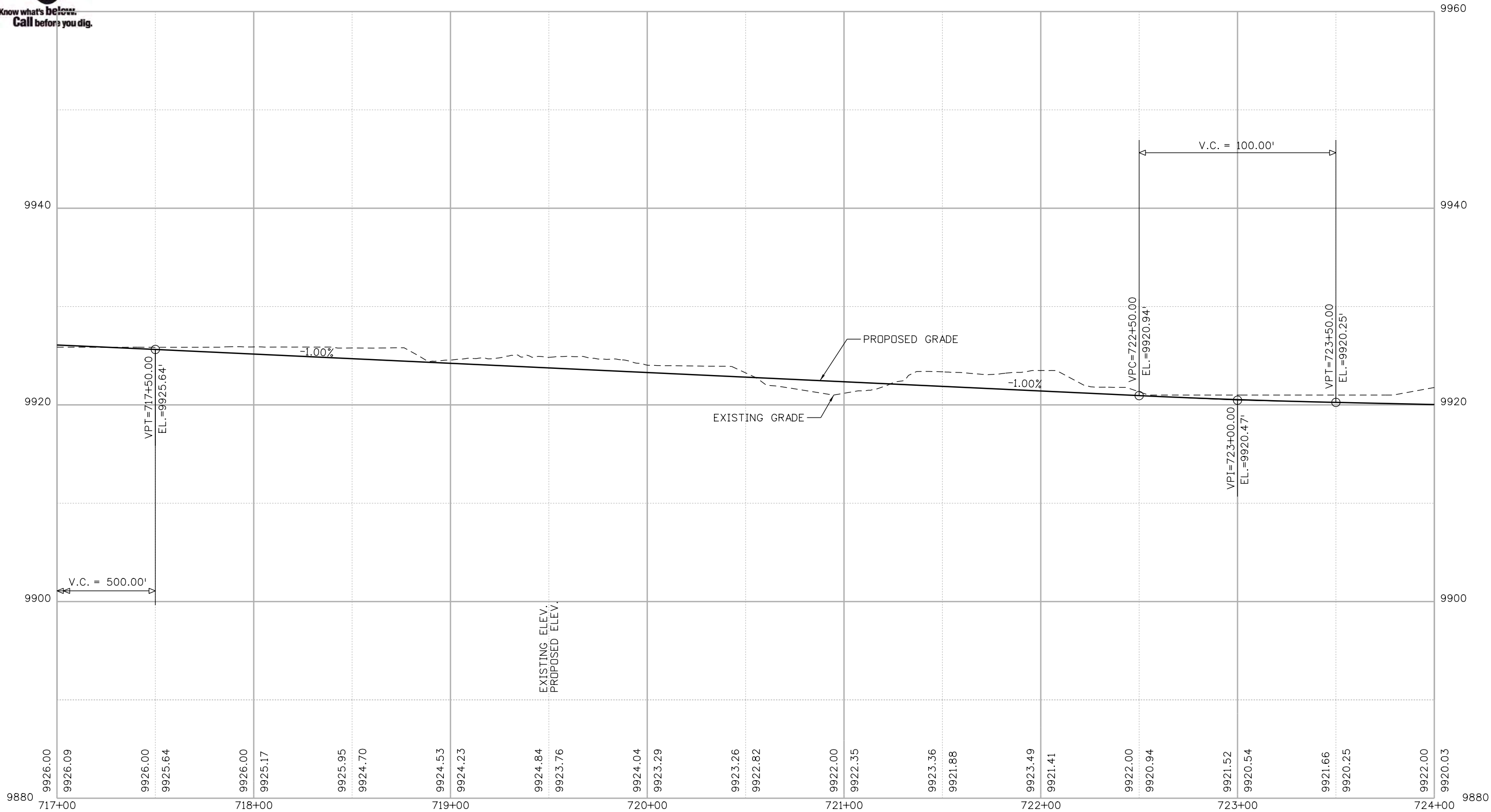
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BLUE RIVER TRAIL PROFILE			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	31 of 48
Sheet Subset:	PROFILE		

Project No./Code
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Sheet Number 107



Know what's below.
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Nikolett 2:51:16 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile32.dgn

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File Name: 22768DES_Profile32.dgn	
Horiz. Scale: 1:50	Vert. Scale: As Noted
Stolfus	
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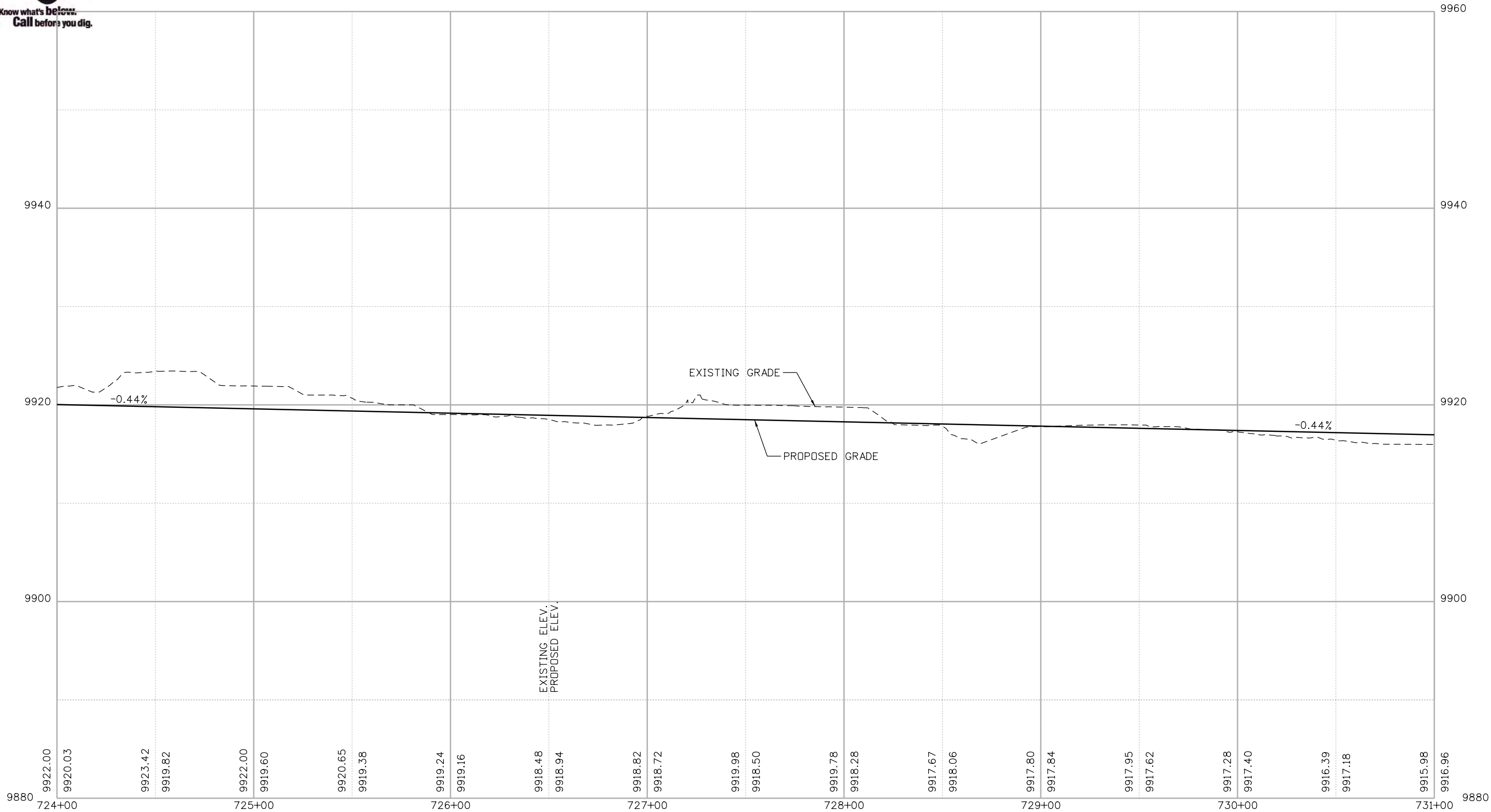
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BLUE RIVER TRAIL PROFILE			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	PROFILE	Subset Sheets:	32 of 48

Project No./Code
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Sheet Number 108




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N:\kott 2:51:17 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile33.dgn


Print Date: 1/10/2020
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 Horiz. Scale: 1:50 Vert. Scale: As Noted



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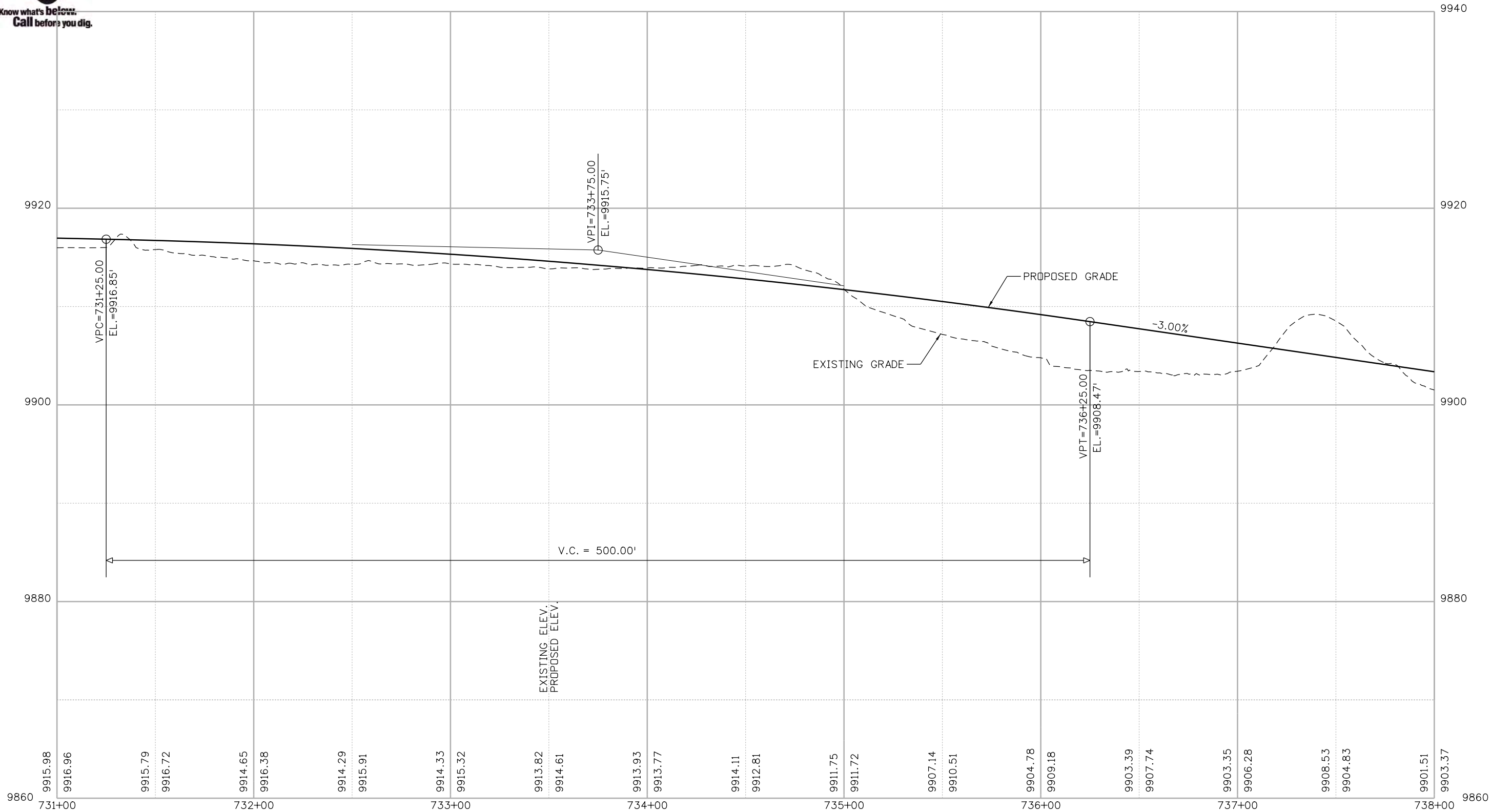
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 33 of 48

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N:\kolett 2:51:18 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile34.dgn

Print Date: 1/10/2020
 File Name: 22768DES_Profile34.dgn
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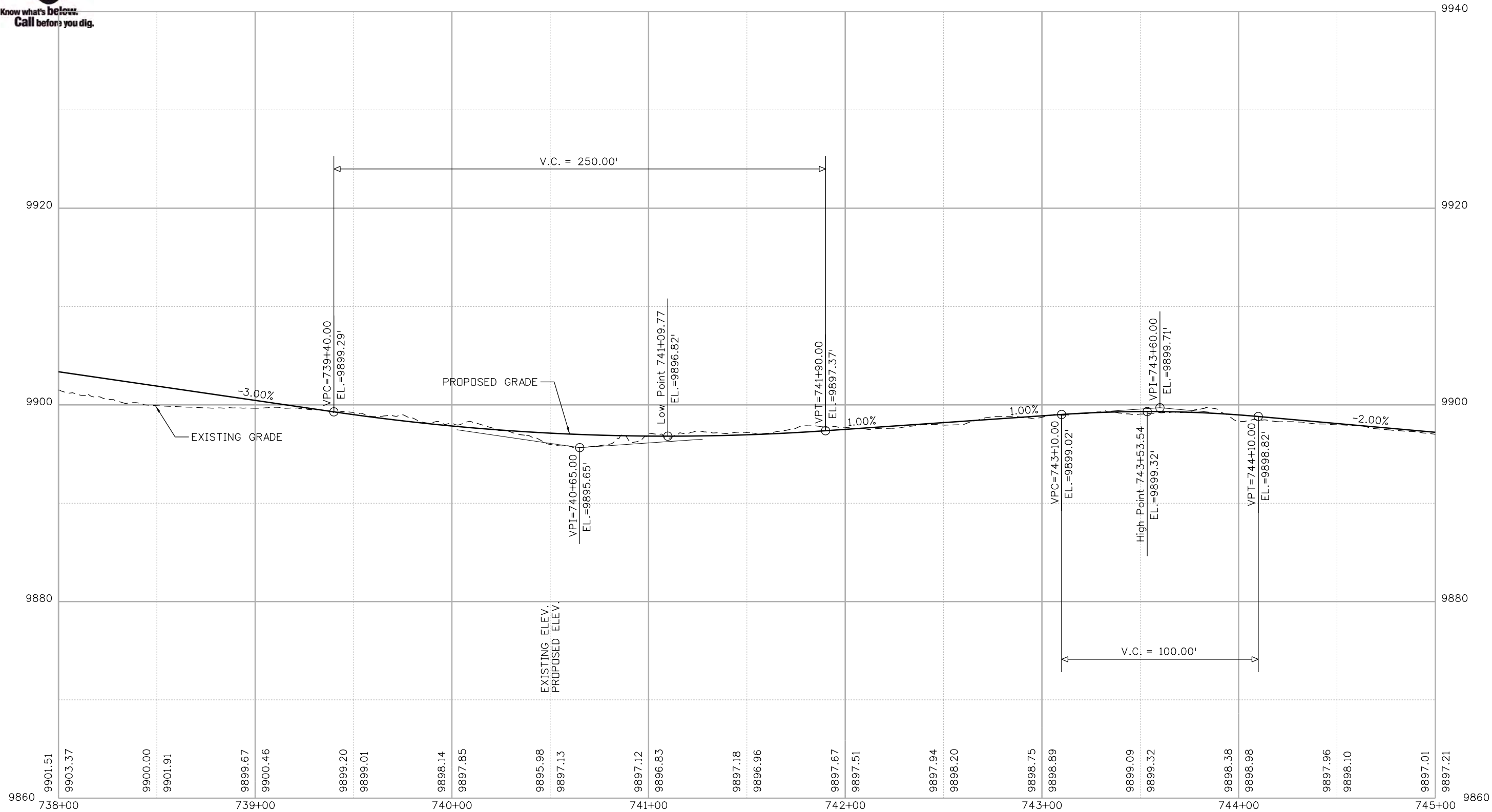
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 34 of 48

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Nikolett 2:51:19 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile35.dgn

Print Date: 1/10/2020
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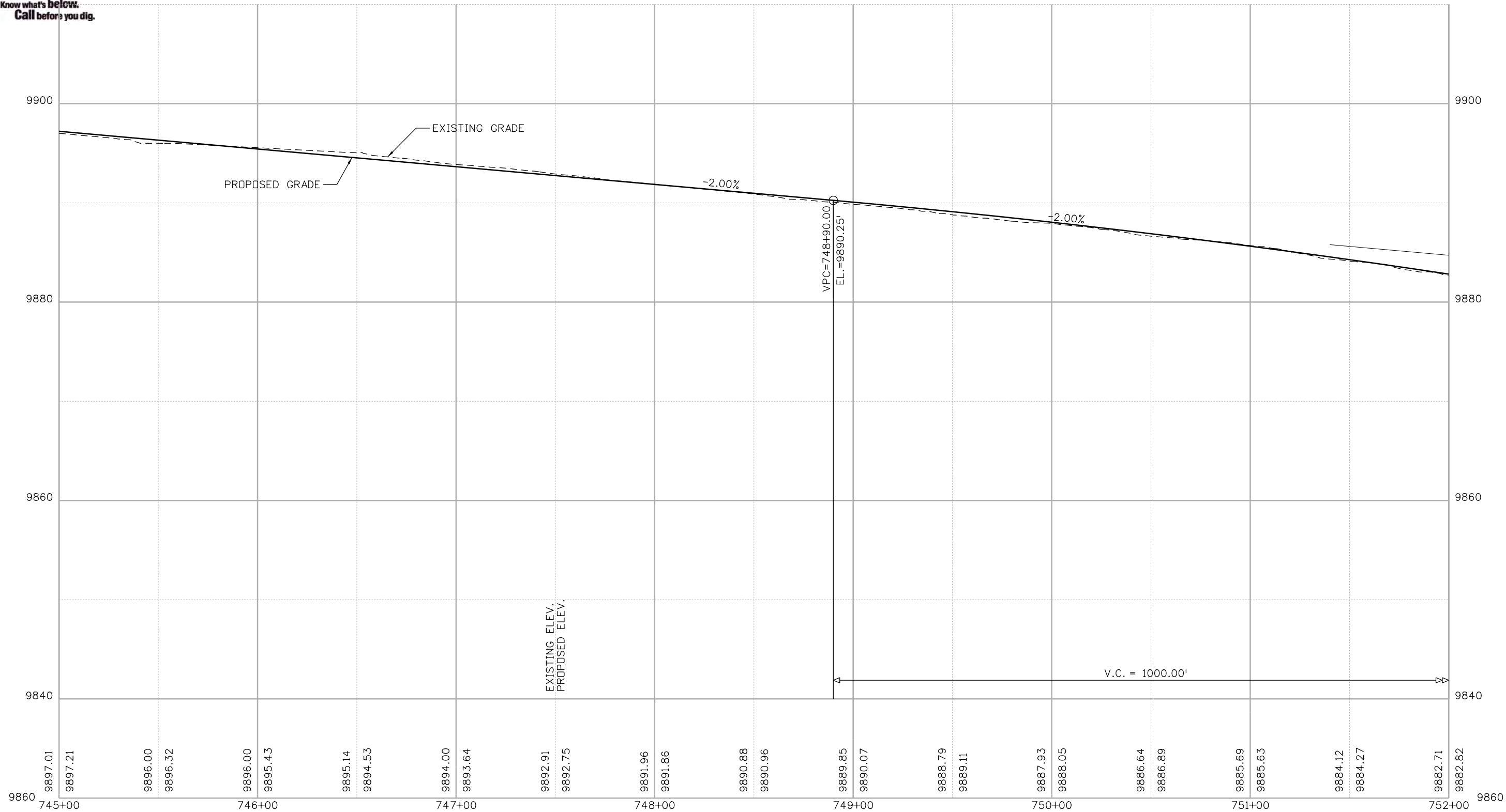
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Structure Numbers
 Subset Sheets: 35 of 48

Project No./Code
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23001
Sheet Number 111



Know what's below.
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Nikolett 2:51:20 PM F:\1000.005.10 R3 CD Blue River ACP\TrailDesign\Sheets\22768DES_Profile36.dgn

Print Date: 1/10/2020
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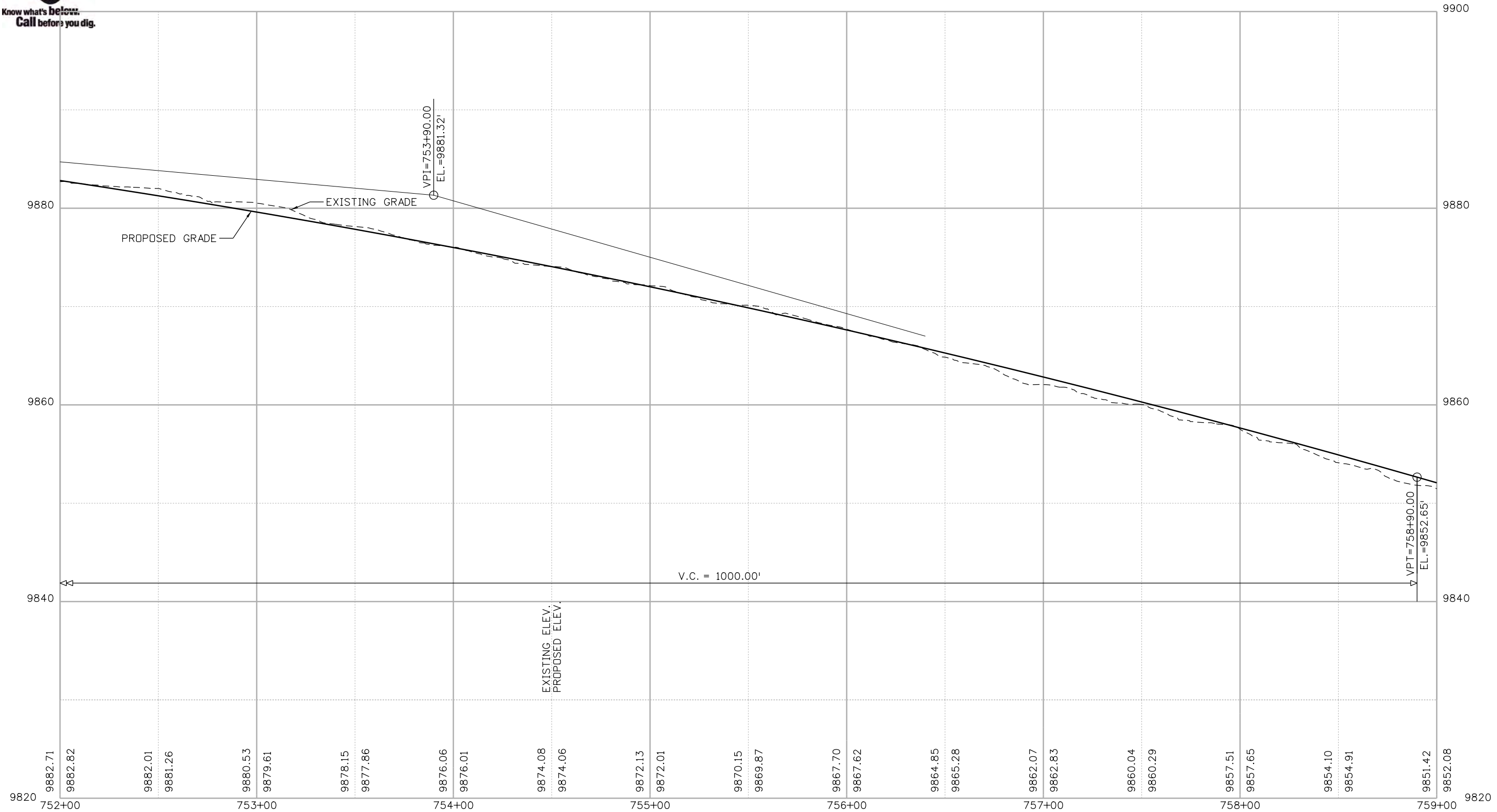
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 36 of 48

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23001	
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Nikolett 2:51:21 PM P:\1000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile37.dgn

Print Date: 1/10/2020
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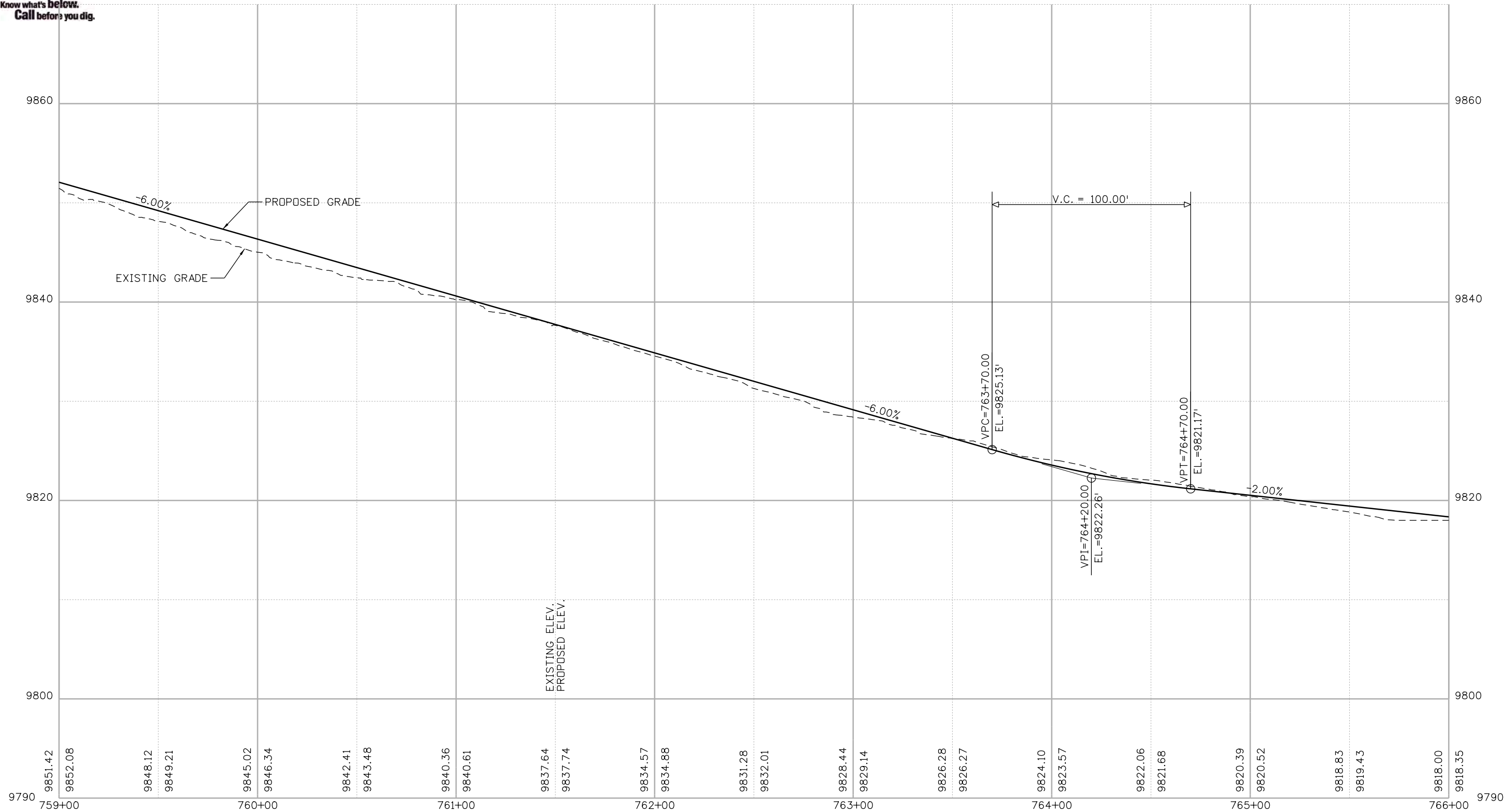
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 37 of 48

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Nikolett 2:51:22 PM F:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile38.dgn

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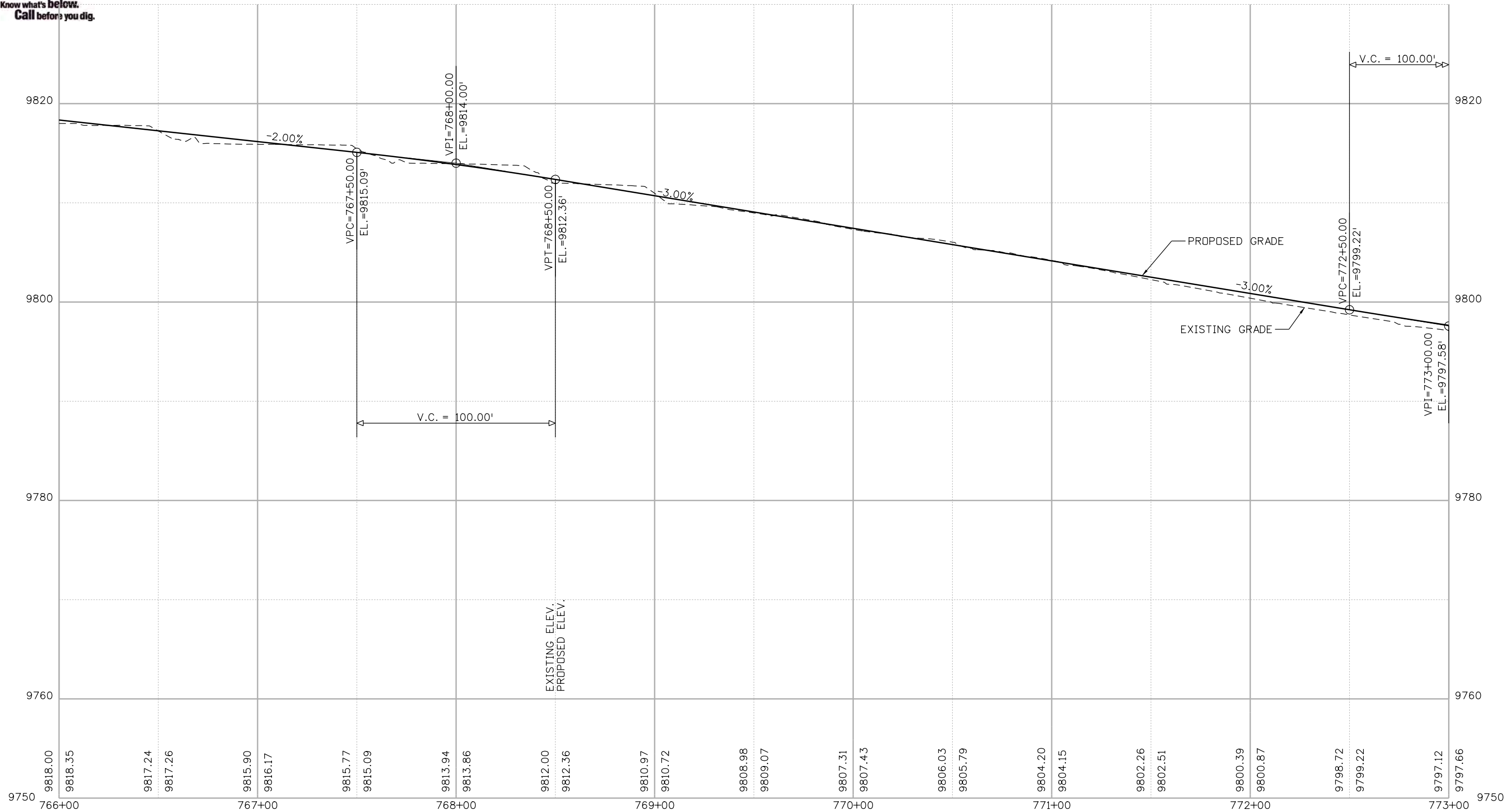
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Structure Numbers
 Subset Sheets: 38 of 48


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Nikolett 2:51:23 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile39.dgn

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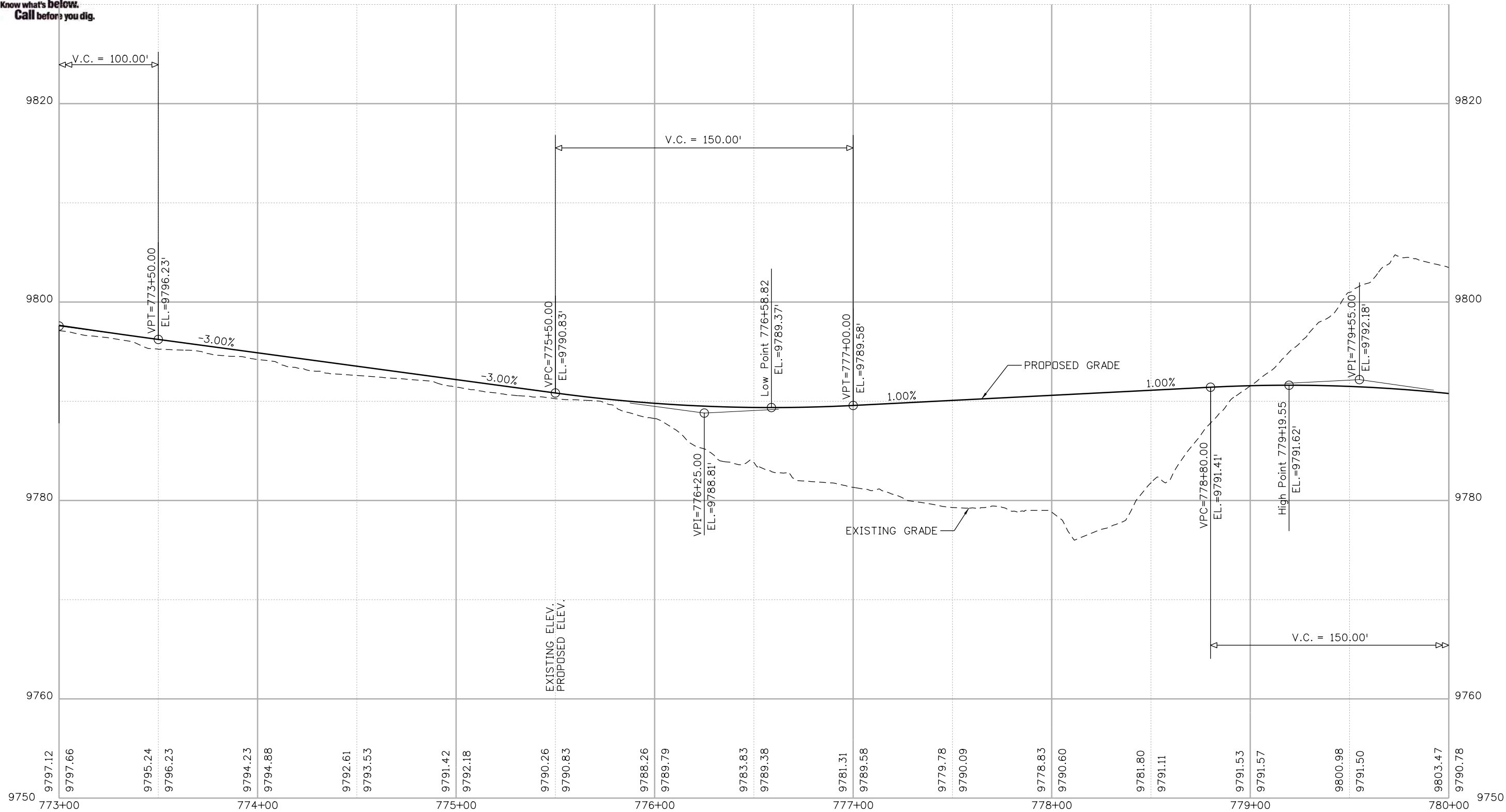
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BLUE RIVER TRAIL PROFILE			
Designer:	CSA	Structure Numbers	
Detailer:	NG		
Sheet Subset:	PROFILE	Subset Sheets:	39 of 48

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Nikolett 2:51:24 PM P:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile40.dgn

Print Date: 1/10/2020
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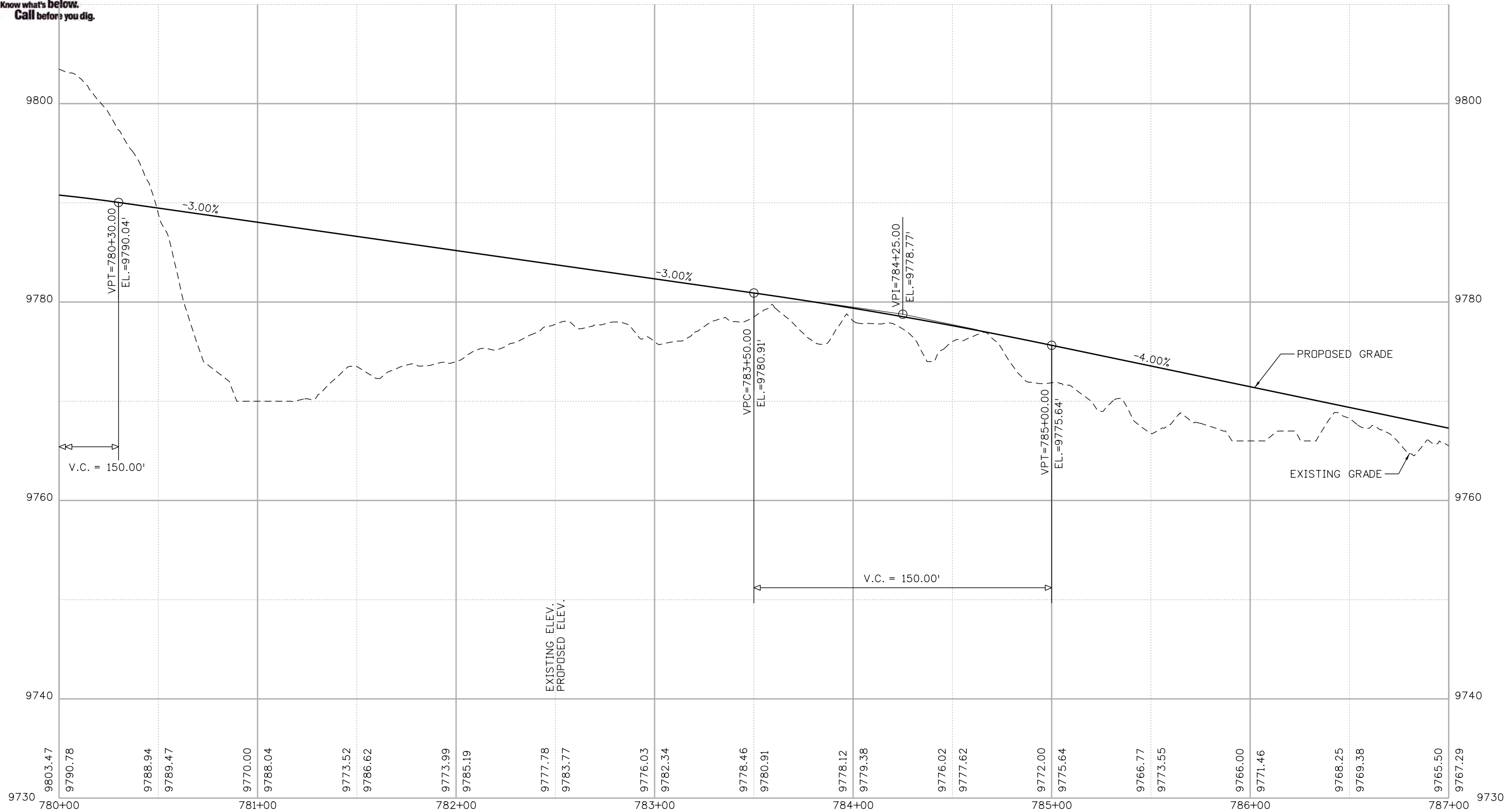
As Constructed
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Revised:
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BLUE RIVER TRAIL PROFILE			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	40 of 48
Sheet Subset:	PROFILE		

Project No./Code
XXXXXXXXXX
23001
Sheet Number 116



Know what's below.
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Nikolett 2:51:26 PM F:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile41.dgn

Print Date: 1/10/2020
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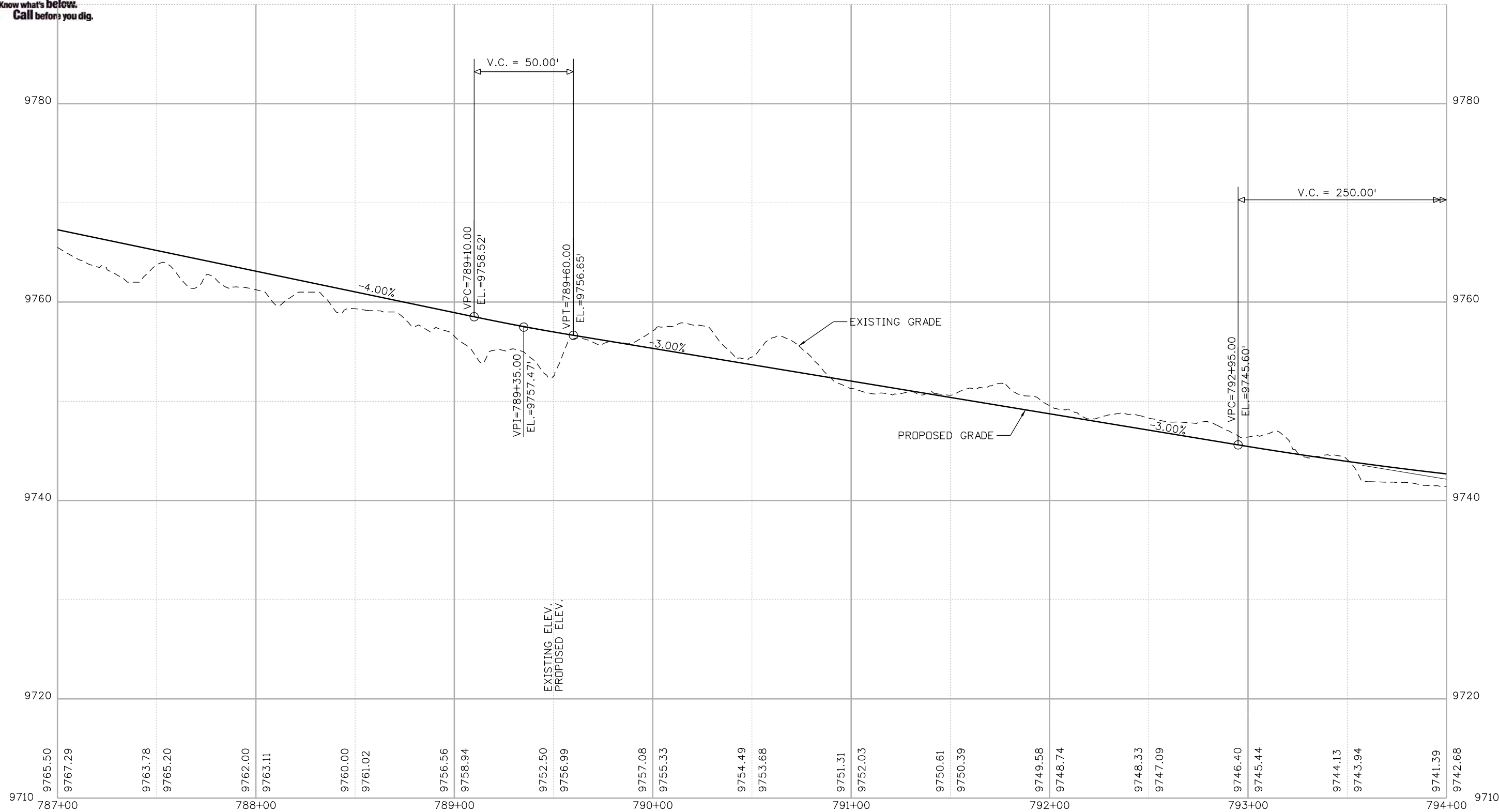
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 41 of 48

Project No./Code
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Sheet Number 117



Know what's below.
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BLUE RIVER TRAIL PROFILE

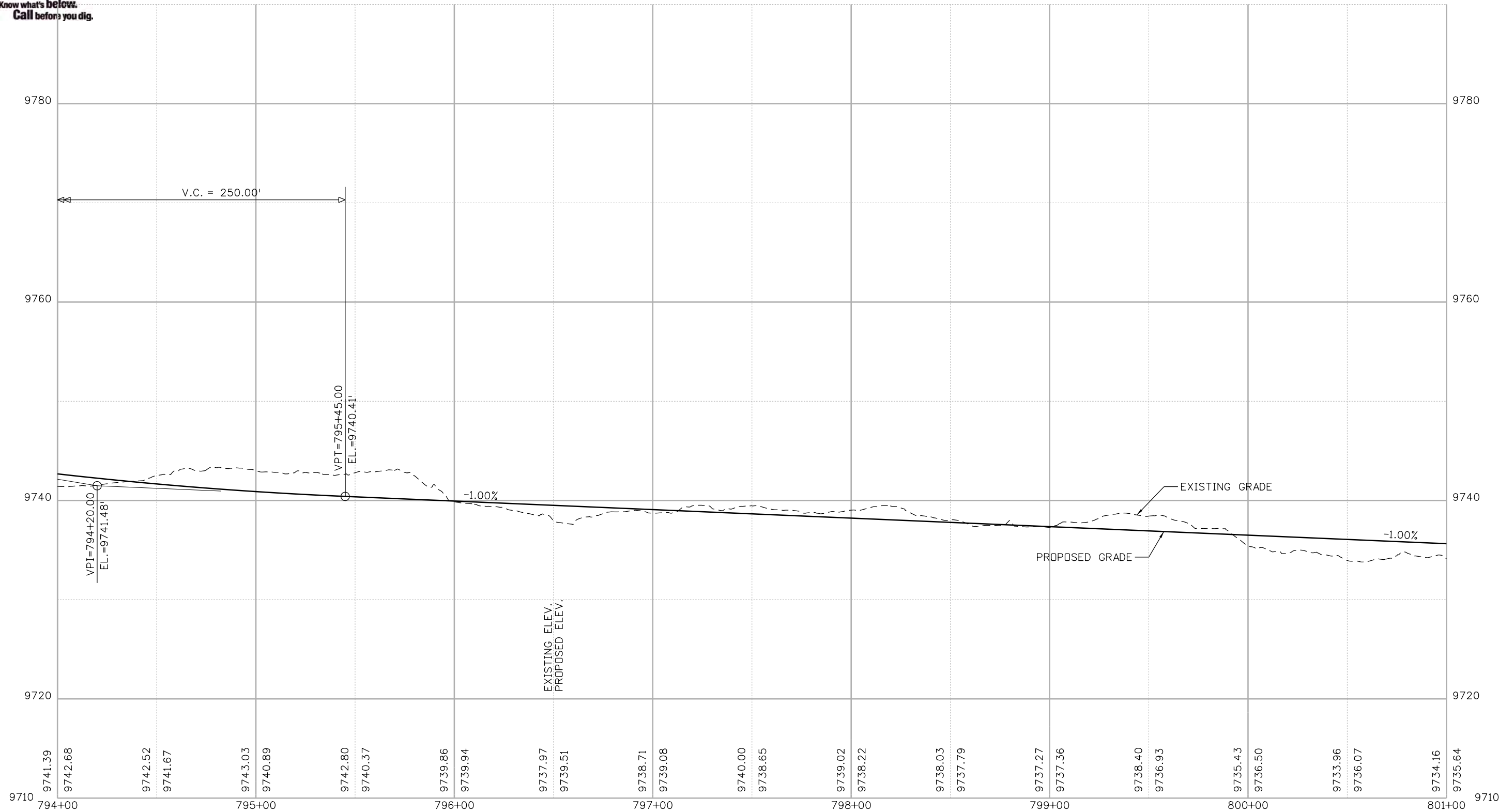
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Structure Numbers
 Subset Sheets: 42 of 48

Project No./Code
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23001
Sheet Number 118



Know what's below.
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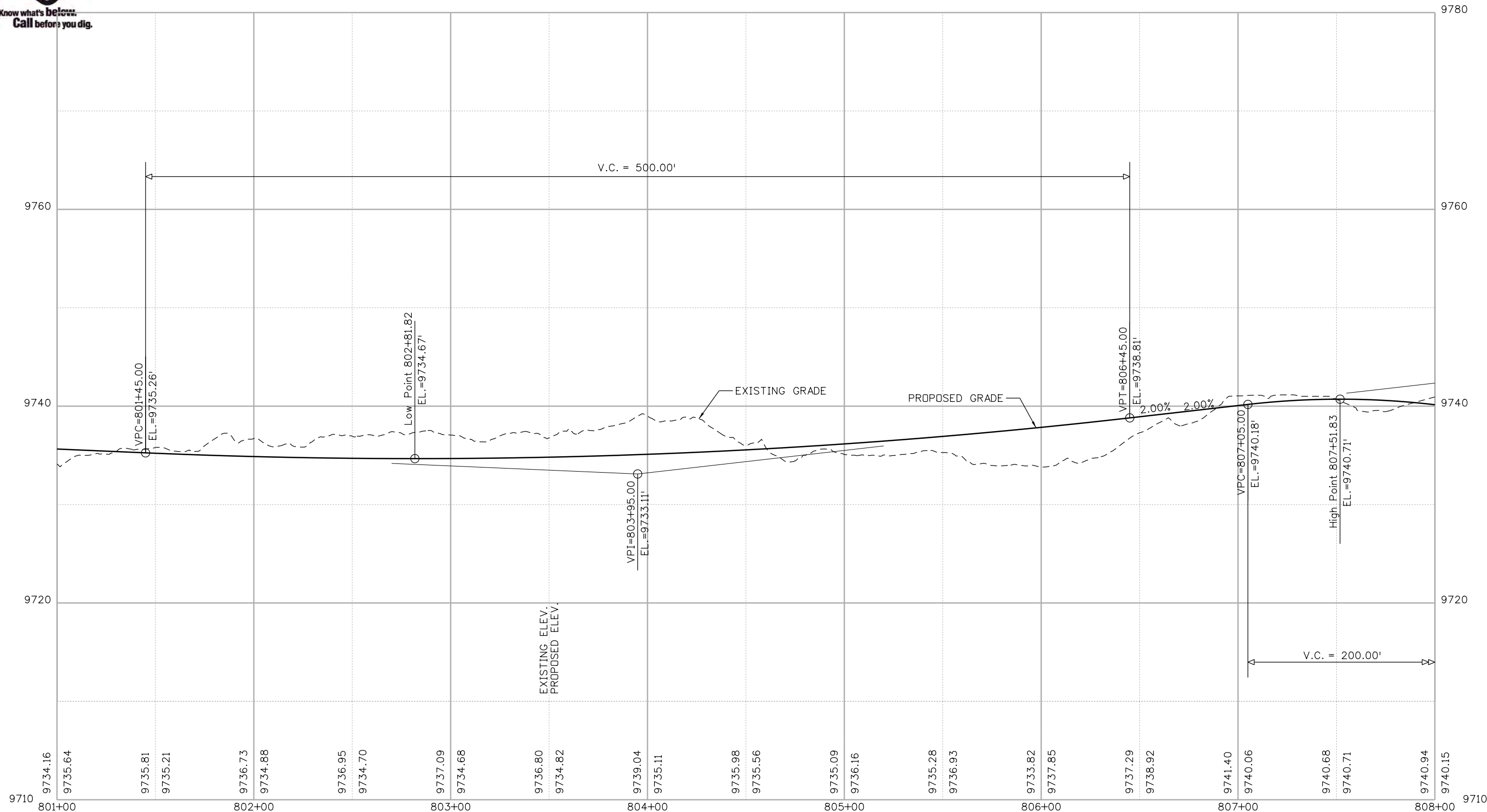
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 43 of 48

Project No./Code
XXXXXXXXXX
23001
Sheet Number 119



Know what's below.
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Nikolett 2:51:29 PM F:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Sheets\22768DES_Profile44.dgn

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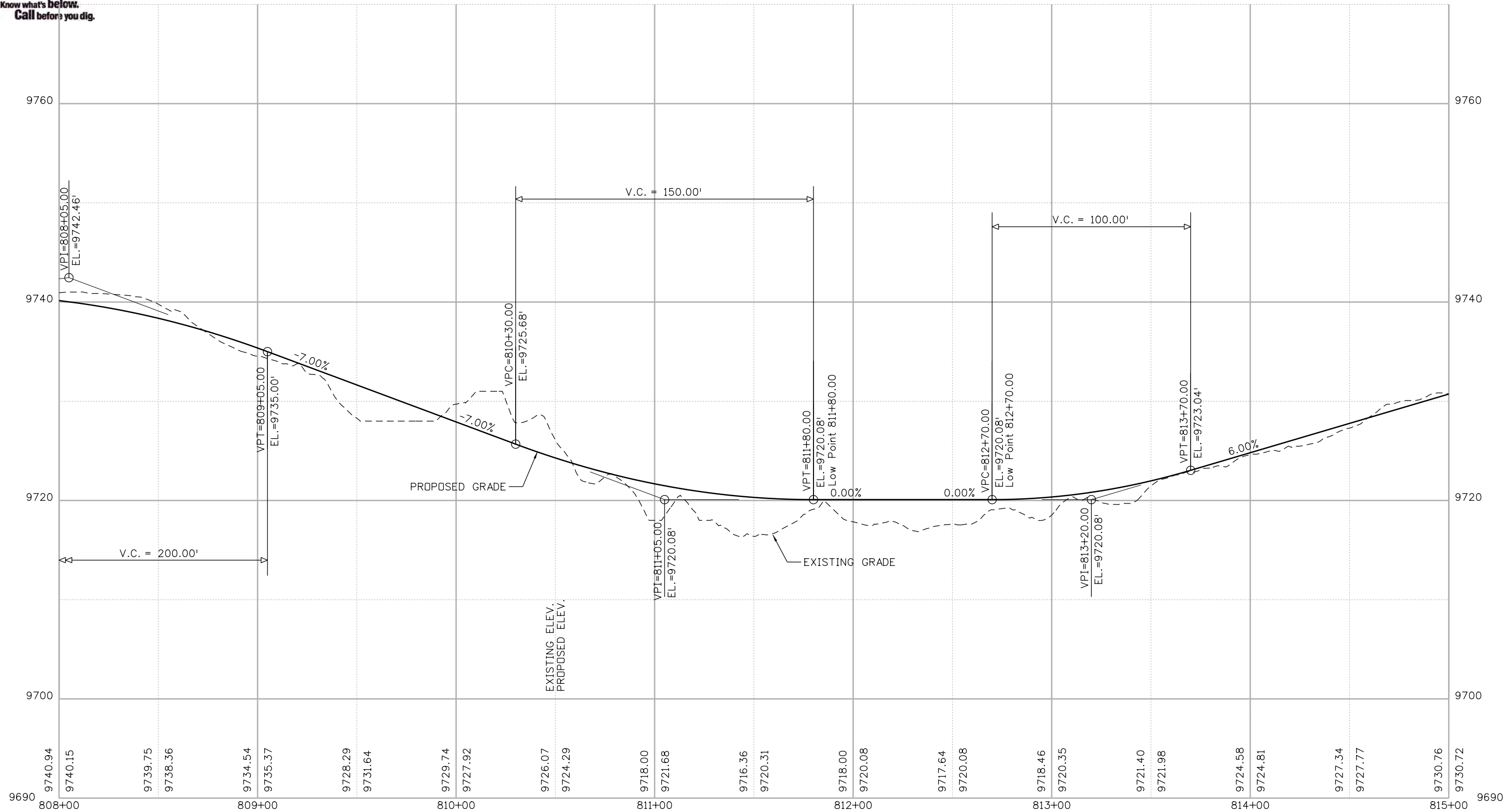
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 44 of 48

Project No./Code
XXXXXXXXXX
23001
Sheet Number 120



Know what's below.
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Nikolett 2:51:30 PM P:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile45.dgn

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Date:	Comments	Init.

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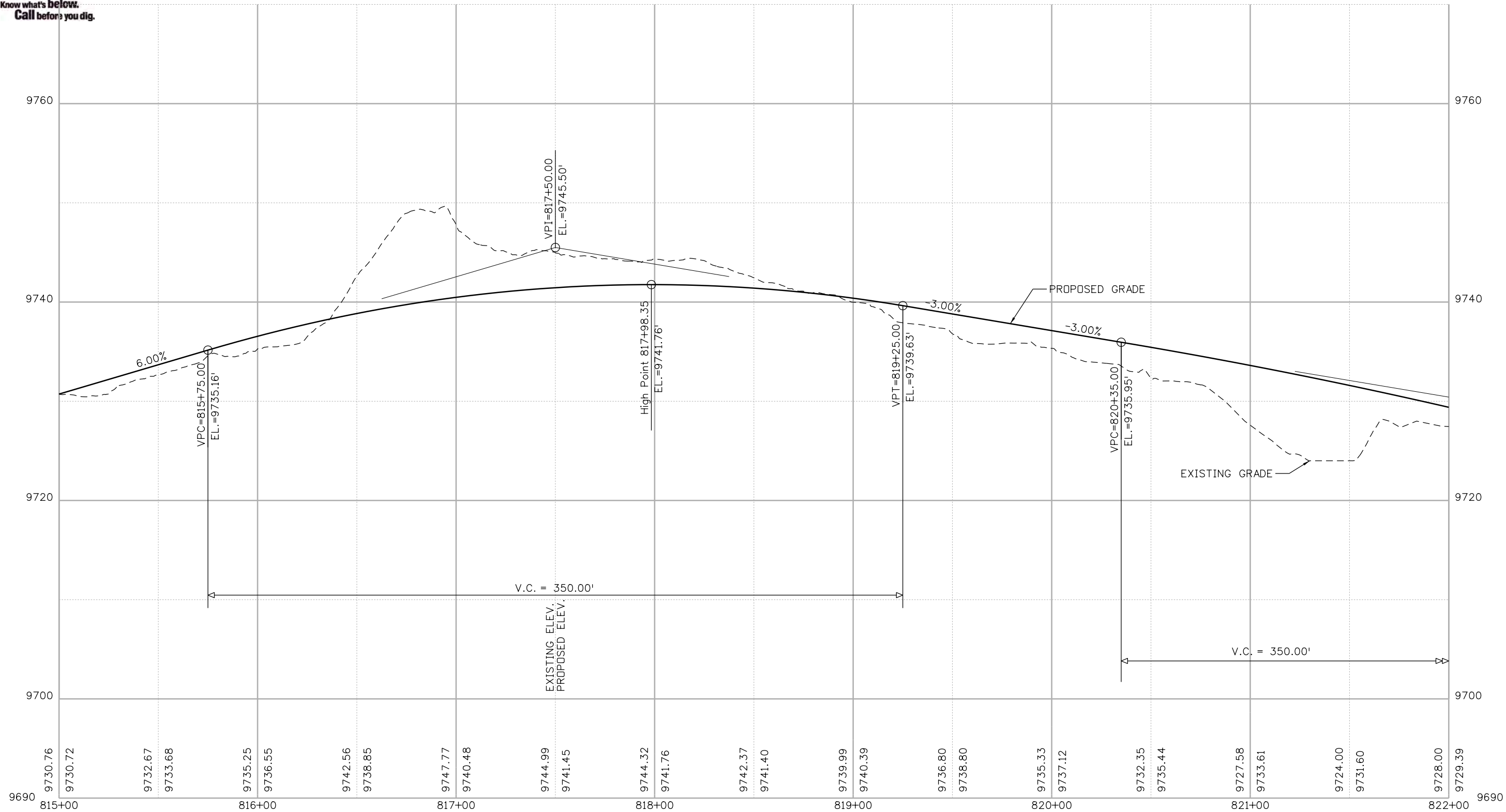
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Revised:
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BLUE RIVER TRAIL PROFILE			
Designer:	CSA	Structure Numbers	
Detailer:	NG	Subset Sheets:	45 of 48
Sheet Subset:	PROFILE		

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23001
Sheet Number 121



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Nikolett 2:51:31 PM P:\10000.005.10 R3 C09 Blue River ACP\Trail Design\Sheets\22768DES_Profile46.dgn

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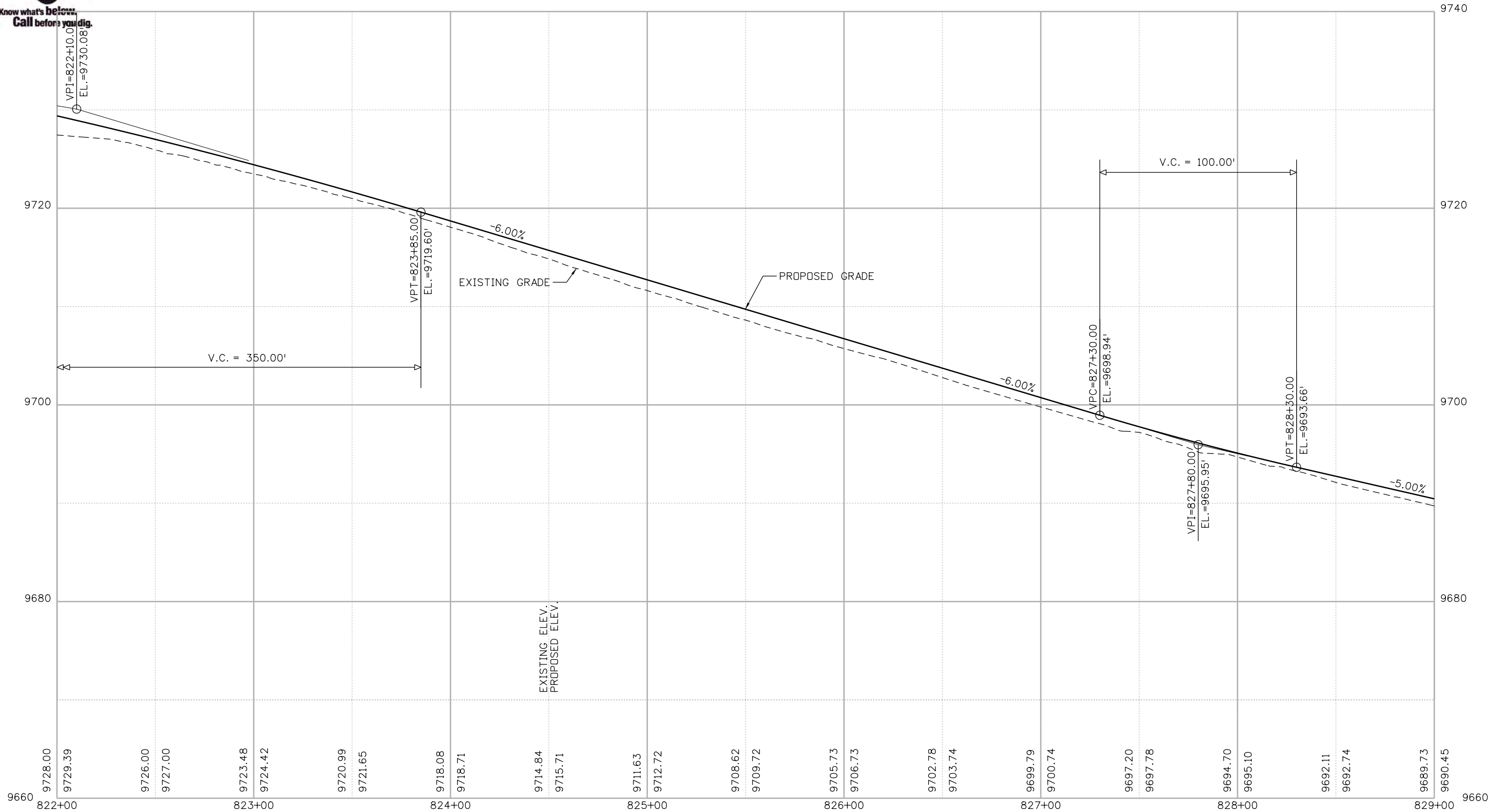
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 46 of 48

Project No./Code
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Sheet Number 122



Know what's below.
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Print Date: 1/10/2020
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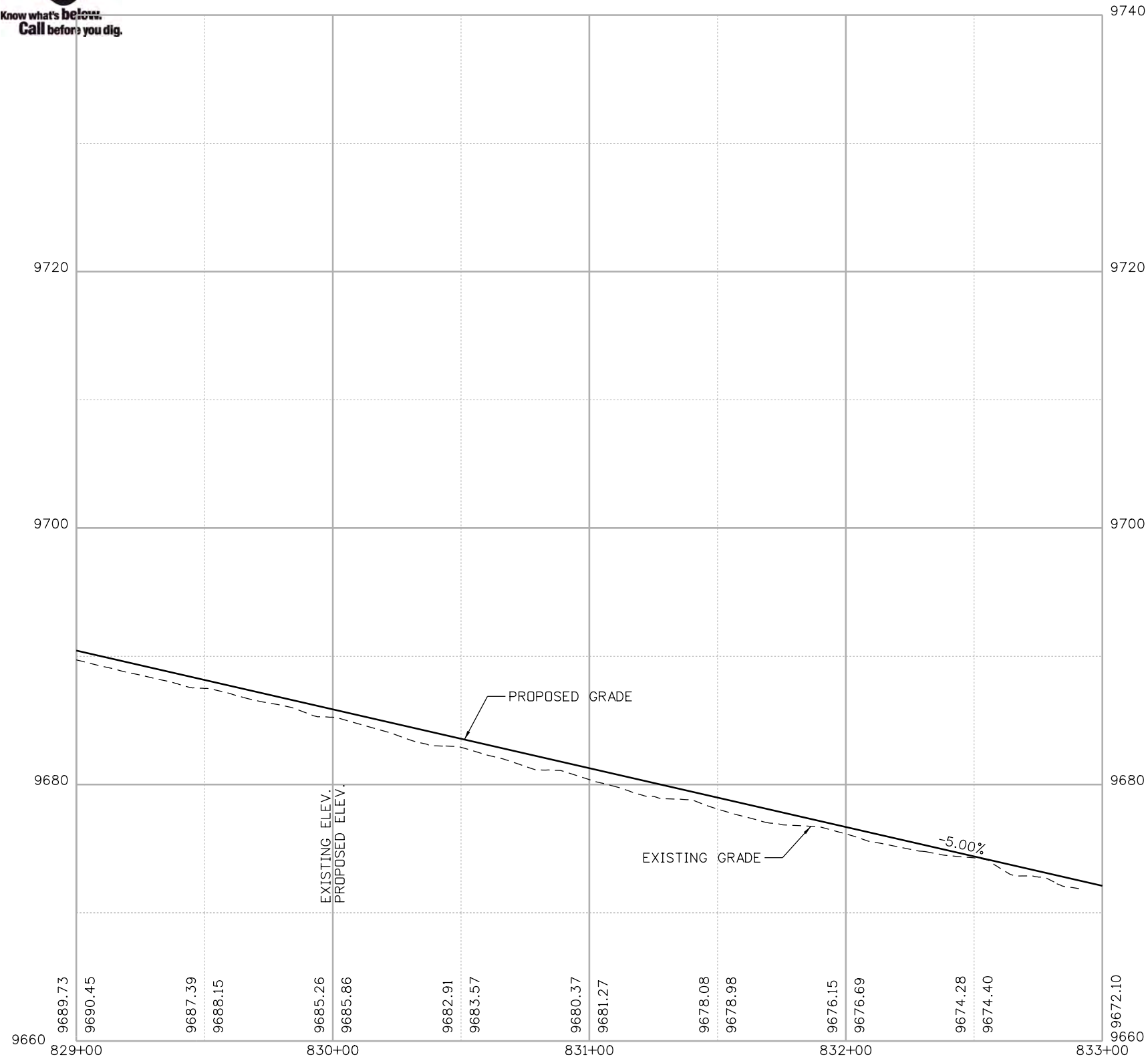
BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 47 of 48

Project No./Code
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Sheet Number 123



Know what's below.
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Nikolett 2:51:33 PM F:\1000.005.10 R3 C09 Blue River ACP\TrailDesign\Sheets\22768DES_Profile48.dgn

Print Date: 1/10/2020
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BLUE RIVER TRAIL PROFILE

Designer: CSA	Structure Numbers
Detailer: NG	
Sheet Subset: PROFILE	Subset Sheets: 48 of 48

Project No./Code
XXXXXXXXX
23001
Sheet Number 124

COLORADO DEPARTMENT OF TRANSPORTATION R3
ENGINEER'S OPINION OF PROBABLE COST
CO 9 South Summit Access Plan - Trail Design



Project Name	CO 9 South Summit Access Plan	Date:	09/25/19	
County of	Summit			Length In Miles 8.75
Type	Conceptual Trail Design (15%)	Path Thickness in inches	Pavement: 3"	Base: 4"
Prepared by	Stolfus & Associates, Inc.	Shoulder Thickness in inches	Pavement: 6"	Base: 6"

In providing opinions of probable construction cost, the Client understands that Stolfus & Associates Inc. has no control over costs or the price of labor, equipment or materials, or over the Contractor's method of pricing, and that the opinions of probable construction costs provided herein are to be made on the basis of our qualifications and experience. These costs do not reflect escalation for future costs. Stolfus & Associates, Inc. makes no warranty, expressed or implied, as to the accuracy of such opinions as compared to bid or actual costs.

ITEM NO.	ITEM DESCRIPTION	UNIT	QUANTITY	UNIT COST	COST
ATTACHED SIDEWALK					
203-00060	EMBANKMENT MATERIAL (COMPLETE IN PLACE)	CY	1,250	\$ 20.00	\$25,000
403-34741	HOT MIX ASPHALT (GRADING SX) (75) (PG 64-22)	TON	500	\$ 110.00	\$55,000
304-06007	AGGREGATE BASE COURSE (CLASS 6)	CY	200	\$ 40.00	\$8,000
608-00000	CONCRETE SIDEWALK	SY	1,200	\$ 40.00	\$48,000
609-21020	CURB AND GUTTER TYPE 2 (SECTION II-B)	LF	1,100	\$ 25.00	\$27,500
504	RETAINING WALL (MODULAR BLOCK)	SF	1,800	\$ 85.00	\$153,000
				TOTAL	\$316,500
DETACHED SHARED USE TRAIL					
203-00060	EMBANKMENT MATERIAL (COMPLETE IN PLACE)	CY	29,100	\$ 20.00	\$582,000
608-01500	BITUMINOUS BIKEWAY	TON	2,100	\$ 110.00	\$231,000
304-06007	AGGREGATE BASE COURSE (CLASS 6)	CY	1,400	\$ 40.00	\$56,000
	BRIDGE	SF	2,300	\$ 100.00	\$230,000
504	RETAINING WALL (MODULAR BLOCK)	SF	15,500	\$ 85.00	\$1,317,500
				TOTAL	\$2,416,500
SHARED USE SIDEPATH					
203-00060	EMBANKMENT MATERIAL (COMPLETE IN PLACE)	CY	21,900	\$ 20.00	\$438,000
608-01500	BITUMINOUS BIKEWAY	TON	7,400	\$ 110.00	\$814,000
304-06007	AGGREGATE BASE COURSE (CLASS 6)	CY	5,000	\$ 40.00	\$200,000
504	RETAINING WALL (MODULAR BLOCK)	SF	13,700	\$ 85.00	\$1,164,500
				TOTAL	\$2,616,500
HIGHWAY - WIDENED SHOULDERS					
203-00010	UNCLASSIFIED EXCAVATION (COMPLETE IN PLACE)	CY	9,000	\$ 30.00	\$270,000
403-34741	HOT MIX ASPHALT (GRADING SX) (75) (PG 64-22)	TON	6,400	\$ 110.00	\$704,000
304-06007	AGGREGATE BASE COURSE (CLASS 6)	CY	3,300	\$ 40.00	\$132,000
504	RETAINING WALL (MODULAR BLOCK)	SF	2,400	\$ 85.00	\$204,000
				TOTAL	\$1,310,000
				TOTAL MAJOR ITEMS	\$6,659,500

Item	Percent Range	Percent Selected	Costs \$
Major Items (above)			\$6,659,500 (A)

Signing & Striping	1 to 5% of (A)	2%	\$134,000 (B)
Drainage	5 to 15% of (A)	10%	\$666,000 (C)
Erosion Control	1 to 5% of (A)	5%	\$333,000 (D)
Construction Surveying	1 to 5% of (A)	2%	\$134,000 (E)
Construction Phasing & Traffic Control	5 to 25% of (A+B+C+D)	15%	\$1,169,000 (F)
Landscaping	1 to 5% of (A)	5%	\$333,000 (G)
Mobilization	4 to 10% of (A+B+C+D+E)	10%	\$793,000 (H)

TOTAL OPINION OF PROBABLE CONSTRUCTION BID ITEMS COST, CBI	(A+B+C+D+E+F+G+H)		\$10,221,500 (I)
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Force Account - Utilities	1 to 5% of (I)	1%	\$103,000 (J)
Force Account - Miscellaneous	1 to 5% of (I)	5%	\$512,000 (K)

TOTAL OPINION OF PROBABLE CONSTRUCTION ITEMS COST, CI	(I+J+K)		\$10,836,500 (L)
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Construction Engineering, CE	10 to 25% of (L)	20.45%	\$2,217,000 (M)
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Engineering Design/Clearances	10 to 20% of (L)	10%	\$1,084,000 (N)
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Utilities	5 to 10% of (L+M+N)	5%	\$707,000 (O)
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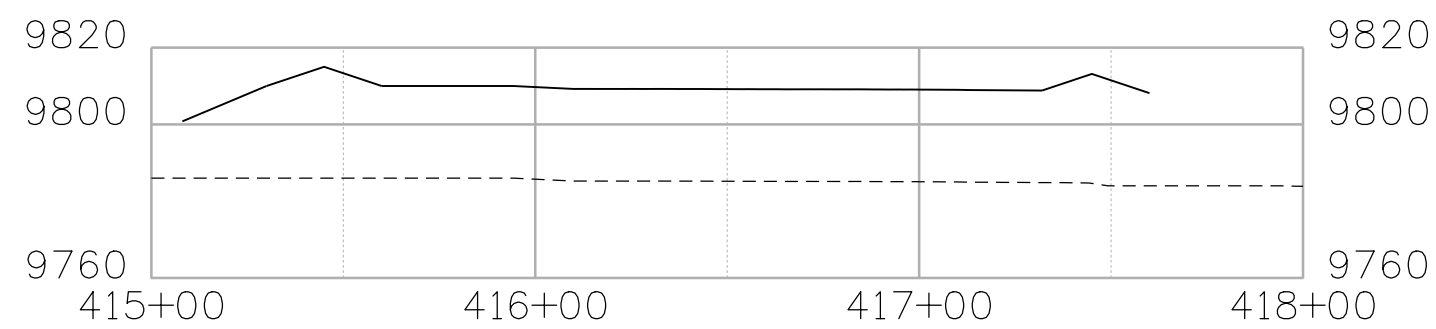
Contingency	30% of (L+M+N+O)	30%	\$4,454,000 (P)
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TOTAL COMPONENT PROJECT OPINION OF PROBABLE COST	(J+K+L+M+N)		\$19,300,000
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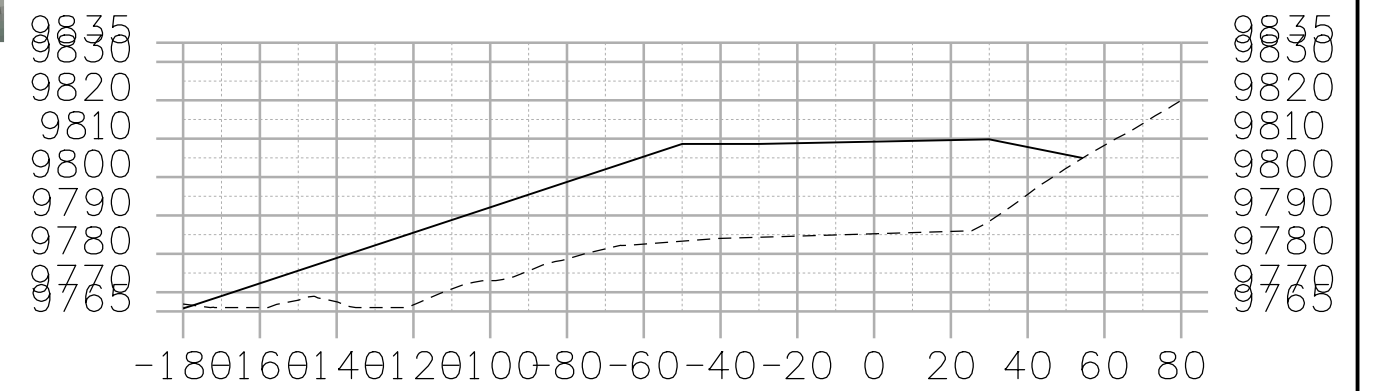
Appendix I-Conceptual Wildlife Crossing and Opinion of Probable Cost



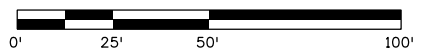
Nikolett 3:29:18 PM P:\1000.005.10 R3 CD9 Blue River ACP\TrailDesign\Working\Wildlife Xing\22768DES_WildlifePlan03.dgn



SECTION A-A



SECTION B-B



Print Date: 10/8/2019
 File Name: 22768DES_WildlifePlan03.dgn
 Horiz. Scale: 1:1 Vert. Scale: As Noted

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 5690 DTC Boulevard, Suite 560E
 Greenwood Village, CO 80111
 Phone: 303-221-2330
 Fax: 303-221-2331
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WILDLIFE CROSSING CONCEPT 2:1 SLOPE DESIGN			
Designer:	NG	Structure	
Detailer:	NG	Numbers	
Sheet Subset:		Subset Sheets:	

Project No./Code
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Sheet Number

